An aerial architectural rendering of a campus master plan, presented in a monochromatic blue color scheme. The rendering shows a central cluster of buildings, a large parking lot, and various green spaces. A road network is visible, including a main thoroughfare that curves around the central area. The background features rolling hills and a large body of water, possibly a lake or river. The overall style is clean and modern, typical of urban planning visualizations.

# HP GREENFIELD MASTER PLAN

NOVEMBER 3, 2020

# CONTENTS

## 01 INTRODUCTION

1.1 A Community Partner	6
1.2 Background	8
1.3 A Catalyst for Development in East Vancouver	10
1.4 Master Plan Purpose	12
1.5 Regulatory Context	14

## 02 AREA PLANNING

2.1 Vision for the HP Property	18
2.2 Site Grading and Elevations	20
2.3 Building Location and Orientation	24
2.4 Scale and Building Massing	28
2.5 Open Space and Plazas	32
2.6 Gateway Streets	34
2.7 Service Elements in the Context of Section 30 and Surrounding Zones	36

## 03 ACCESS, CIRCULATION, AND PARKING

3.1 Street Pattern and Layout	40
3.2 Street Design	46
3.3 Parking	52
3.4 Traffic Study Results	58
3.5 Traffic Demand Management Approach	59

## 04 PUBLIC FACILITIES AND SERVICES

4.1 Storm Water Facilities and Green Design	62
4.2 Regional Sanitary Sewer Facilities	64
4.3 Water, Electricity, Data, Telecommunications Routing and Connections	66

## 05 FULL SITE UTILIZATION PLAN AND PHASING

5.1 Full Site Utilization Plan	70
5.2 Phases of Development	72

## 06 LANDSCAPE PLANNING

6.1 Landscaping	78
6.2 Signage	86
6.3 Screening and Boundaries	88

## APPENDICES

Provided under separate cover:

Appendix A. Regulatory Compliance Matrix
Appendix B. Development Agreement
Appendix C. SEPA Checklist
Appendix D. Transportation Analysis
Appendix E. Drawings
L1.0 Vicinity Plan
L1.1 Full Site Utilization Plan
L1.2 Parking Plan
L1.3 Phasing plan
L2.0 Master Landscape Plan



# 01 / INTRODUCTION

## 1.1 A COMMUNITY PARTNER

HP Inc. (HP) has been an active member of the Vancouver (City) community for more than 35 years. Over the course of HP's tenure, thousands of people have been employed, raised their families, and become deeply invested in Vancouver community activities. HP's culture has fostered this deep involvement because the company understands its responsibility as a positive influence on communities where HP employees live, work, and play. HP's employees have contributed their time and energy to City of Vancouver committees and task forces, and held positions with local non-profits and community organizations. Community service is a value so deeply ingrained at HP that employees are offered paid volunteer hours and encouraged to apply for the Time-off Community Support Grant, which sponsors employees to volunteer full-time for a week.

In addition to time, HP offers donation matching for employees, grants for non-profit organizations, and sponsorship in their communities. In Vancouver alone, an average of 1,500 volunteer hours per year are contributed within the local community through HP's Community Outreach team in support of a range of nonprofits, including, most recently, Oregon and Clark County Food Bank, Columbia Springs, Humane Society of Southwest Washington, Fort Vancouver Regional Library, Avamere and Kamlu Resort, Vancouver Watershed Alliance, Firstenburg, Lifeline Connections, Washington Trails Association,

Evergreen Habitat for Humanity, Sharehouse, Camp Hope, and Global Watershed Shoreline.

HP protects the City of Vancouver's natural and social environment through its Site Council. The Site Council funds approximately \$20,000 annually for HP's Community Outreach team to organize and complete community improvement projects, along with sponsorships and donations to local events such as providing holiday giving trees. HP is also a PRIDE Rainbow supporter.

Furthermore, HP supports the next generation of diverse technology experts. Each fall, HP Vancouver employees vote for local nonprofit organizations to receive the HP Imagine Grant, supporting programs that align with the foundation's mission by focusing on education and technology related learning experiences for underrepresented and/or underserved communities. In 2019, three local charities were each awarded \$9,000 through this grant. Additionally, HP mentors and trains the next generation of innovators through its annual intern program, HP LIFE program, WSU Vancouver Capstone Project, iUrbanTeenPitch project, CAMAS Girls in STEM, and other programs.

HP is a corporate leader in environmental stewardship and has always focused on incorporating environmental sustainability in everything it does, including constructing sustainable buildings for HP sites in regions where HP operates.



