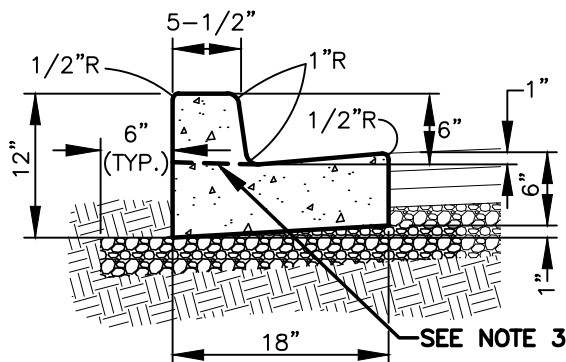
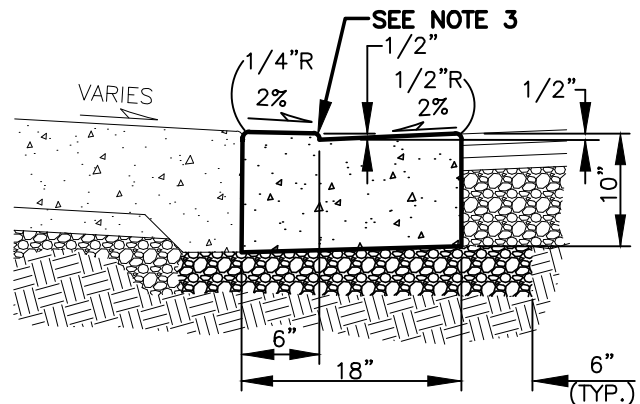
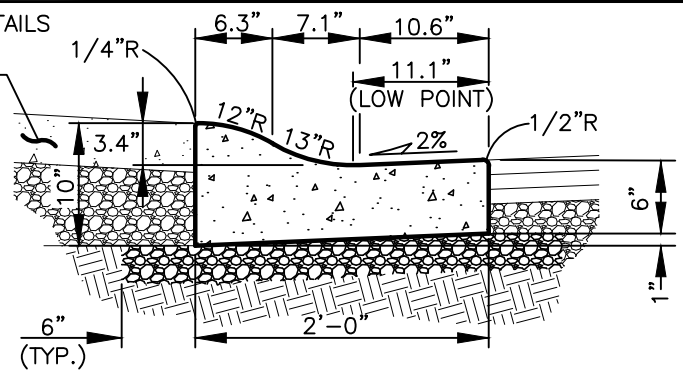


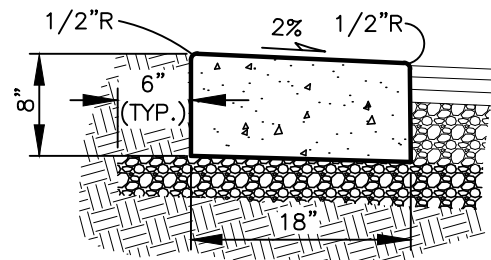
TYPE E-1 CURB



TYPE A-1 CURB AND GUTTER



CURB AND GUTTER AT DRIVEWAY DROP



NOTES:

1. CONCRETE CURBS SHALL BE 3000 PSI MIN. (CL 3000), 3-1/2" SLUMP (MAX.), DRIVEWAY OPENINGS SHALL BE 4000 PSI MIN. (CL 4000).
2. CURBS ADJACENT TO PAVEMENT OR SIDEWALK SHALL HAVE EXPANSION AND/OR CONSTRUCTION JOINTS TO MATCH EXISTING PATTERNS. 3/8" EXPANSION JOINTS SHALL BE PLACED ON BOTH SIDES OF CATCH BASINS, AT TOPS OF DRIVEWAYS, ALL CHANGES IN DIRECTION, AND AS DIRECTED BY THE INSPECTOR. CONTRACTION JOINTS TO BE PLACED AT 15' MAXIMUM SPACING.
3. FOR CURB DROPS AT DRIVEWAYS 1/2" LIP WITH A 1/2" TOOLED EDGE, SEE **RAMP LIP, DRIVEWAY LIP AND DETECTABLE WARNING PATTERN DETAIL T02-15**.
4. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% MAXIMUM DRY DENSITY (3" MIN. DEPTH).
5. SEE **PAVEMENT RESTORATION/WIDENING AT CURBS DETAIL T05-01A**.
6. CURB TO BE MEDIUM BROOM FINISHED, PARALLEL TO GUTTER LINE.
7. WHERE MATCHING EXISTING CURBS, ALL EXISTING EDGES SHALL BE SAWCUT.
8. WHEN ATTACHED SIDEWALKS ARE USED WITH ROLLED CURB AND GUTTER, THICKENED SIDEWALKS (6" MIN.) SHALL BE CONSTRUCTED UNDER THE SAME CONSTRUCTION CONTRACT.
9. FOR CURBS AROUND MEDIANS, USE 12" WIDE CURB OR CURB AND GUTTER. SEE **TYPICAL PEDESTRIAN REFUGE SECTION A-A DETAIL T02-17C** WHERE APPLICABLE.

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REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
1	3/17	RAW	MHA	
2	4/18	RAW	MHA	
3	9/21	RAW	MHA	
APPROVED BY: <i>M.H. Pedraza</i>				ENGINEER MANAGER
APPROVED DATE: 9/1/2021				

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CONCRETE CURBS

STANDARD PLAN
NUMBER

T01-01A

SEE NOTE 5

VARIES

1/4" R 2%

1/2" R 2%

1/2"

10"

6"

18"

6"

(TYP.)

