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Agenda

- Project Overview
- Traffic Analysis findings
- Engagement Plan

Project Purpose



Safety

- Make travel safer and more comfortable for all travelers



Equity

- Engage with - and provide solutions that benefit - a full range of community members



Climate

- Identify solutions that support BRT on the corridor, and walking & small mobility modes

Meeting Purpose & Outcome

Purpose

- Review traffic analysis and findings from Phase 1
- Provide an overview of public engagement plan

Target Outcome

- Share phase 1 traffic findings; discuss how they integrate with phase 2 work
- Share engagement plan, gather feedback on tactics & goals

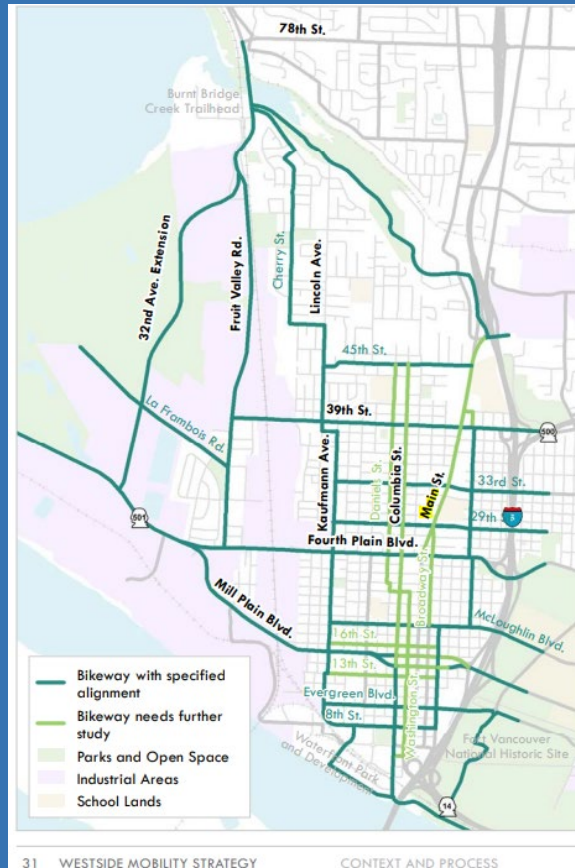
Project Overview

- Project area is Main Street from Fourth Plain Boulevard to northern city limits (~1.7 miles)
- Carter Park, Shumway, Lincoln, West Minnehaha neighborhoods
- Paving from 39th Street to Hazel Dell Avenue and northbound I-5 off ramp to city limits
- C-TRAN BRT opening 2027



- Upper Main Project area
- - - Paving Scheduled 2025
- Proposed C-Tran BRT Station

Project Overview: Previous Planning Work



Comprehensive Plan (2011) Identified as a growth corridor
Westside Mobility Strategy (2016) “Main Street is established as the vibrant, commercial spine of the westside and a source of civic identity...”

- 4.1 Implement capital projects that would improve pedestrian accessibility in Uptown Village
- 4.2 Coordinate Main Street improvements from 5th to 45th Street to achieve economic development objectives and improve safety and streetscape conditions
- Recommended network connections:
Jefferson/Kauffman; F Street/ C Street

Vancouver City Center Vision (2007) “Improve Main Street Corridor as a central spine of diverse and complementary uses that establish downtown as a regional center for commerce, culture, and urban living”

Project Overview: Transportation System Plan (TSP) Modal Networks

- Primary Pedestrian Corridor
- Enhanced Transit Corridor
- Protected Bike and Small Mobility lanes or Multi-use path (north of E 39th Street)

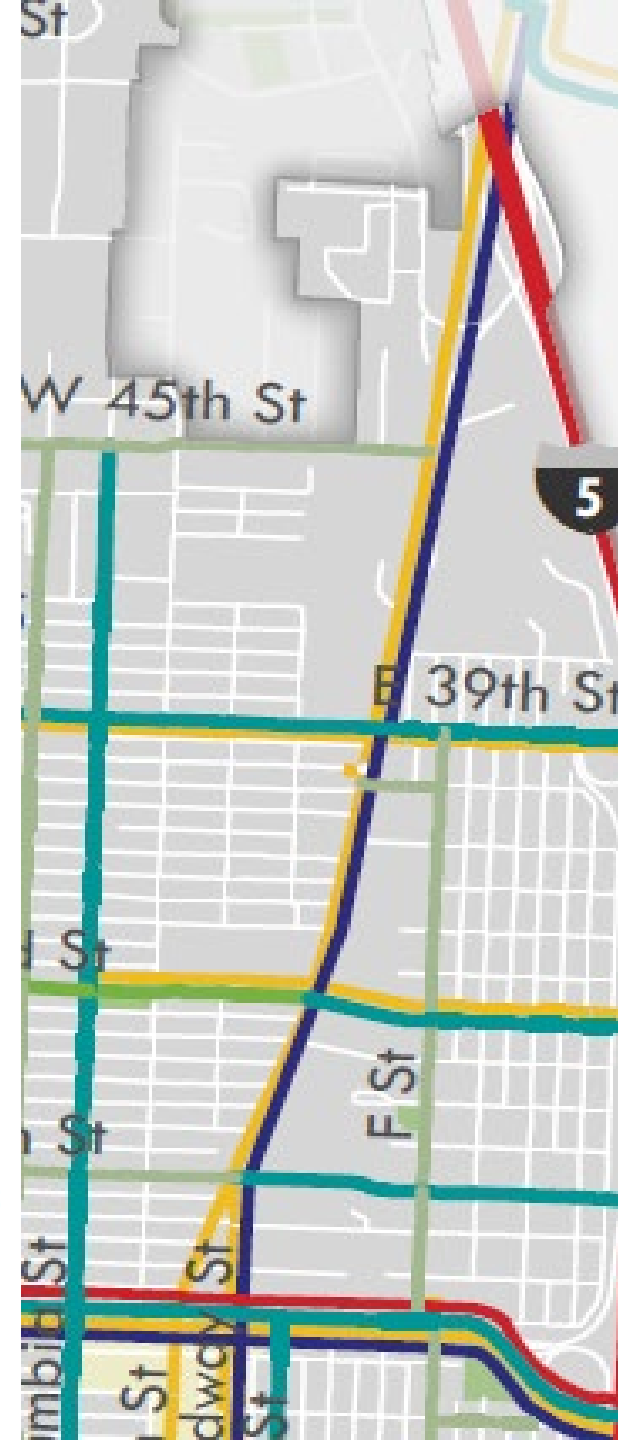
Zone 1

Primary Pedestrian Network

- Pedestrian Centers
- Pedestrian Corridors (Primary)
- Enhanced Transit Corridors
- Freight Corridors