FIGURE 1.A - A COMMUNITY PARTNER

HP's Corporate Real Estate and Workplace Solutions (CREWS) team is highly focused on reinventing the workplace as an environment that advances the wellbeing of its people and the planet. HP's industry-leading sustainability standards will be incorporated as a baseline along with additional focus on specific goals maximizing renewable resources and emission reduction. (SP UD-6).

For example, HP is working towards 100% renewable electricity by 2035; and 60% greenhouse gas reduction and 35% increase in water efficiency by 2025. (SP UD-5).

As the HP Vancouver site development is a multi-year project in its early stages, HP will continue to consider the various ways to achieve a technologically smart and green development over time while minimizing its environmental impact.

## 1.2 BACKGROUND

In 2018, HP reviewed its options for planned growth over the next 15-20 years for office, research and development, and other uses consistent with the Development Agreement, and determined the City of Vancouver an excellent location to expand its footprint. HP's deep community connection, developed over nearly four decades, gave the company confidence to continue to grow with the City of Vancouver as it moves forward to develop the last large swath of vacant land currently within the City's urban growth boundary in Section 30. (SP LU-1).

In 2009, the City of Vancouver set forth a vision to transform 553 acres of land historically used for aggregate extraction and processing into a new employment center / mixed-use neighborhood in the Section 30 Urban Employment Center Subarea Plan. Approximately 140 acres of Section 30 property is publicly owned by Clark County, the City of Vancouver, and the Bonneville Power Association. The HP Master Plan represents the first major development proposal within the Section 30 area. HP's property is comprised of 68 acres in the southwest corner of the undeveloped area located immediately north of the Columbia Tech Center (CTC), near the intersection of NE 184<sup>th</sup> Avenue and SE 1<sup>st</sup> Street. (SP LU-10).

This Master Plan will govern development of the HP property over the next 15-20 years and beyond. The Master Plan sets forth HP's vision of a multi-building development comprising up to 1.5 million gross square feet (GSF). Proposed uses will be similar to existing uses in HP's other Vancouver work spaces, including office, research and development, and other uses consistent with the Development Agreement, as well as the potential for occupant-supported small-scale retail, restaurant and accessory uses. (SP LU-13).

The HP property is zoned Employment Center Mixed Use (ECX). It is bordered on the west by residential neighbors, to the south by the English Farm winery and vineyard, and to the north and east by other aggregate extraction and processing sites and an asphalt plant. In order to prepare the Master Plan, HP incorporated input from neighboring Section 30 property owners to anticipate appropriate development to the north and east. Those assumptions are used to assess longer-term development of the entire plan area. (SP UD-1-2).