CURB AND GUTTER AT ADA RAMPS

- \\CITYAPPS\AUTOCAD\STD_DETAILS\DRAWING_FILES\T01-01B

REV NO.	DATE	BY	APPR
1	3/17	RAW	MHA
2	4/18	RAW	MHA
3	9/21	RAW	MHA

**PUBLIC WORKS – STREETS
AND TRANSPORTATION**

APPROVED BY: *MHA*
ENGINEER MANAGER

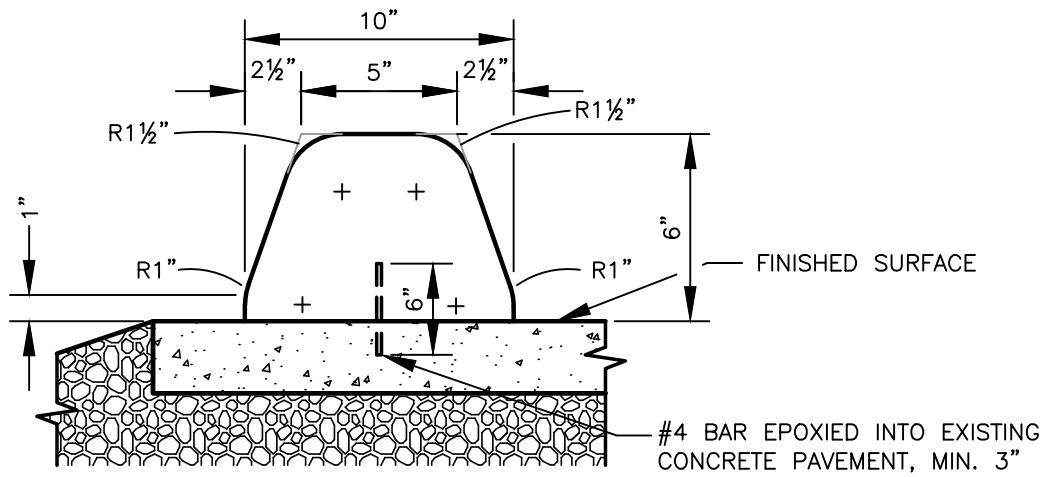
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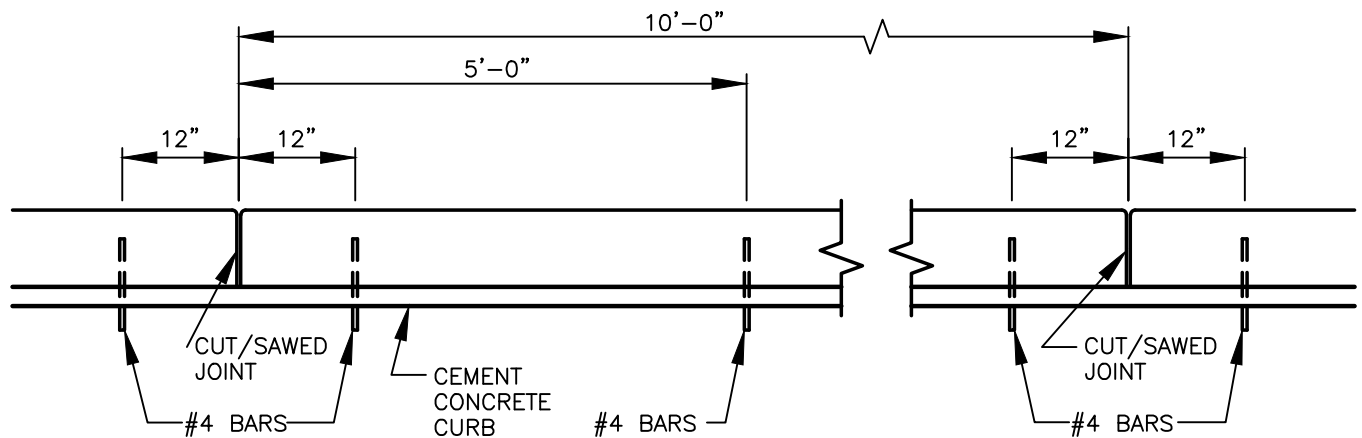
ADA RAMP CURBS

**STANDARD PLAN
NUMBER**

T01-01B



EXTRUDED CONCRETE DOWELED CURB



SPACING OF ANCHOR BARS

NOTES:

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3-1/2" SLUMP (MAX.).
2. FOR CONCRETE CURBS, 3/8" EXPANSION JOINTS SHALL BE PLACED ON BOTH SIDES OF CATCH BASINS, AT TOPS OF DRIVEWAYS, ALL CHANGES IN DIRECTION, OR AS DIRECTED BY INSPECTOR. 1 1/2" CONTRACTION JOINTS TO BE PLACED AT 10' INTERVALS. SEE **CONCRETE JOINTS DETAIL T05-02**.
3. THE PAVEMENT SHALL BE DRY AND CLEANED OF LOOSE AND DELETERIOUS MATERIAL PRIOR TO PLACEMENT.
4. CONTRACTOR TO PROTECT EXTRUDED CURB UNTIL FULLY CURED.
5. TO BE USED FOR TEMPORARY USE OR ENGINEERS APPROVAL.

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REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
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2	3/05	RAW	MHA	
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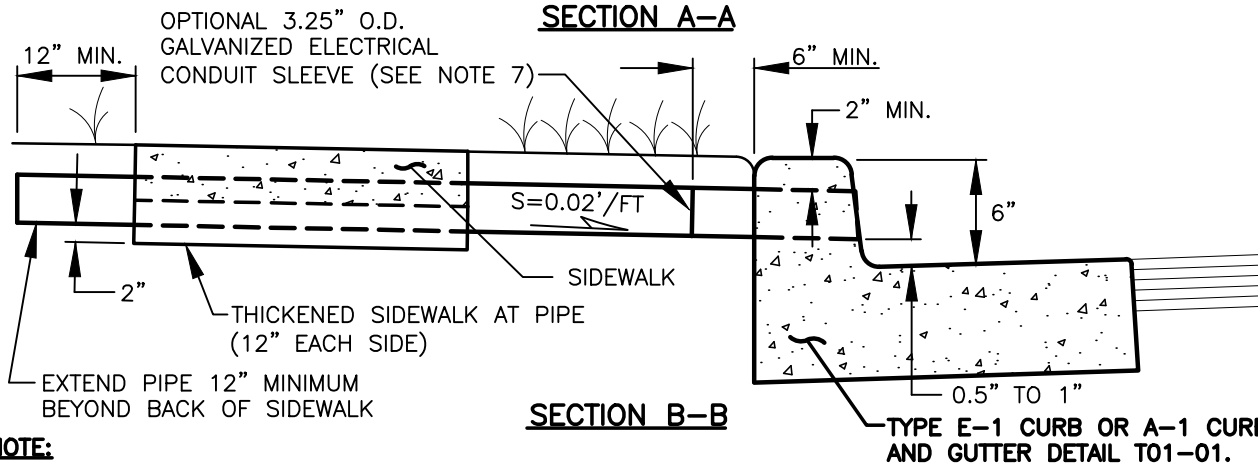
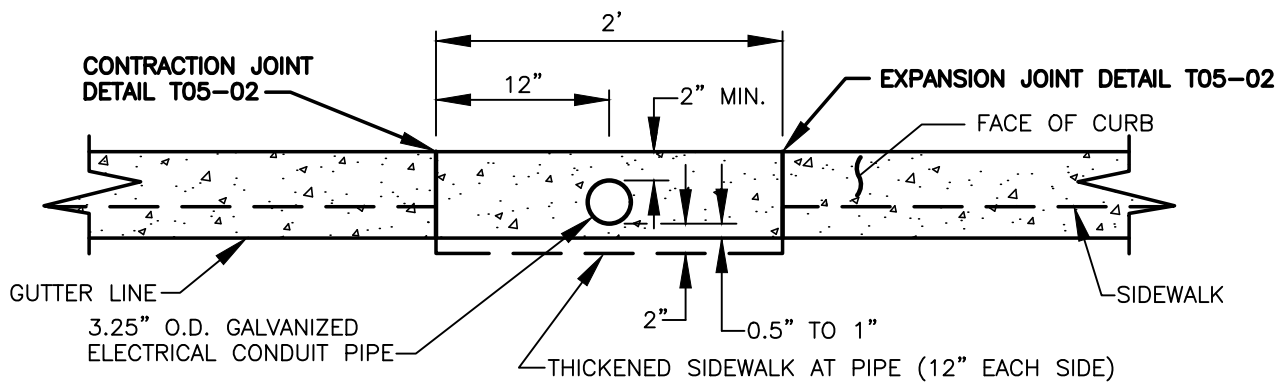
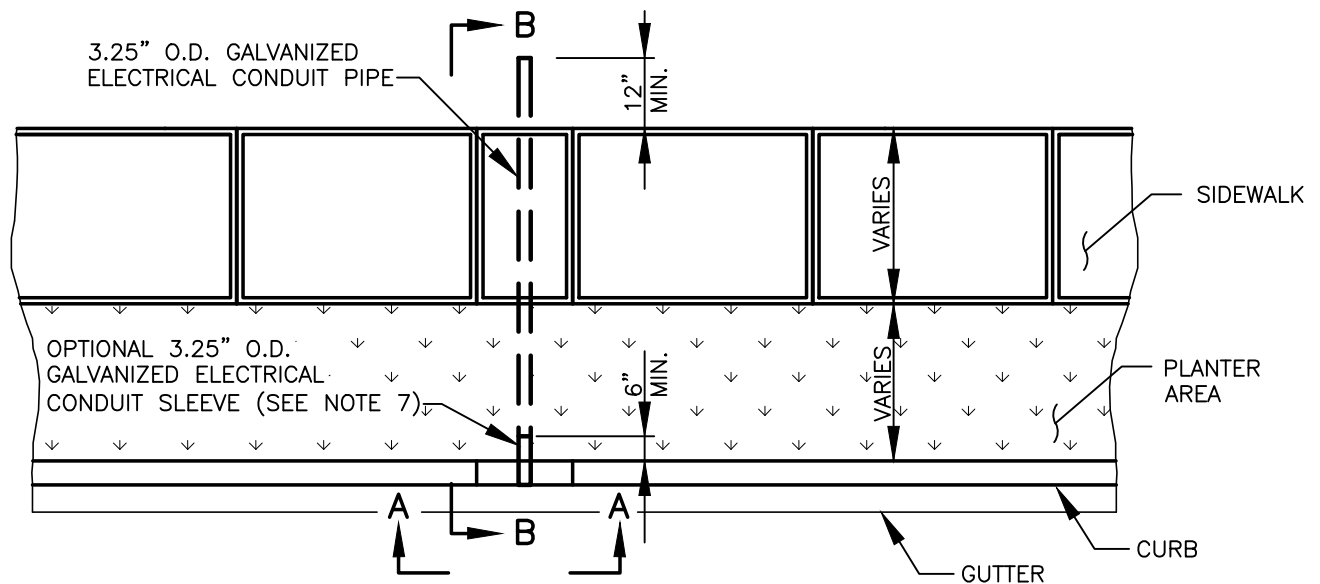
APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021