Primary Bicycle and Small Mobility Network

- Protected / Buffered Mobility Lanes
- Mobility Lanes
- Neighborhood Greenways



Traffic Analysis



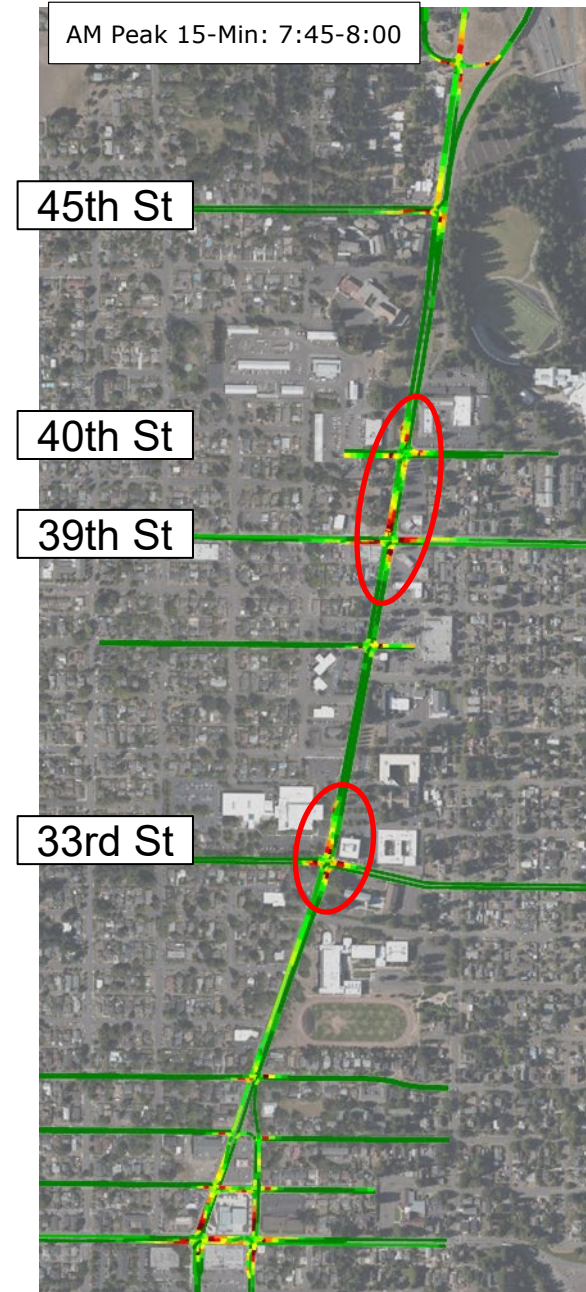
**Existing
Conditions &
Analysis Goals**



Traffic Findings

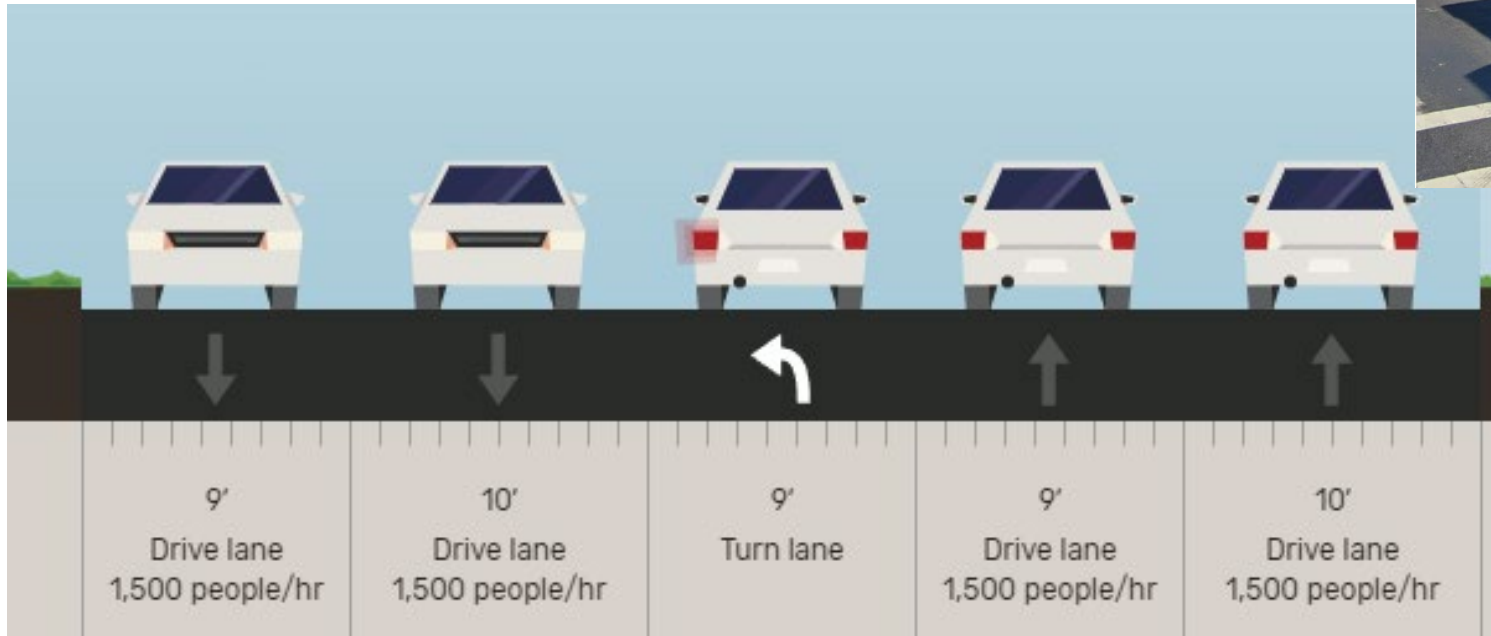
Existing Traffic Conditions

- Main/39th: Peak period congestion
- Southbound queues occasionally back up from 39th to 40th
- Modeling represents typical peak conditions
- Southbound Main experiences diversion from I-5 incident (not modeled)