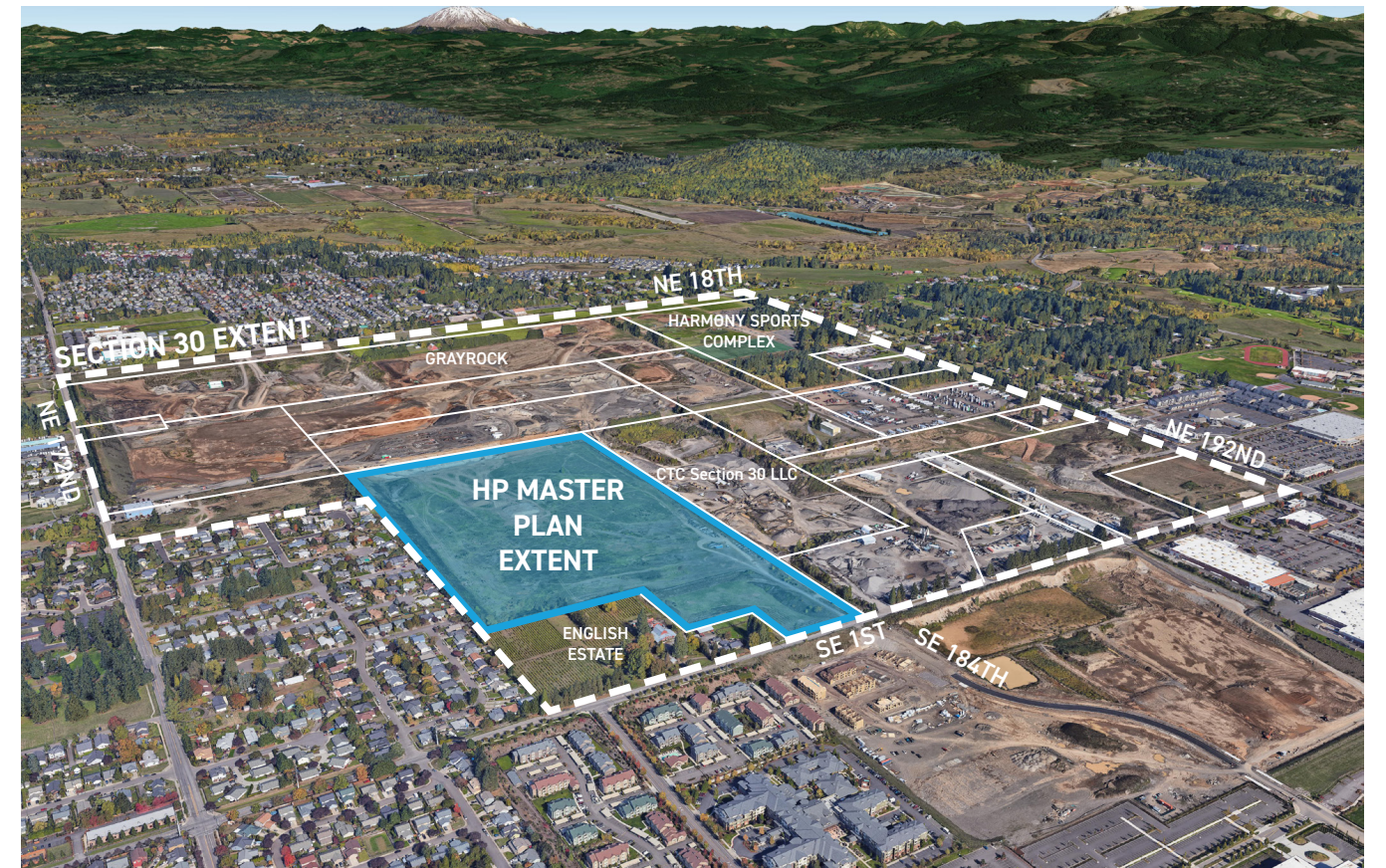


FIGURE 1.B - HP MASTER PLAN AND SECTION 30 EXTENT

### 1.3

## A CATALYST FOR DEVELOPMENT IN EAST VANCOUVER

The HP Master Plan and the development anticipated to occur over the next 15-20 years as a result of the plan's adoption will be a catalyst for investment and employment growth within the City. The HP property will be developed in phases to create a comprehensive new office and research development.

Section 30's status as a mostly undeveloped former aggregate extraction and processing area means that a new vision for infrastructure is required at the outset to ensure that development of the area can be served by necessary public services from utilities to a new multi-modal street grid. In this visioning process, HP has worked with project neighbors to identify Section 30 infrastructure needs. In addition, the Master Plan incorporates landscape design guidance, and other design elements to support plan approval. (SP LU-5).

HP and the City have initiated coordination of infrastructure planning for this portion of Section 30. On December 16, 2019, HP and the City entered into a Development Agreement related to HP's property to encourage development throughout Section 30. A copy of the Development Agreement is included as Appendix B to this Master Plan.

The City recognized the need to build Section 30 infrastructure to include a sanitary sewer pump station ("Area 2 Pump Station"), a public stormwater collection facility benefiting from the native gravels along the planned extension of 9<sup>th</sup> Street, looped water service, and a new road network. The City agreed to reinvest up to \$10,000,000 of City Reinvestment Funds towards Core Public Improvements. Examples of Core Public Improvements include collector and arterial streets, inclusive of all frontage improvements: sidewalks, roundabouts, landscaping, street lighting and water mains. Also included are public recreational trails, public transportation facilities, public stormwater collection facilities, public water facilities, and public sanitary sewer facilities and infrastructures related to each. (DA § 7(b)).

The City agreed that Core Public Improvements include the extension of 184<sup>th</sup> Avenue from HP's first phase to 9<sup>th</sup> Street, construction of 9<sup>th</sup> Street between 172<sup>nd</sup> Avenue and 184<sup>th</sup> Avenue, payment of System Development Charge fees (including the One Pacific Agreement related to fees), any developer-dedicated public recreational trail, and construction of the Area 2 Pump Station and related infrastructure as contemplated in the Subarea Plan. In addition, the City agreed to make Section 30 public infrastructure a top priority in the City's grant writing program and other public funding plans. (DA § 4(f), 7) (SP LU-6, UD-8).



**FIGURE 1.C - CONCEPTUAL AERIAL VIEW OF SECTION 30 PROPERTIES**

An aerial view of the Section 30 properties and their surroundings are shown looking northeast with the 68-acre HP property development illustrated in concept in the foreground. Roundabouts on NE 184<sup>TH</sup> Avenue are shown, with the principal entry to the property from SE 1<sup>ST</sup> Street shown bottom right and additional roundabouts at 3<sup>RD</sup>, 6<sup>TH</sup> and 9<sup>TH</sup> Streets.