EXTRUDED CONCRETE
DOWELED CURB

STANDARD PLAN
NUMBER

T01-02



NOTE:

1. WHEN CURB DRAINS ARE USED, DRAINAGE FACILITIES MUST BE SIZED FOR BOTH QUANTITY AND QUALITY STORM WATER TREATMENT.
2. DETACHED SIDEWALK SHOWN. ATTACHED SIDEWALK AND CITY CENTER SIDEWALK SIMILAR.
3. FINISH PIPE END FLUSH WITH FACE OF CURB.
4. GROUT ANY VOIDS IN CONCRETE SURROUNDING PIPE.
5. SHOW LOCATION ON PLAN TO AVOID CONFLICT'S WITH STREET LIGHTS, FUTURE DRIVEWAY DROPS, WATER METERS AND OTHER UTILITIES.
6. CURB DRAINS NOT ALLOWED IN ROLLED CURBS.
7. CONNECT SLEEVE AND CURB DRAIN SO THAT THE CONNECTION IS WATERTIGHT.

REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
1	3/06	RAW	MHA	
2	2/07	RAW	MHA	
3	8/08	RAW	MHA	
4	7/14	RAW	MHA	
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6	9/21	RAW	MHA	

APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER

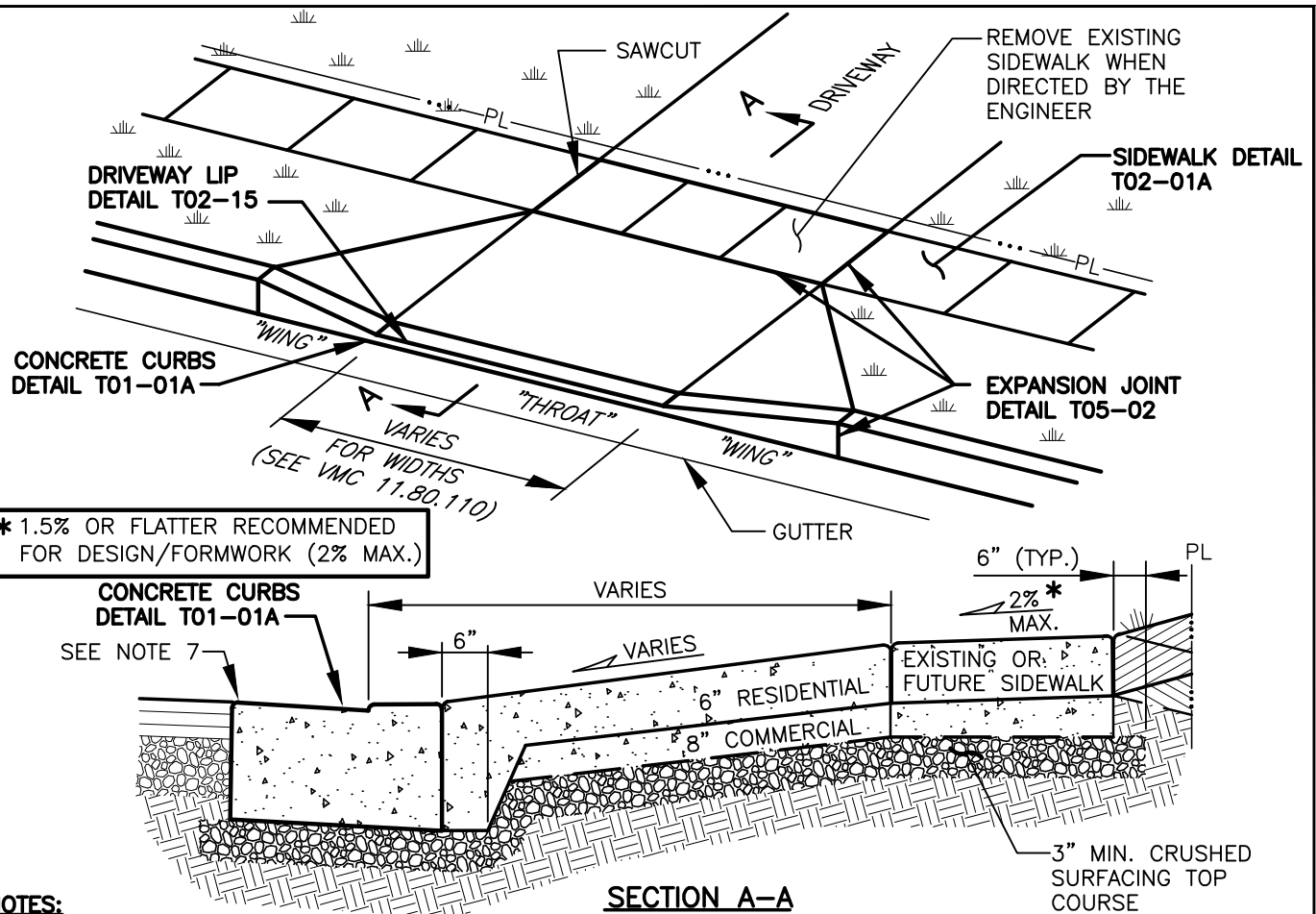
APPROVED DATE: 9/1/2021

CITY OF
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CURB DRAIN

STANDARD PLAN
NUMBER

T01-03



NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMMERCIAL DRIVEWAYS REQUIRE 8" CONCRETE WITH REINFORCING STEEL (6x6 - W2.9xW2.9 WWF, MIN.), 1 1/2" COVER FROM BOTTOM OF SLAB. RESIDENTIAL DRIVEWAYS REQUIRE 6" CONCRETE.
4. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
5. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
6. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
7. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
8. ALL EXISTING EDGES SHALL BE SAWCUT.
9. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
10. 3' WING MIN. FOR NON-ARTERIAL STREETS.
11. 45° ANGLE FOR WINGS ON ARTERIAL STREETS.
12. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
13. FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.

REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
1	1/15	RAW	MHA	
2	3/17	RAW	MHA	
3	4/18	RAW	MHA	
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6	9/21	RAW	MHA	

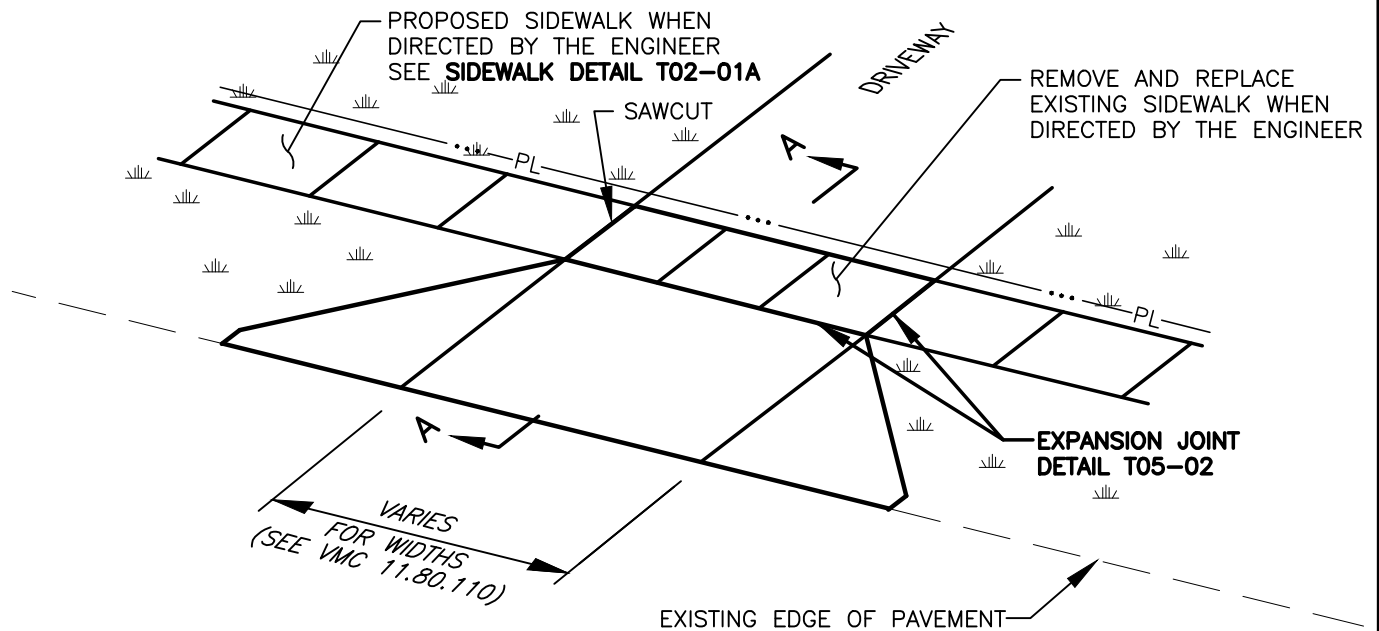
APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021

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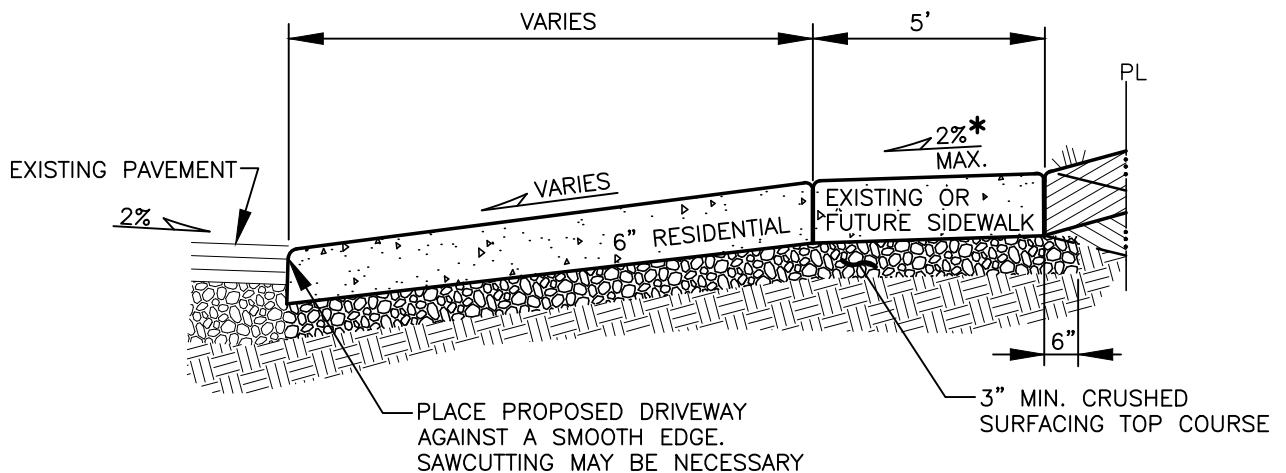
DRIVEWAY WITH
DETACHED SIDEWALK

STANDARD PLAN
NUMBER

T01-04



* 1.5% OR FLATTER RECOMMENDED
FOR DESIGN/FORMWORK (2% MAX.)



SECTION A-A

NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
4. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
5. 3' WING MIN.
6. NO WATER METERS IN DRIVEWAY APPROACH OR WING.
7. FOR SINGLE FAMILY RESIDENCE DEVELOPMENTS ALONG ROADWAYS WITH OUT CURBS.
8. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.