Existing Lane Conditions

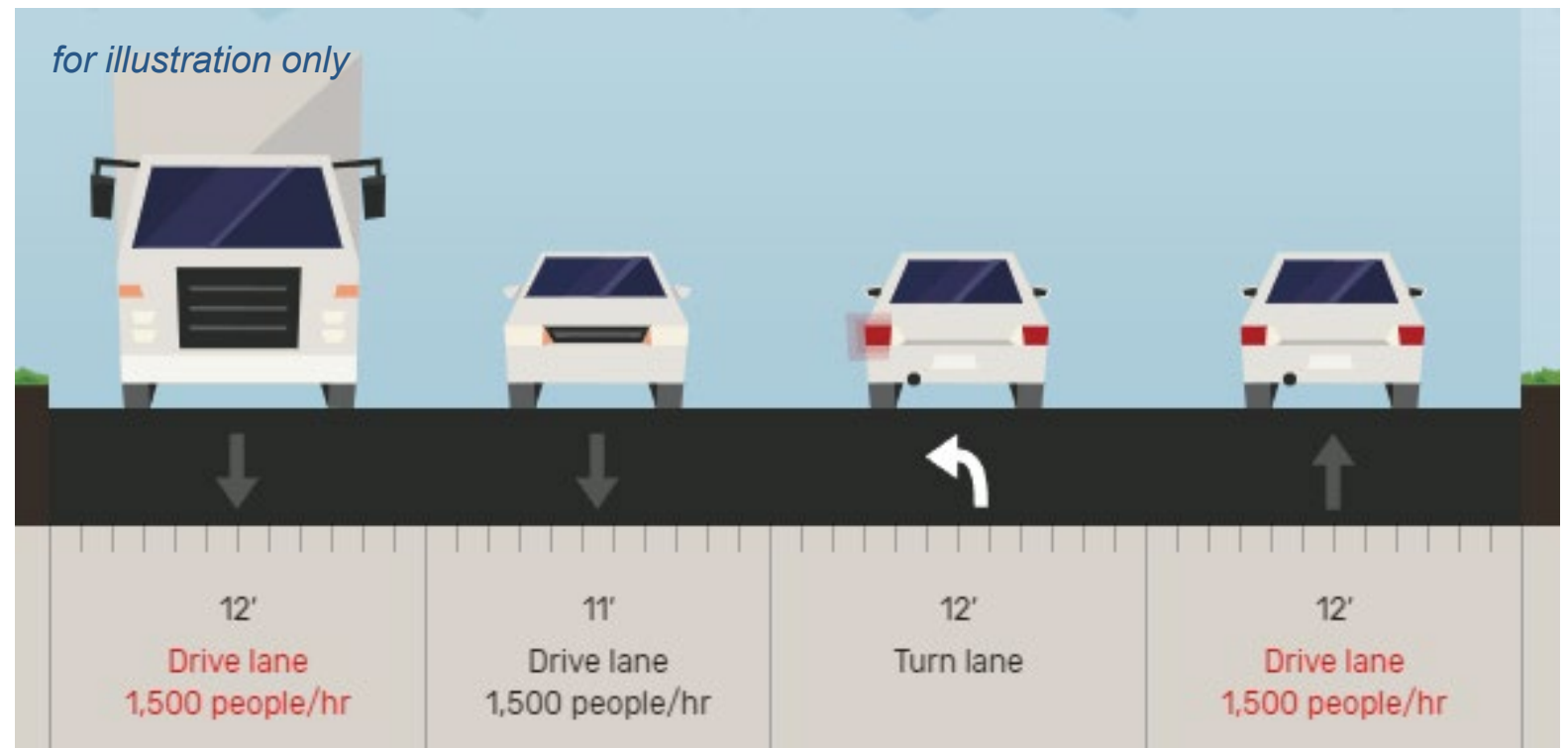
- Currently buses have a 9-10 ft lane along Main St
- Difficult to maneuver or stay within the lane



Changing Lane Configuration: Alternative 1

Alternative 1: Northbound Main St reconfigured to one vehicle lane

- Turn lanes still provided northbound and southbound
- Allows for at least an 11-foot travel lane for all vehicles
- Two lanes southbound accommodates bus reliability during incidents.