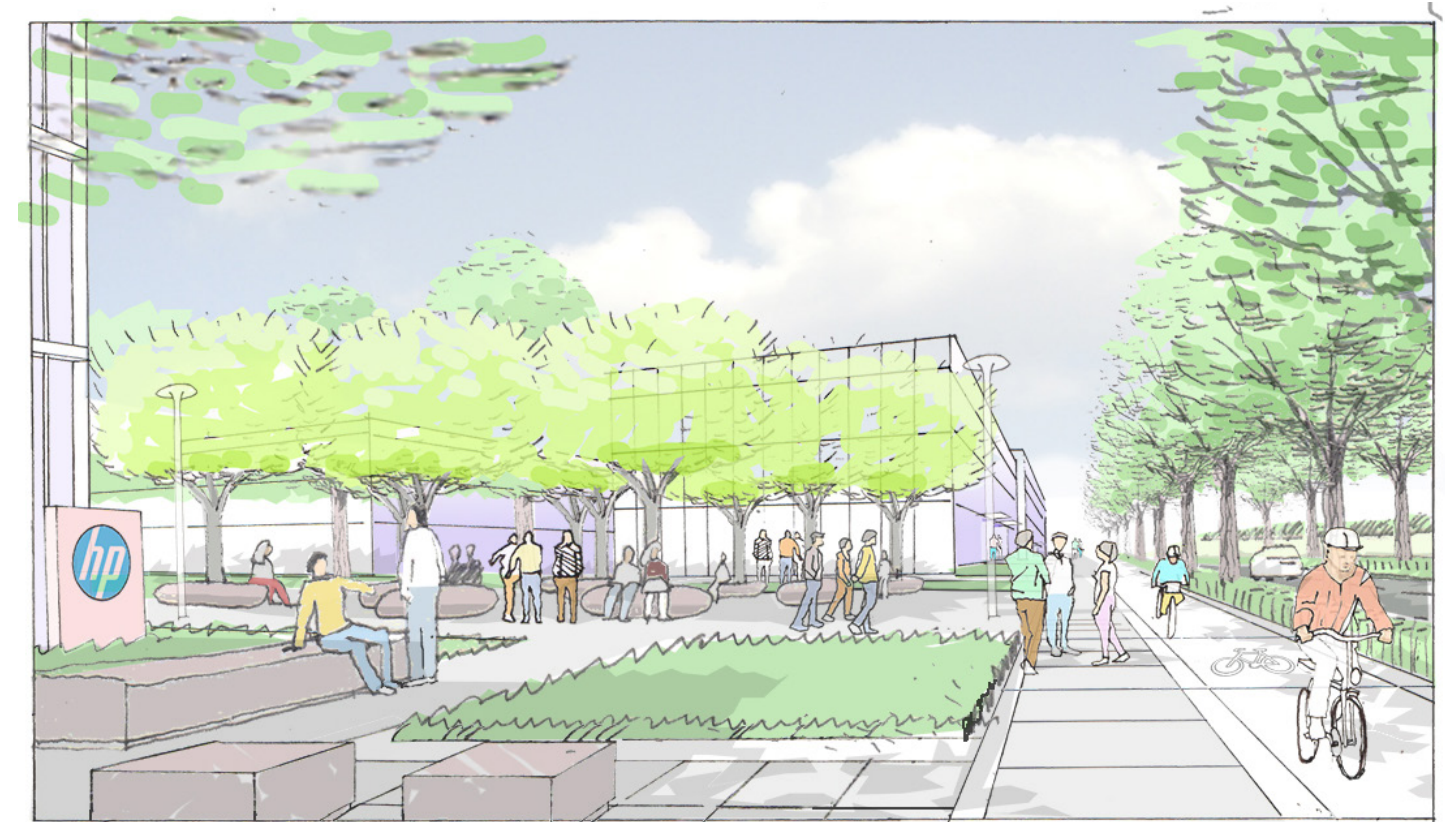
## 1.4 MASTER PLAN PURPOSE

The HP Master Plan is intended to set forth clear objectives for development of its 68 acres of property. The plan incorporates the City's visions, goals, and policies that are set forth in several Section 30 planning documents described in greater detail in Section 1.5 Regulatory Context. In addition, the Master Plan discusses phased development of the HP property and phased infrastructure plans to serve HP's uses and other surrounding Section 30 properties. Further, the plan takes into consideration surrounding uses, and long-term development of the entire plan area to analyze street and pedestrian connectivity, transitional grades between surrounding development sites, stormwater management, open space connectivity, utility service, and traffic impacts. (SP TR-10, MS-2-5).