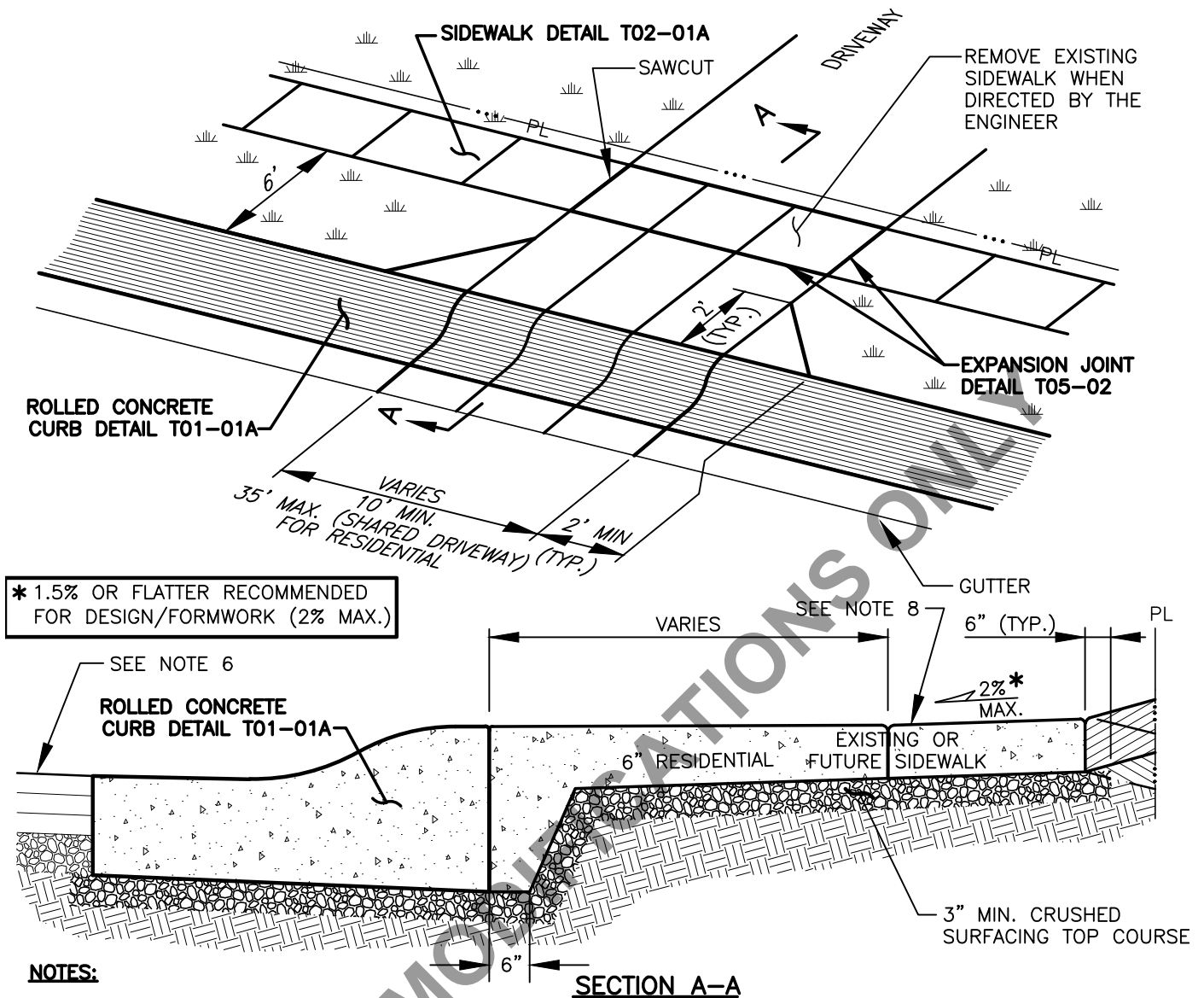
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3	1/15	RAW	MHA	
4	3/17	RAW	MHA	
5	4/18	RAW	MHA	
6	9/21	RAW	MHA	

APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021

CITY OF
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SINGLE FAMILY RESIDENCE
DRIVEWAY WITH
WINGS AND NO CURB

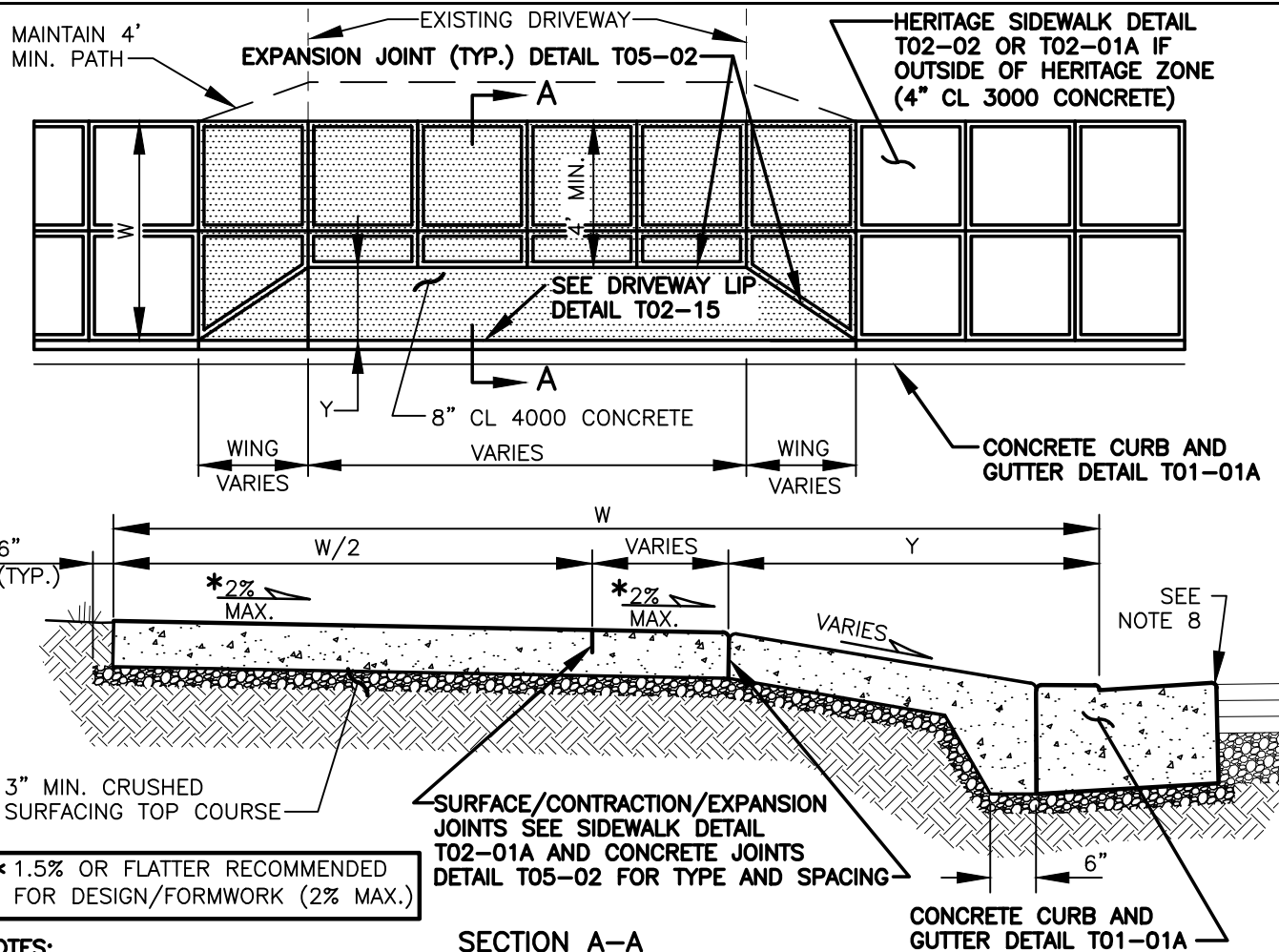
STANDARD PLAN
NUMBER
T01-04B



NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
4. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
5. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01** WHEN CUTTING EXISTING CURB.
7. ALL EXISTING EDGES SHALL BE SAWCUT.
8. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
9. NO WATER METERS IN DRIVEWAY APPROACH OR WINGS.
10. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.

ROLLED CURB DRIVEWAY WITH DETACHED SIDEWALK
(T01-04C)



- IF W IS LESS THAN 8' IN WIDTH, THEN Y=2'. (IF W=<6', THEN PUSH OUT SIDEWALK BEHIND DRIVEWAY TO MAINTAIN 4' MIN. PATH). IF W IS MORE THAN 8' AND LESS THAN 12' IN WIDTH, THEN Y=W/2
IF W IS GREATER THAN OR EQUAL TO 12' IN WIDTH, THEN Y=4'
- CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3½" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
- REINFORCING STEEL REQUIRED (6x6 - W2.9xW2.9 WWF, MIN.), MIN. 1½" COVER FROM BOTTOM OF SLAB.
- COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
- DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED BY THE CITY INSPECTOR. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
- PARALLEL JOINTS SHALL BE SEPARATED BY A MINIMUM OF 2'.
- SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
- ALL EXISTING EDGES SHALL BE SAWCUT.
- EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
- IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
- FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.