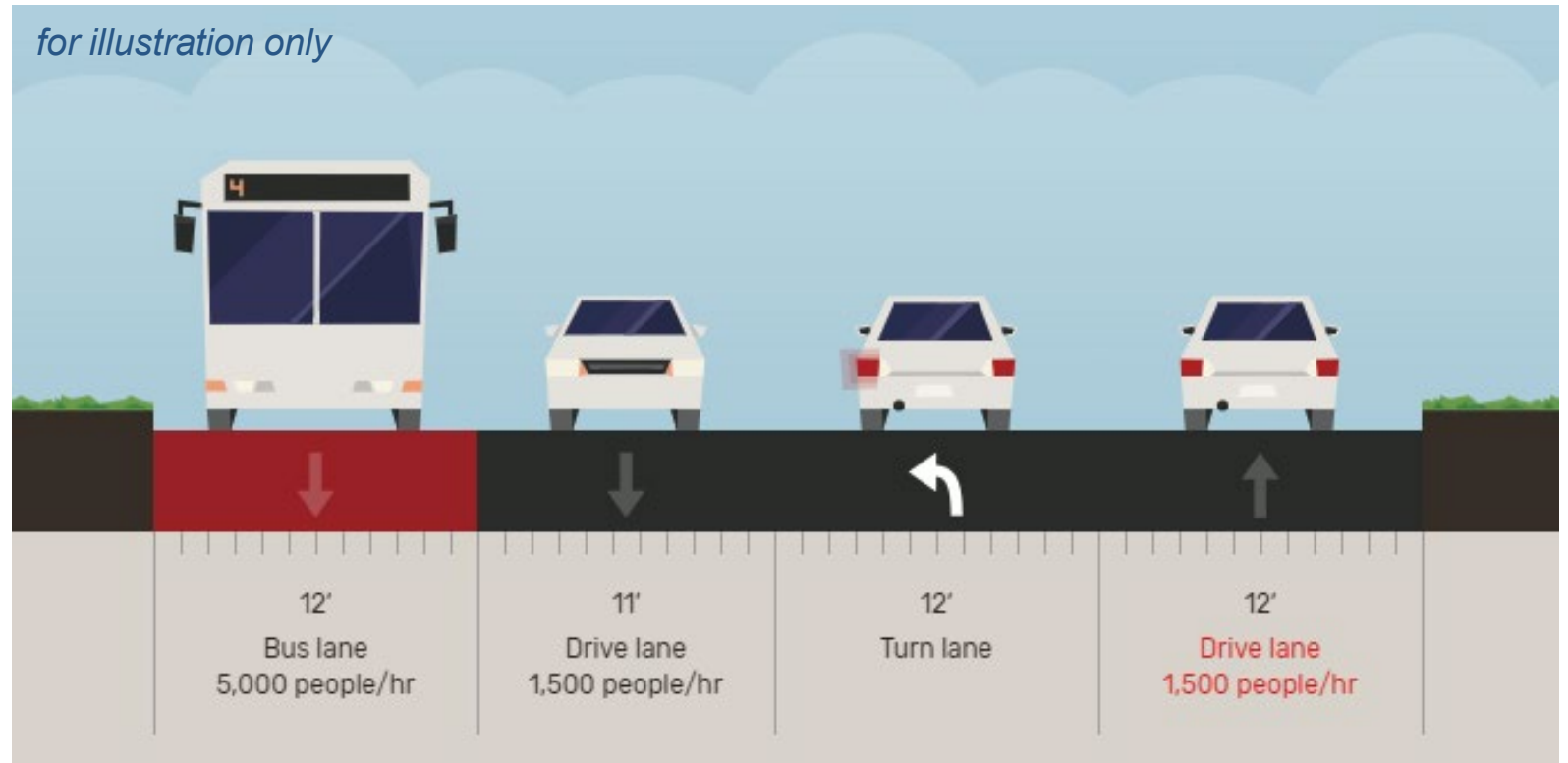


Changing Lane Configuration: Alternative 2

Alternative 2:
Northbound Main St
reduced to one lane

Southbound Main St
converted to BAT
lane

- Same cross-section as Alternative 1
- Southbound Main St outside lane converts to a Business Access Transit (BAT) lane
 - Cars use BAT lane for right turns at intersections and driveways



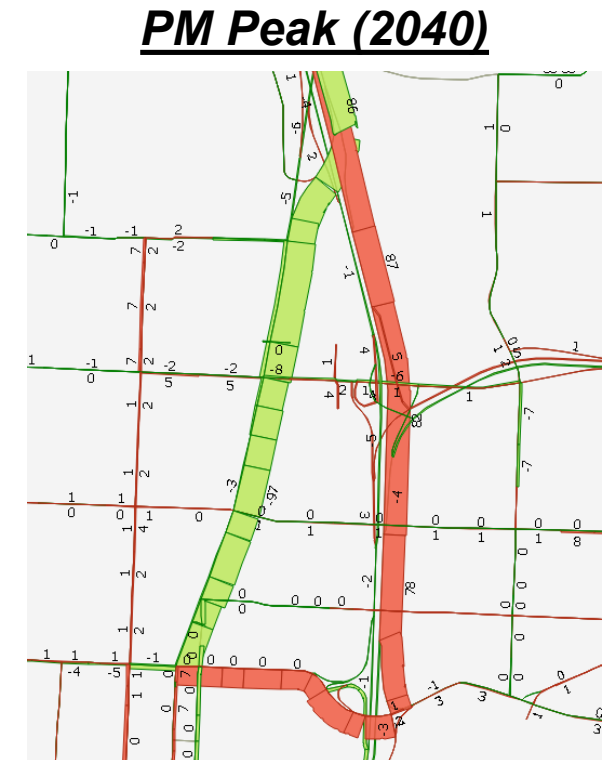
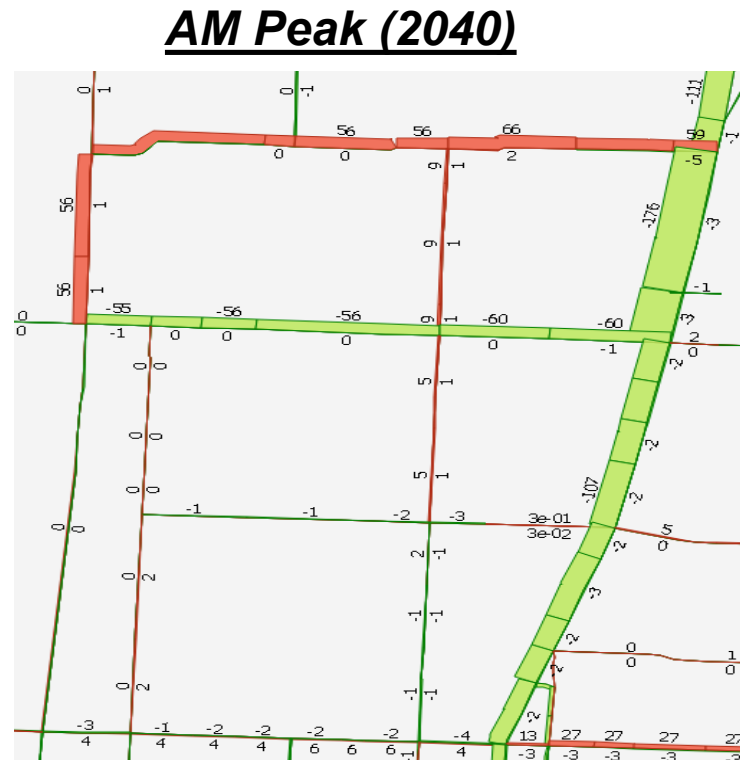
Testing single travel lanes: Volume Diversion

AM Peak (2040)

- ~100 vehicles (20%) stay on I-5 until Fourth Plain instead of using Main St
- Some vehicles turn early at 45th instead of 39th
- Results in queue back to 42nd