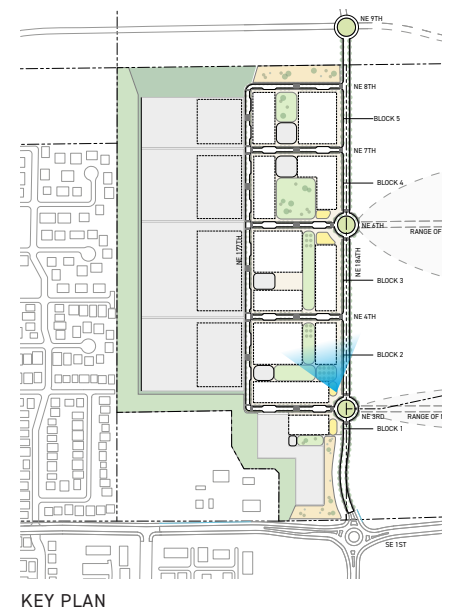
HP's future development will be subject to site plan approvals that are consistent with this Master Plan and the Development Agreement. In the Development Agreement, the City determined that Phase 1 of HP's development will involve construction of one or more buildings that will comprise up to 330,000 GSF of office, research and development, and other uses consistent with the Development Agreement, as well

as the potential for occupant-supported small-scale retail, restaurant and accessory space, and related parking and associated on site improvements that may be reviewed under the City's Type II process ("Phase 1"). The Master Plan incorporates specific infrastructure allowances for Phase 1 to allow the longer-term planning and construction associated with Section 30 area wide improvements to serve subsequent phases of development. (DA § 8(a)(i)).

Finally, since the Master Plan governs the development of the HP property over the long-term, the City's processes allow for refinement and the potential for modifications of the plan document. Graphic illustrations in this Masterplan are conceptual in nature. If refinement is necessary, HP will undertake the requisite review for such changes. Further, as identified in the Development Agreement, HP may purchase additional property in the future within Section 30 and in the company's discretion may, with the City's consent, annex such additional property into this Master Plan through future amendment. (DA §§ 4(c), 8(j)).



**FIGURE 1.D - CONCEPTUAL VIEW OF BLOCK 2 LOOKING NORTHWEST FROM NE 184TH AVENUE**



**KEY PLAN**

## 1.5 REGULATORY CONTEXT

The following documents provide specific direction for master planning in Section 30:

- › Section 30 Urban Employment Center Subarea Plan, 2009, presenting the vision and policies related to the future development of properties within the Section;
- › Section 30 Urban Employment Center Design Guidelines, 2009 (“Design Guidelines”), implementing the Subarea Plan vision;
- › Vancouver Municipal Code (“VMC”), particularly portions of Chapter 20.690, that govern the Master Plan requirements. (DA § 4(d)(vii)).

In addition, HP and the City of Vancouver entered into a Development Agreement on December 16, 2019 to vest HP to certain site-specific development standards, recorded in the City’s Development Standards binder. Within this regulatory context, HP provides this Master Plan to bring high quality development to East Vancouver marrying planning and design to create an attractive office, research and development (and other uses consistent with the Development Agreement), as a central economic development use within Section 30. (DA §§ 4(d), 26).

As observed in the Development Agreement, the above set of regulations anticipated development to occur on a parcel-by-parcel basis, instead of the coordinated effort led by HP’s catalyzing development involving a

significant portion of Section 30’s privately owned land. However, the Subarea Plan did anticipate flexibility in its application and is referred to as a framework that envisioned a joint effort for development between the City and property owners, and between HP and its neighbors. This Master Plan is the next step to turn the framework into a reality and transform Section 30 into the neighborhood envisioned in the Subarea Plan.

The content of this Master Plan is arranged to respond directly to topics as addressed in the Design Guidelines. Each section also refers to relevant portions of the Development Agreement, other documents listed above, and in the Appendices. A Regulatory Compliance Matrix has been prepared (included in the Appendix) that correlate specific sections of City Planning Documents, City Code, and the City of Vancouver-HP Inc. Development Agreement to associated sections within this Master Plan. The HP Master Plan will establish a site-specific path for development in Section 30 that creates a high quality built and landscaped environment that is both respectful to preexisting neighboring communities, and supportive of the new mixed-use community that will grow to the north and east of the HP property. HP uses parenthetical notations within this document to show where particular portions of the Master Plan address Subarea Plan Policies (referred to as SP), Design Guidelines (referred to as DG), the City code (referred to as VMC), and Development Agreement (referred to as DA).