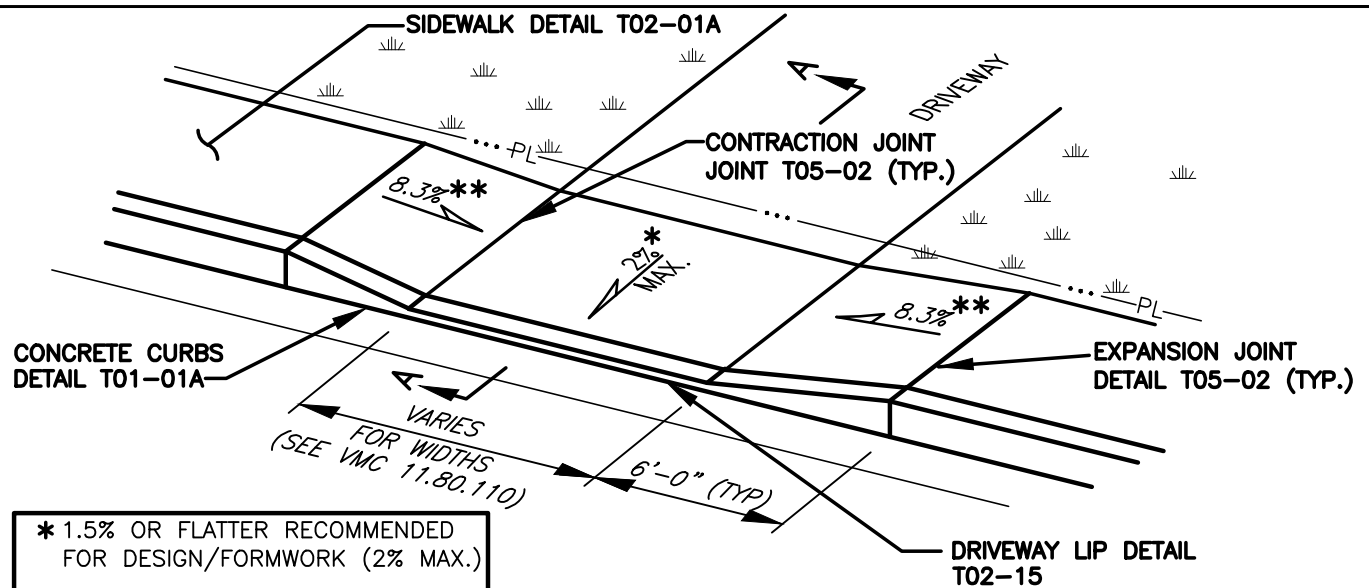
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2	3/17	RAW	MHA	
3	4/18	RAW	MHA	
4	7/19	RAW	MHA	
5	2/21	OIS	MHA	
6	9/21	RAW	MHA	

APPROVED BY: *M.H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021

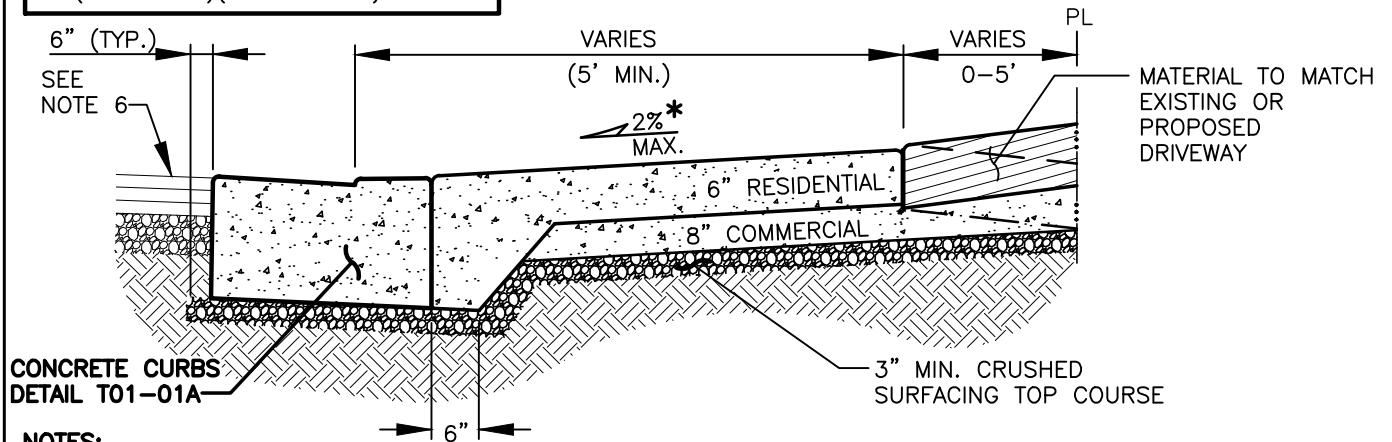


DRIVEWAY WITH ATTACHED
SIDEWALK OPTION A

STANDARD PLAN
NUMBER
T01-05A



* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)
 ** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)(SEE NOTE 4)



NOTES:

SECTION A-A

- CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
- COMMERCIAL DRIVEWAYS REQUIRE REINFORCING STEEL (6x6 - W2.9xW2.9 WWF, MIN.), MIN. 1 1/2" COVER FROM BOTTOM OF SLAB.
- COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
- DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
- EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
- SEE **PAVEMENT RESTORATION/WIDENING AT CURBS DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
- ALL EXISTING EDGES SHALL BE SAWCUT.
- SET ALL POLES AND SIGNS BEHIND SIDEWALK.
- IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
- FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.

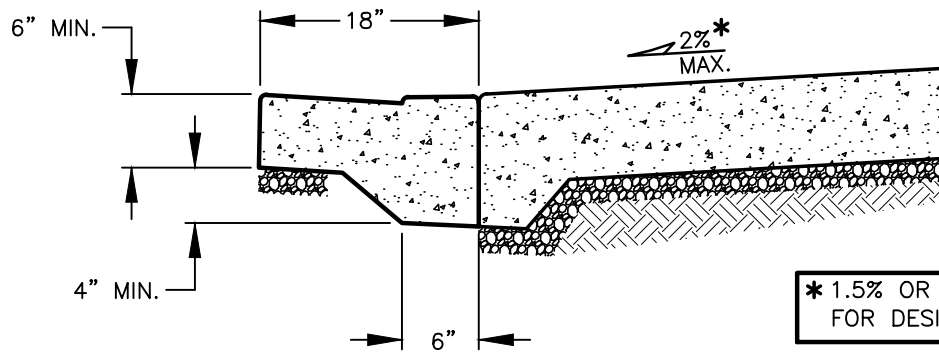
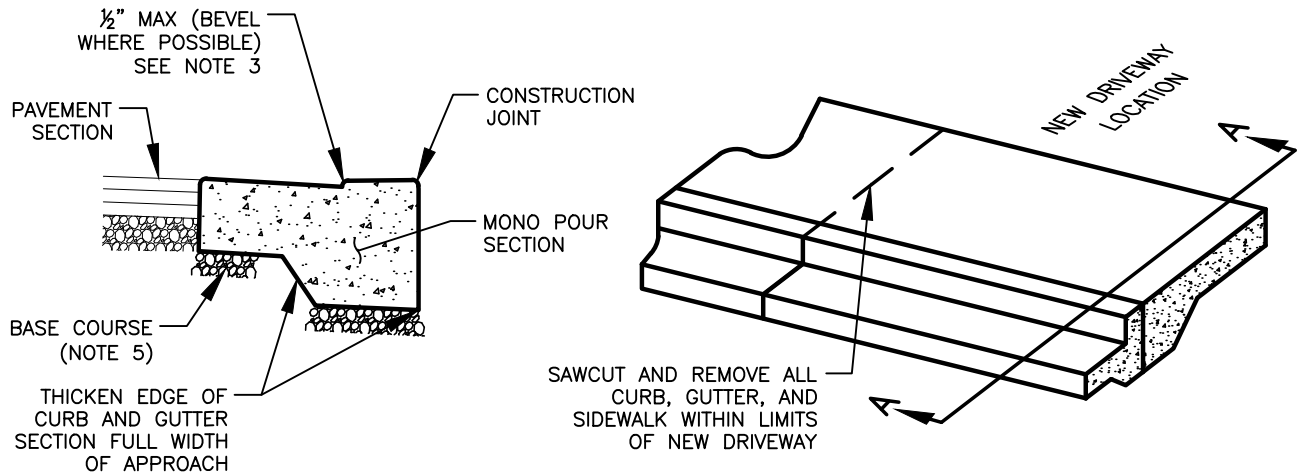
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3	4/18	RAW	MHA	
4	7/19	RAW	MHA	
5	2/21	OIS	MHA	
6	9/21	RAW	MHA	