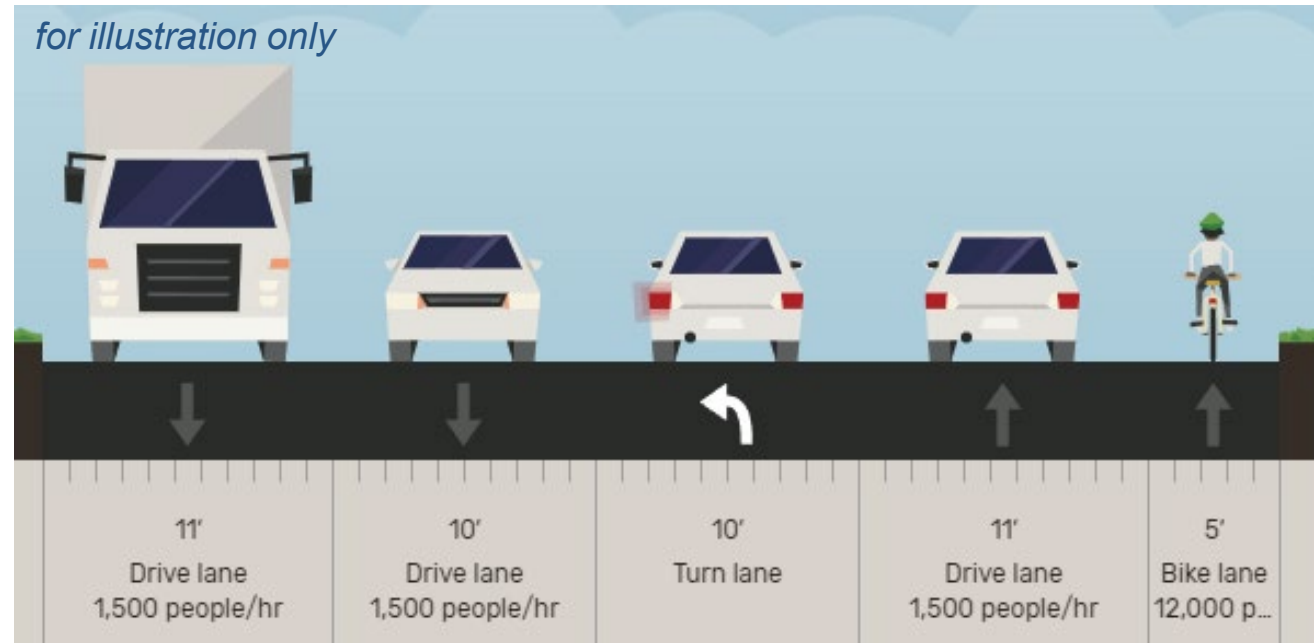
PM Peak (2040)

- ~90 vehicles (25%) get onto I-5 at Fourth Plain to go north



Other Considerations

- With 11-foot transit lanes, only space for one 5 ft. mobility lane within existing Main Street cross section
- North-South BSM network east on F Street to 40th St. and west on Columbia Street to 45th St.
- I-5 Interstate Bridge Replacement (IBR) expected to relieve some I-5 congestion, but incidents may still cause traffic to divert to Main Street



Traffic Operations Results

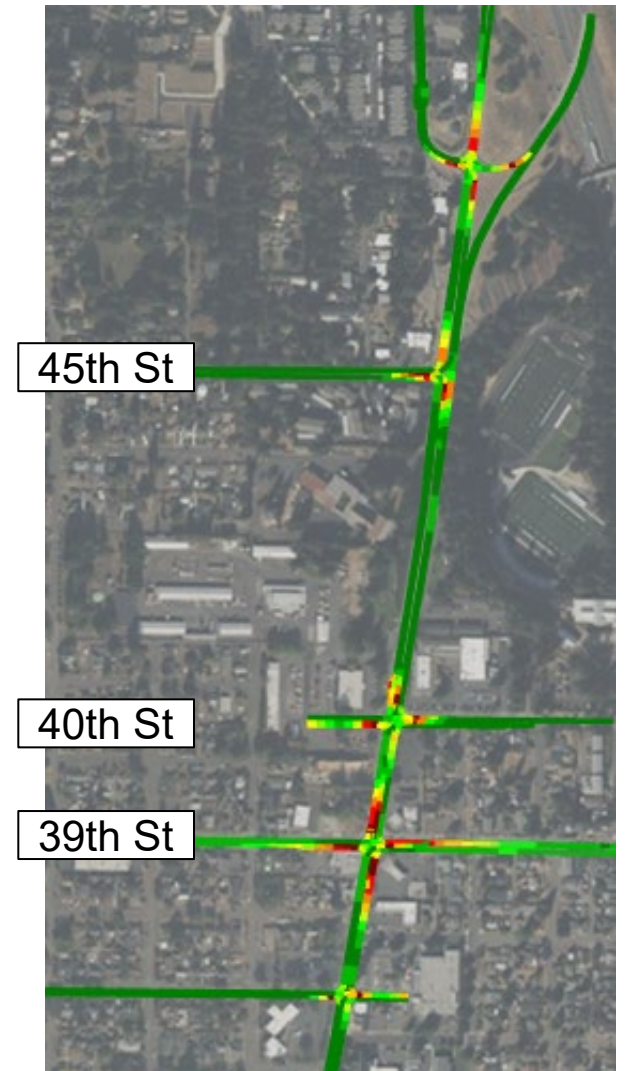
Baseline (year 2040)

- Due to lower forecasted growth along Main St, 2040 operations are similar to existing conditions
- Main & 39th faces some congestion during both peaks. Southbound queues occasionally back up to 40th