**FIGURE 1.E - EXISTING CONDITIONS WITHIN SECTION 30**

Existing conditions within Section 30 including HP land (top), Harmony Sports Complex and English Estate Winery (middle) and vision for a public park from Section 30 Subarea plan (bottom).





# 02/ AREA PLANNING

## 2.1 VISION FOR THE HP PROPERTY

HP proposes a mix of office, research and development, and other uses consistent with the Development Agreement, as well as the potential for occupant-supported small-scale retail, restaurant and accessory space, and related parking and associated onsite improvements. These uses are consistent with ECX zoning of the site.

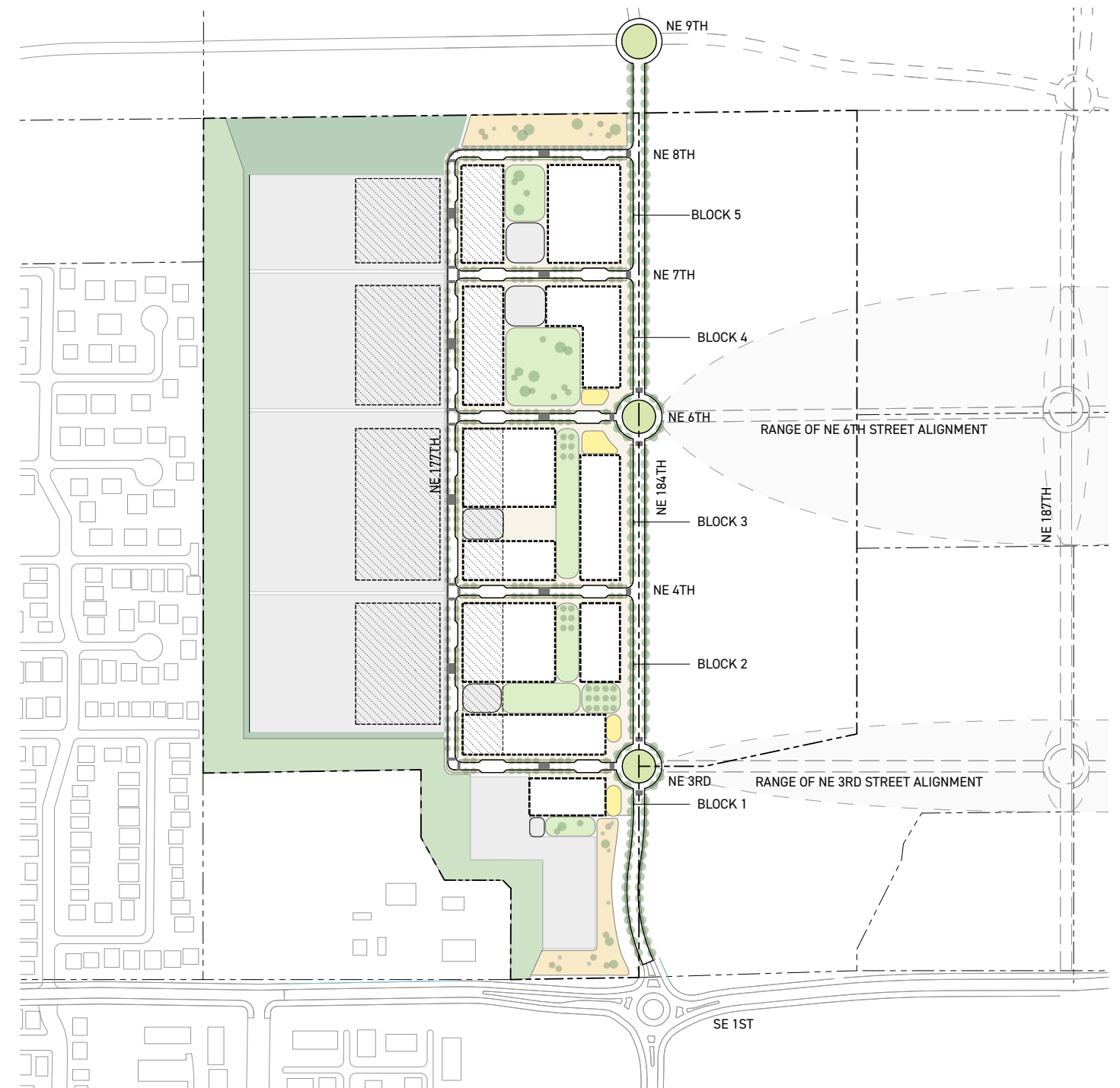
Development of the site will begin with Phase 1 at an improved entry point from SE 1<sup>st</sup> Street where newly constructed 184<sup>th</sup> Avenue will extend north to the future alignment of NE 3<sup>rd</sup> Street. Phase 1 buildings and parking will be located to the west of 184<sup>th</sup>, where parking areas will be screened from street view by a combination of elevation changes and landscaping. (DA § 4(b)).