APPROVED BY: *M.H. Pedraza*
 ENGINEER MANAGER
 APPROVED DATE: 9/1/2021

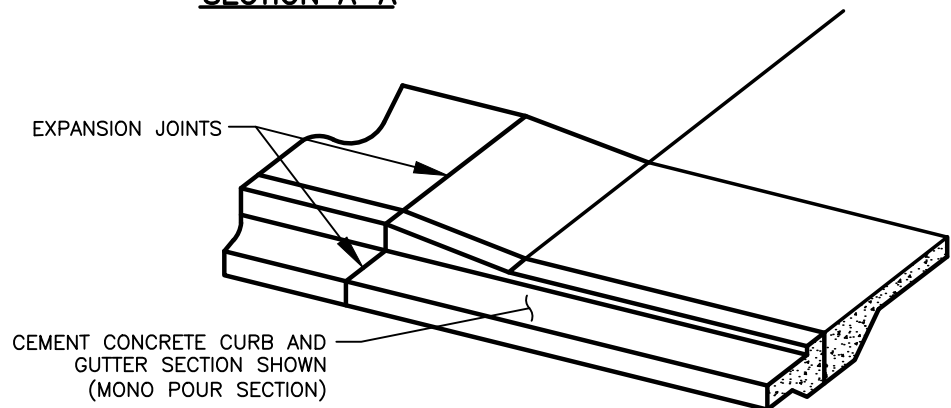
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DRIVEWAY WITH ATTACHED
 SIDEWALK OPTION B

STANDARD PLAN
 NUMBER
 T01-05B



SECTION A-A



HALF ISOMETRIC

NOTES:

1. CONCRETE SHALL BE CLASS 4000 MIN.
2. CURB TO BE BRUSHED FINISHED. ALL EXISTING EDGES SHALL BE SAWCUT.
3. 1/2" VERTICAL LIP ACROSS APPROACH.
4. SUBGRADE SHALL BE COMPACTED TO 95% MAX DRY DENSITY.
5. MIN. 3" DEPTH CSBC, COMPACTED TO 95% OF MAX DRY DENSITY.
6. SEE **STANDARD DETAIL T05-02** FOR CONCRETE JOINTS.

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2	9/21	RAW	MHA

PUBLIC WORKS – STREETS AND TRANSPORTATION

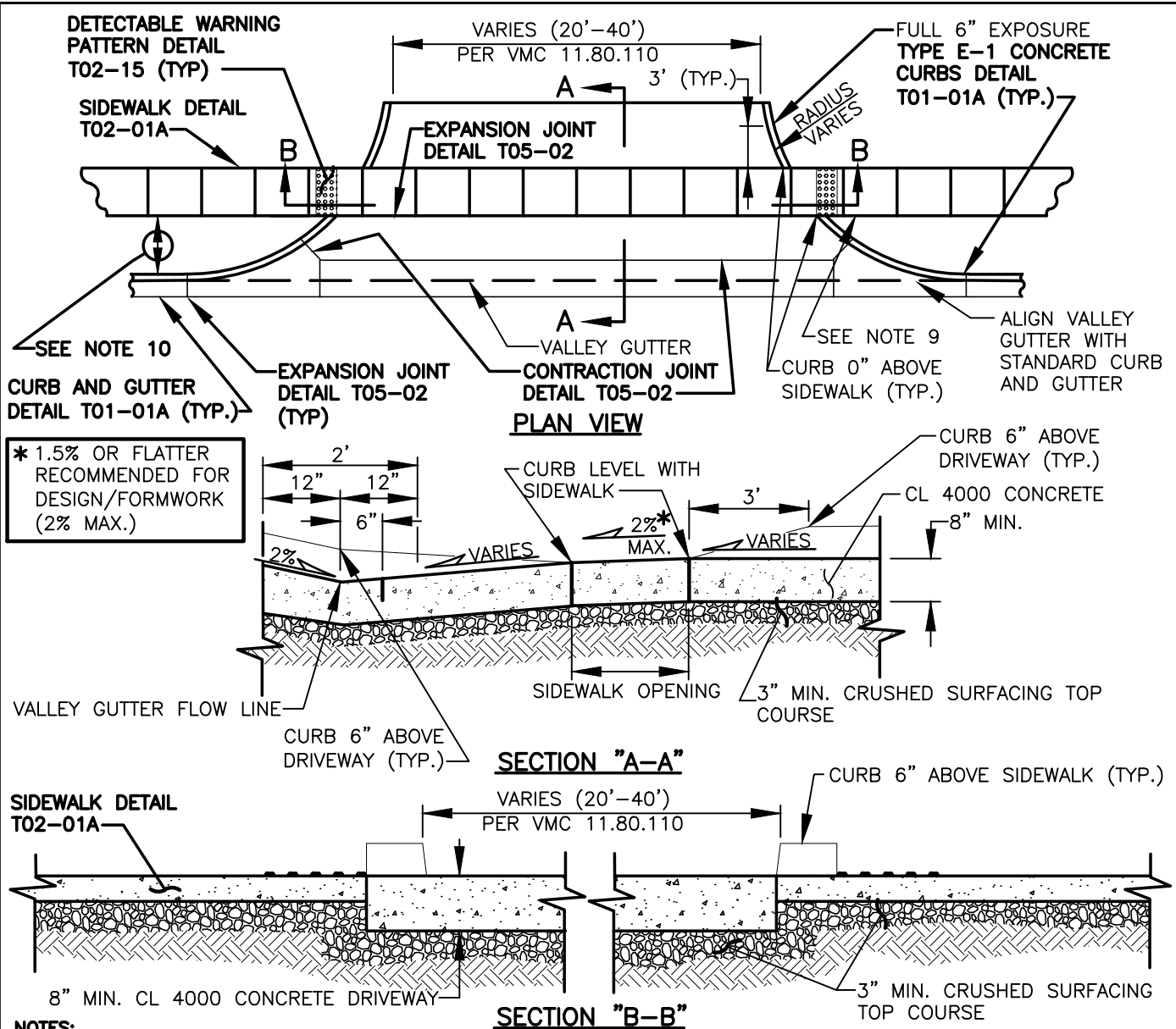
APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER

APPROVED DATE: 9/1/2021

CITY OF
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WASHINGTON

CONCRETE DRIVEWAY
CUT DETAIL

STANDARD PLAN
NUMBER
T01-06



NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN., (CL 4000) 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. DRIVEWAY SHALL BE CONSTRUCTED WITH REINFORCING STEEL (6x6 - W2.9xW2.9 WWF, MIN.), MIN. 1 1/2" COVER FROM BOTTOM OF SLAB.
3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
4. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. CONTROL JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
5. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
7. MAXIMUM 2% CROSS SLOPE ACROSS PEDESTRIAN CROSSING.
8. TRANSITION CURB FROM FULL 6" EXPOSURE TO 0" OVER THE FIRST 6' FROM CORNER.
9. MAINTAIN ADJACENT SIDEWALK GRADES THROUGH DRIVEWAY (NO RAMPS) UNLESS OTHERWISE APPROVED.
10. SEE **T10-XX SERIES STANDARD PLANS** FOR PLANTER STRIP WIDTH.
11. CURB RETURN ELEVATIONS SHALL BE SHOWN ON PLANS TO DETERMINE SIDEWALK GRADE.
12. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.

REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
1	8/08	RAW	MHA	
2	7/14	RAW	MHA	
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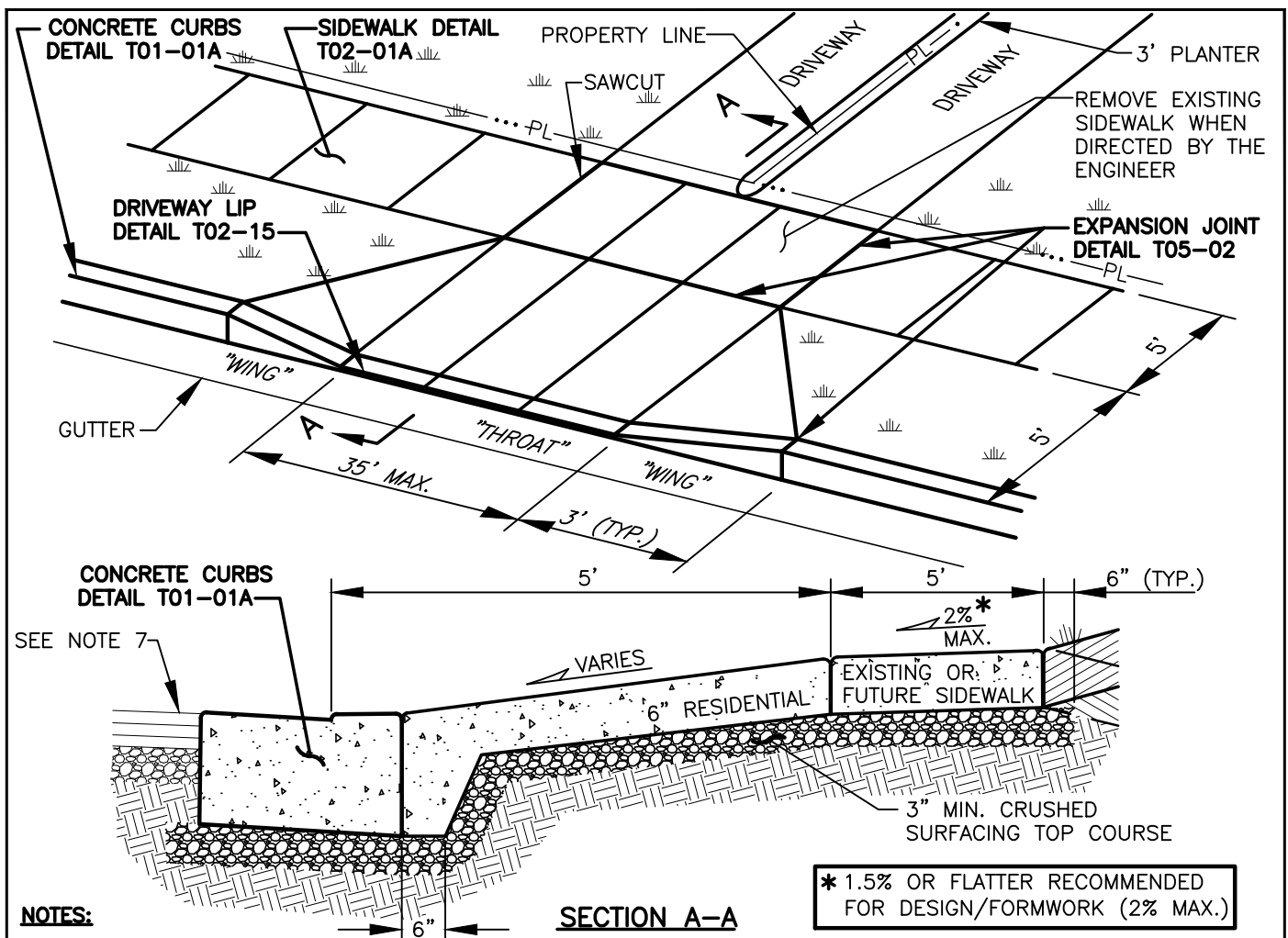
APPROVED BY: *M. H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021



MAJOR COMMERCIAL
DRIVEWAY

STANDARD PLAN
NUMBER

T01-07



NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
4. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
5. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
7. ALL EXISTING EDGES SHALL BE SAWCUT.
8. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
9. WHERE DRIVEWAY IS OFF OF ROADWAY, SIDEWALK IS NEEDED, IF DRIVEWAY IS OFF OF ALLEY, NO SIDEWALK IS REQUIRED.
10. SEE **NARROW LOT STREET PLAN AND SECTION DETAILS T10-24A AND T10-24B AND NARROW LOT ALLEY PLAN AND SECTION DETAILS T10-25A AND T10-25B** FOR ADDITIONAL INFORMATION.
11. OPTIONAL DRIVEWAY WINGS ARE AVAILABLE THROUGH THE "ADMINISTRATIVE ROAD MODIFICATION" PROCESS.
12. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
13. FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.

* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

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REV NO.	DATE	BY	APPR	PUBLIC WORKS - STREETS AND TRANSPORTATION
1	1/15	RAW	MHA	
2	3/17	RAW	MHA	
3	4/18	RAW	MHA	
4	7/19	RAW	MHA	
5	2/21	OIS	MHA	
6	9/21	RAW	MHA	

APPROVED BY: *M.H. Pedraza*
ENGINEER MANAGER
APPROVED DATE: 9/1/2021

CITY OF
Vancouver
WASHINGTON

SHARED DRIVEWAY WITH
DETACHED SIDEWALK

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1. STREET NAME SHALL BE STAMPED ON PROPOSED CURB AND BE CENTERED WITHIN THE 5' WIDE PEDESTRIAN RAMP
2. TEXT STYLE SHALL BE SansSerif OR APPROVED EQUAL.
3. CONTRACTOR SHALL SUBMIT STAMP SAMPLE FOR CITY APPROVAL PRIOR TO STAMPING.
4. FAILURE TO STAMP STREET NAMES WILL REQUIRE CURB REPLACEMENT AT THE CONTRACTORS EXPENSE.
5. STAMP SHALL BE PLACED WITHIN 30 MINUTES OF CONCRETE POUR AND SHALL BE CLEARLY VISIBLE AFTER CURING.
6. CONTRACTOR SHALL PLACE EXISTING CONTRACTOR AND DATE STAMPED CONCRETE IN PLANTER STRIP AT GRADE. ONE PER NAME PER INTERSECTION

CITY OF
Vancouver
WASHINGTON

**STANDARD PLAN
NUMBER**

T01-09