Baseline AM Peak



Baseline PM Peak

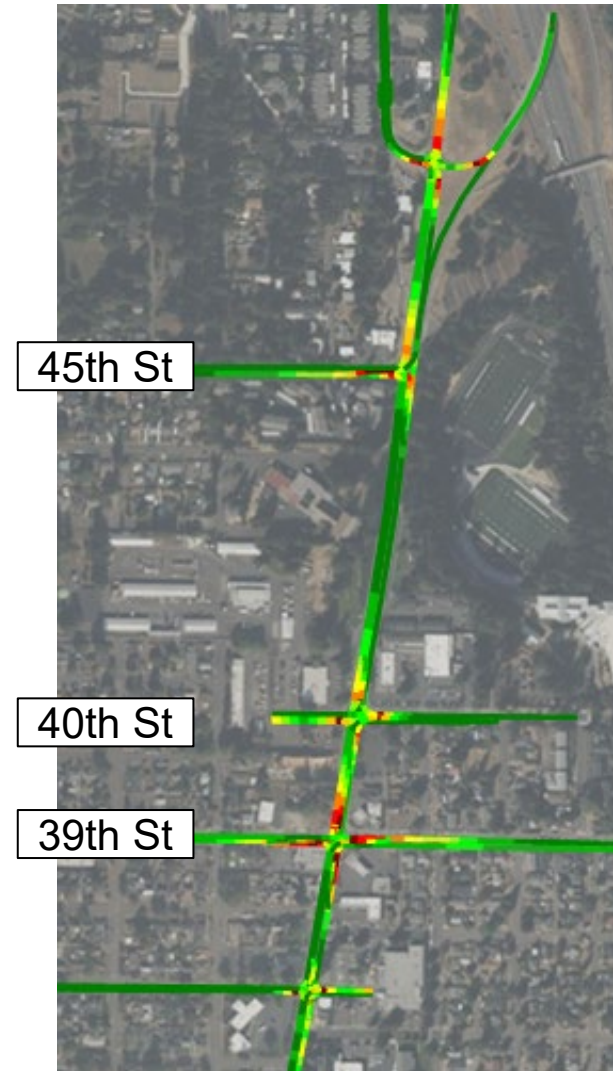


Traffic Operations Results

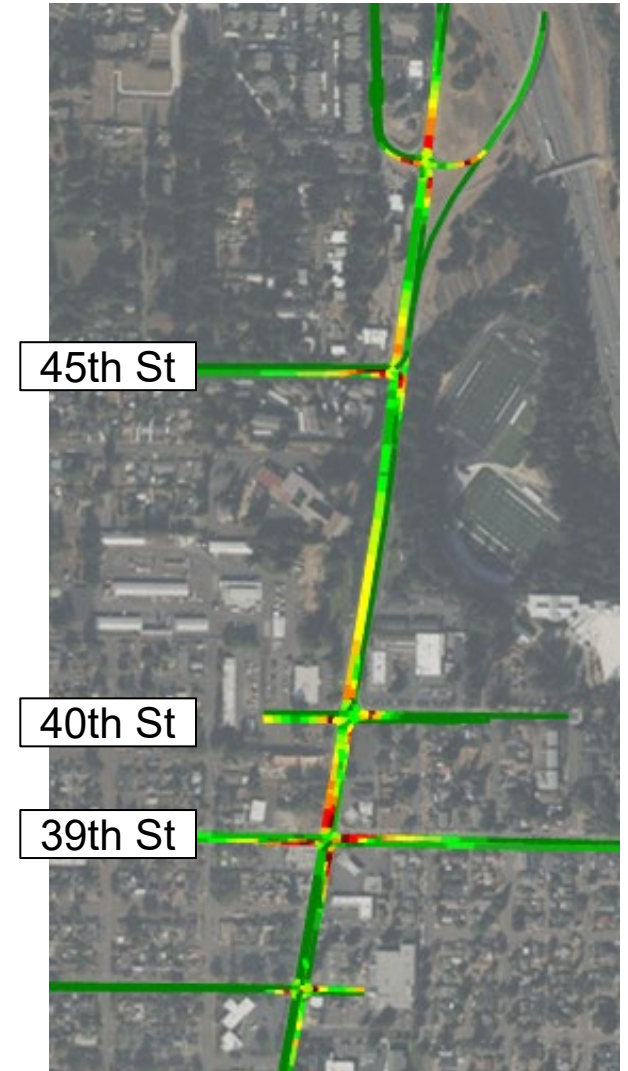
AM Peak (year 2040)

- Alternative 1: congestion resembles baseline, with some backups at 39th but not major
- Alternative 2: Going southbound, the BAT lane starts at 40th Street
 - Congestion still begins at 39th Street
 - Queues occasionally reach back to Kiggins Bowl / First Presbyterian

Alternative 1



Alternative 2



Traffic Operations Results

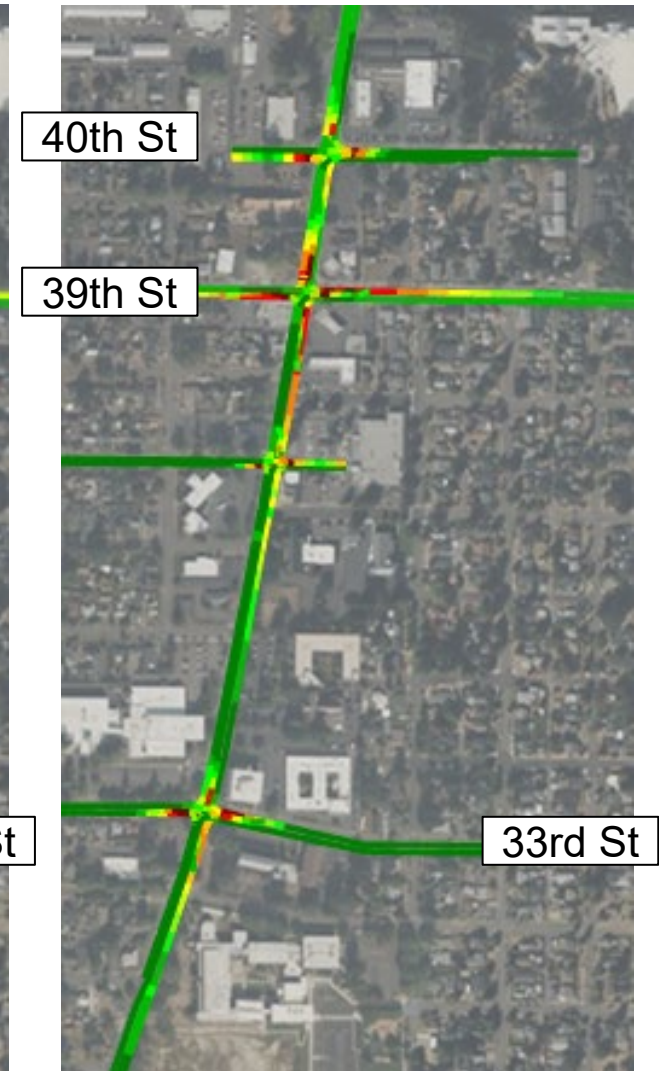
PM Peak (year 2040)

- Northbound is the peak direction
- Alternative 1 & 2 show similar operations
- Northbound queues spill back from 39th Street to around 35th Street
- Westbound approach along 39th Street also doesn't clear every cycle