Circulation, infrastructure and landscape improvements will be expanded as necessary with each phase of development. The Subarea Plan contains a concept for a public recreational trail along the western boundary of the HP property. The recreational trail location in the Subarea Plan is conceptual and allows for deviation to be determined through the master plan process. To meet the goals of the Subarea Plan, the recreational trail must be open to the public and must connect north to a location near 9<sup>th</sup> Street and west towards 172<sup>nd</sup> Avenue along or near the northern boundary of the HP property. To satisfy this Subarea Plan goal, a shared use recreational trail on the west side of 184<sup>th</sup> will be constructed as part of Phase 1 and will connect north to a location near 9<sup>th</sup> Street in subsequent phases. The western slope is not currently planned for use as fill material across the HP property. If the City funds an additional western perimeter trail, HP is willing to discuss an arrangement

for use of an area of potential dedication up to 12 feet in width from the western HP property line only on the top of the western slope for such recreational purpose. (SP LU-3, OS-1, OS-2, OS-9, OS-11, OS-14) (DG A.6.1.c)(DA § 8(d), Ex. 6, p. 3).

Banks at HP's south and west boundaries will be stabilized and landscaped, providing a natural buffer between site development and existing neighboring properties. The Full Site Utilization Plan (FSUP) shows the long-term configuration of the developed site. Development contemplated in this Master Plan is expected to take 15-20 years and possibly longer for the full 1.5 million GSF build-out, depending on market conditions and industry demands. Therefore, the FSUP, consistent with VMC 20.690.060.C, proposes potential sizes, locations, configurations and uses associated with full site build-out. (DA § 4(a)).

*Other Uses:* The ECX zoning provides for a mix of permitted, limited, and conditional uses that HP can develop on its property, and HP reserves the right to update the Master Plan in the future should other uses be desired. Notwithstanding the range of ECX allowed uses, the HP property shall not be developed with Group Living, Community Recreation, Park & Ride Facilities: Structure, Vehicle Fuel Sales, Rail Lines/Utility Corridors, or Uses listed under RCW 36.70B.170(3)(e). In addition, the VMC contemplates urban neighborhood overlays in Section 30. HP neither seeks application of an Urban Neighborhood Overlay, nor to include a residential component at this time. As a result, this Master Plan does not address residential use specific Subarea Plan policies and Design Guidelines.



**FIGURE 2.A - A VISION FOR THE HP PROPERTY**

This plan illustrates the general locations dedicated to buildings, open space and parking. Note the zone running north-south along NE 177TH may include either buildings or parking uses depending on HP's future space and area requirements.