Alternative 1

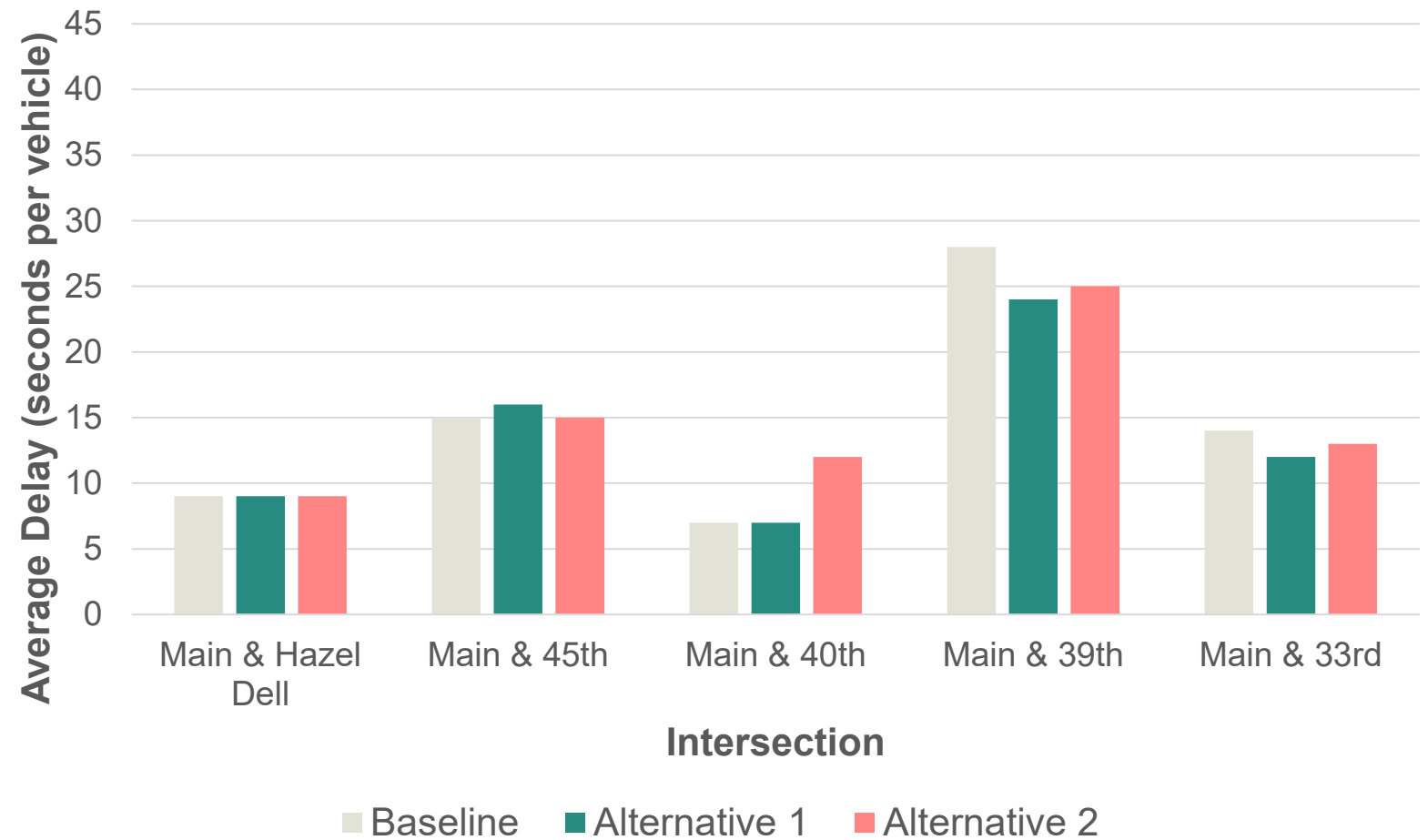


Alternative 2



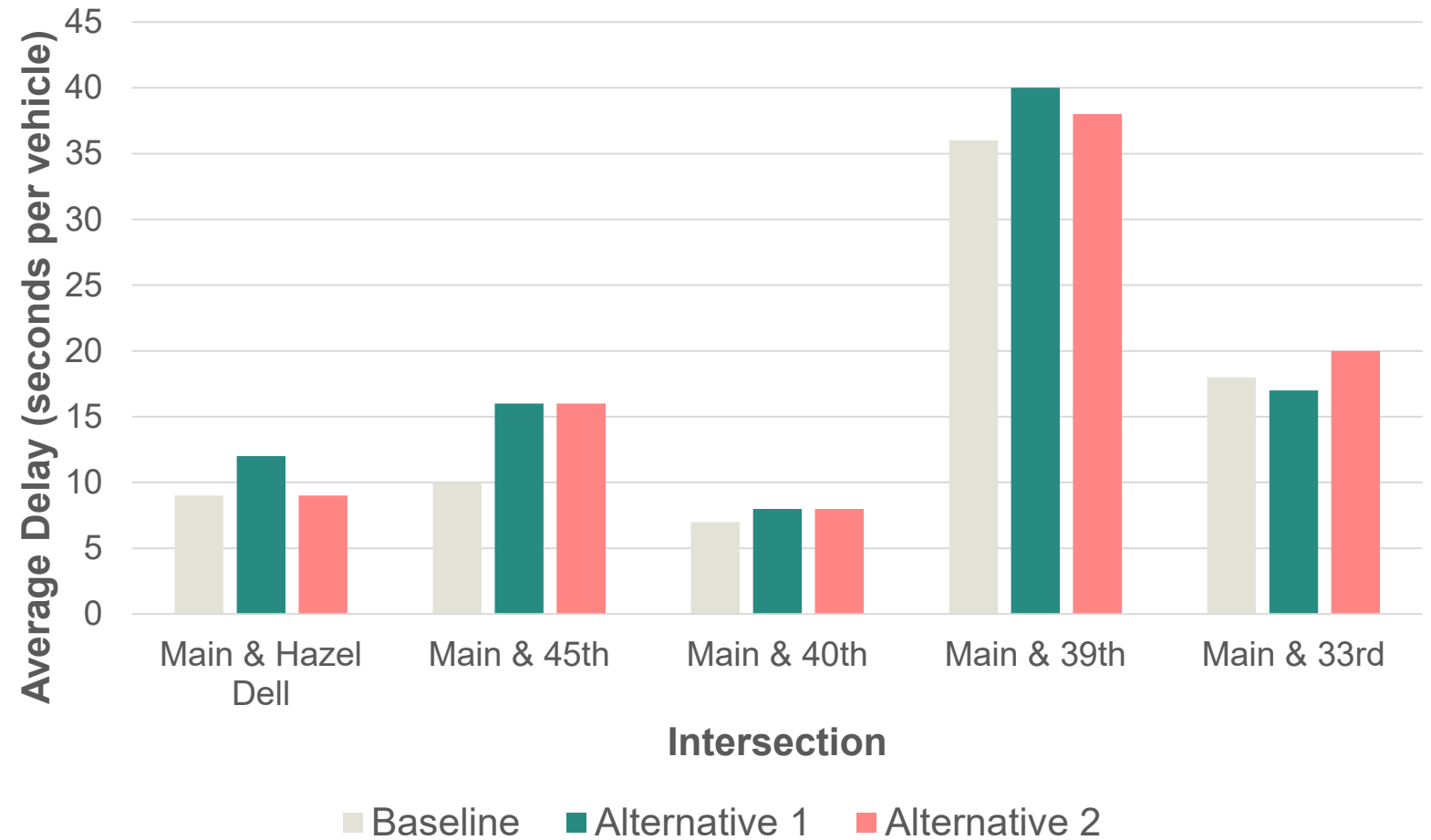
Intersection Delay – 2040 AM Peak

AM peak operations show some change at 40th with the BAT lane. 39th shows better operations due to southbound Main getting more green time.



Intersection Delay – 2040 PM Peak

Lane reduction + BAT lane somewhat affects 45th, as well as 39th. 33rd Street overall less affected.



Multimodal Connection Opportunity: 40th to Discovery Trail



- Compatible in both alternatives 1 & 2
- Connection type TBD (Phase 2)
- Would fill in gaps in the Bike/Small Mobility and Pedestrian Networks

Phase 2 – Complete Streets



Projects goals



Engagement Plan

Project Overview

Phase 2 Goals



Improve pedestrian safety by adding/upgrading crossings and filling sidewalk gaps that improve connections to transit

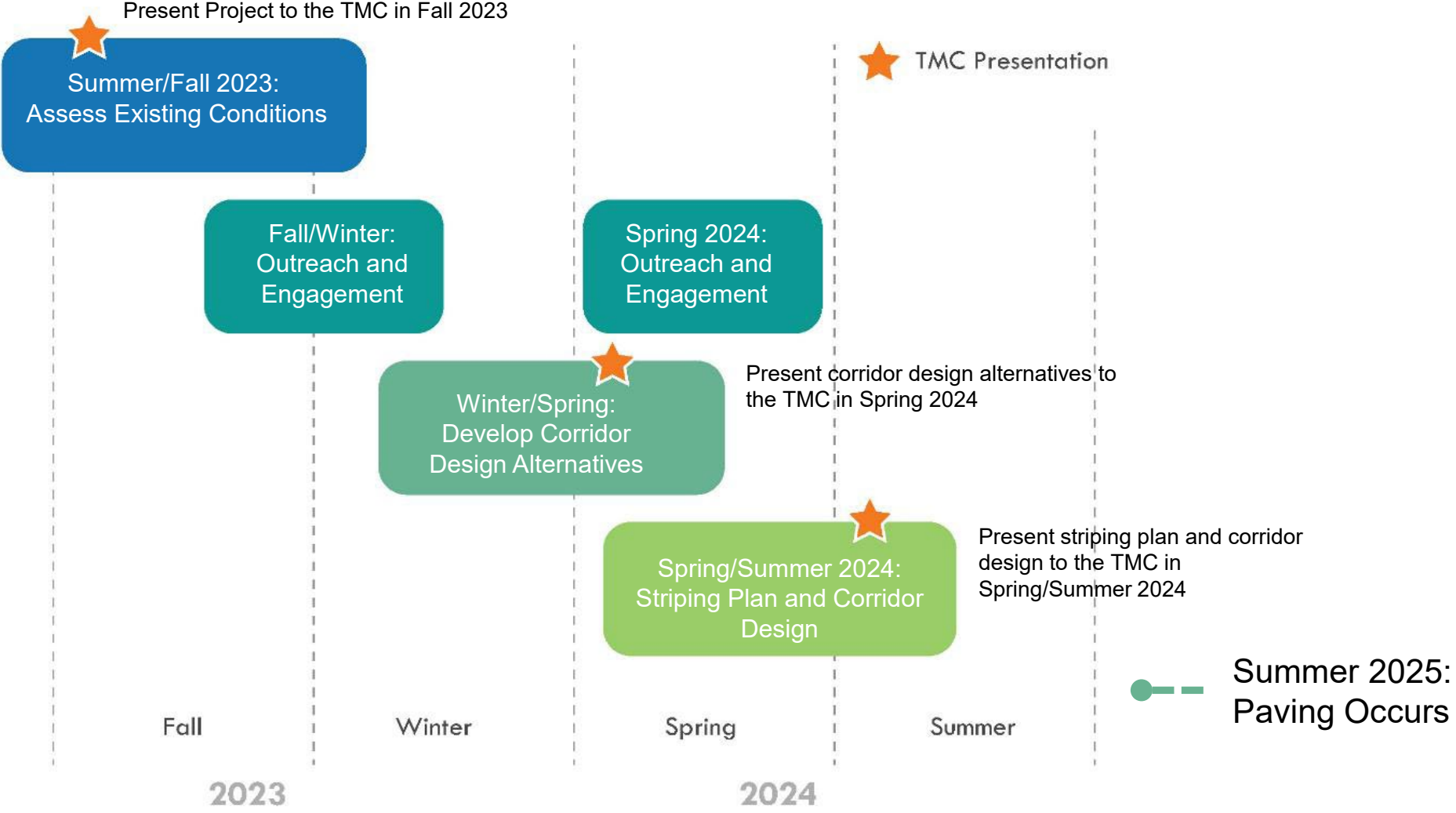


Improve bike and small mobility connectivity by addressing network gaps; connecting to regional trails



Explore transit speed and reliability treatments and improve transit stop access in anticipation for C-TRAN's Hwy 99 BRT project

Upper Main Street Safety & Mobility Project Timeline



Outreach

Timeline and Activities Overview

Fall/Winter 2023

- Develop project engagement plan
- Introduction of project to TMC
- Publish project website
- Create project materials such as a fact sheet and mailer
- Begin coordination with C-TRAN



Winter/Spring - 2024

- Conduct door-to-door canvassing effort
- Update project website
- Tabling events
- Open house and online survey
- Continue coordination with TMC and C-TRAN



Spring/Summer- 2024

- Update project website
- Tabling events
- Virtual public meeting
- Open house and online survey
- Continue coordination with TMC and C-TRAN



Summer - 2024

- Update project website
- Updates to C-TRAN
- TMC Hearing
- City Council briefing

Outreach

Audiences and key stakeholders

- Residents near the project corridor
- Business owners and employees near the project corridor
- Students, teachers, education support staff, SR2S coordinators
- Faith based organizations
- Neighborhood Associations
- Transit-dependent community members
- Topic-specific organizations (e.g. Vancouver Bicycle Club)
- Community members with disabilities
- Elders

Questions for TMC:

- What should our outreach boundaries be for reaching residents, businesses and employees?
- Are we missing any groups/are there specific organizations that are important to reach?
- Any other outreach methods to consider?

Thank You

To learn more, visit beheardvancouver.org/uppermain