LEGEND	
	POTENTIAL BUILDING
	POTENTIAL BUILDING OR PARKING
	PARKING AREA

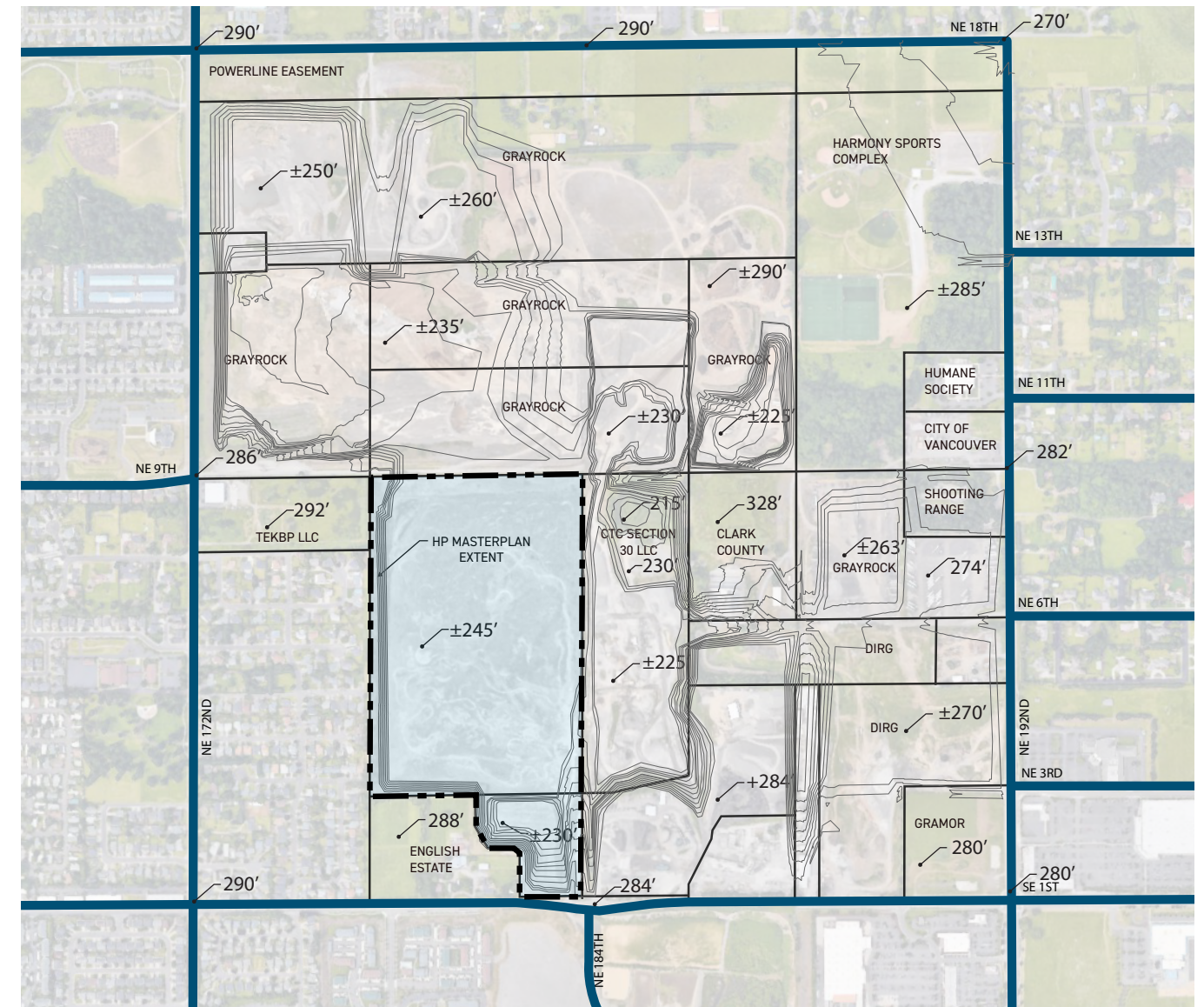
## 2.2 SITE GRADING AND ELEVATIONS

### EXISTING GRADES

The HP Master Plan takes advantage of post-reclamation elevations to use the valley features of Section 30 to create a sense of place and community through a well-connected street system with utilities and services and an environment conducive to bicycling and walking. HP has coordinated with its neighbors to ensure that grade transitions between development sites will create an interesting and cohesive development, while maximizing the developable area of HP and neighboring properties. Finished elevations are indicated on the Vicinity Plan, Sheet L.O. The remainder of Section 30 provides ample opportunities to accomplish transitional grades as development extends east and north. (SP GE-1-5).



The HP property is currently a quarry with variable floor elevations ranging from 234 to 246 feet. Surrounding elevations include the English Farm property to the south at an elevation of 288 feet, the CTC Section 30, LLC property to the east ranging from 215 to 230 feet, and property to the north ranging from 234 to 235 feet. Grades at the perimeter of the HP property will change little, except in the vicinity of 184<sup>th</sup> as it slopes down from its connection with SE 1<sup>st</sup> Street where it will straddle HP property and CTC Section 30, LLC property as it proceeds north to the 3<sup>rd</sup> Street roundabout. The roundabout at 184<sup>th</sup> and SE 1<sup>st</sup> Street will drop from an elevation of 284 feet to 262 feet.

HP intends to develop the site to prepare for finished grades between 2% and 4% across the Phase 1 property. The southernmost portion of the Phase 1 property has been filled and compacted to an approximate elevation of 246 feet, with gentle slopes to the west.



**FIGURE 2.B - EXISTING GRADES**

The HP property is 40 to 50 feet below the existing neighborhood to the west and current grade of SE 1ST Street.

LEGEND	
	EXISTING STREETS
	HP PROPERTY LINE

**FUTURE GRADES**

Extension of 184<sup>th</sup> Avenue north of SE 1<sup>st</sup> Street will occur during Phase 1 to provide transportation access to the HP property. The extension will be approximately 1,000 linear feet and HP will construct this stub road as a Core Public Improvement. The extension will serve the CTC Section 30, LLC property to the east of the HP property. The northward extension of 184<sup>th</sup> Avenue will slope down approximately 20 feet to access the Phase 1 property. Consistent with the Development Agreement, construction is to be coordinated with the City's improvements on SE 1<sup>st</sup> Street.

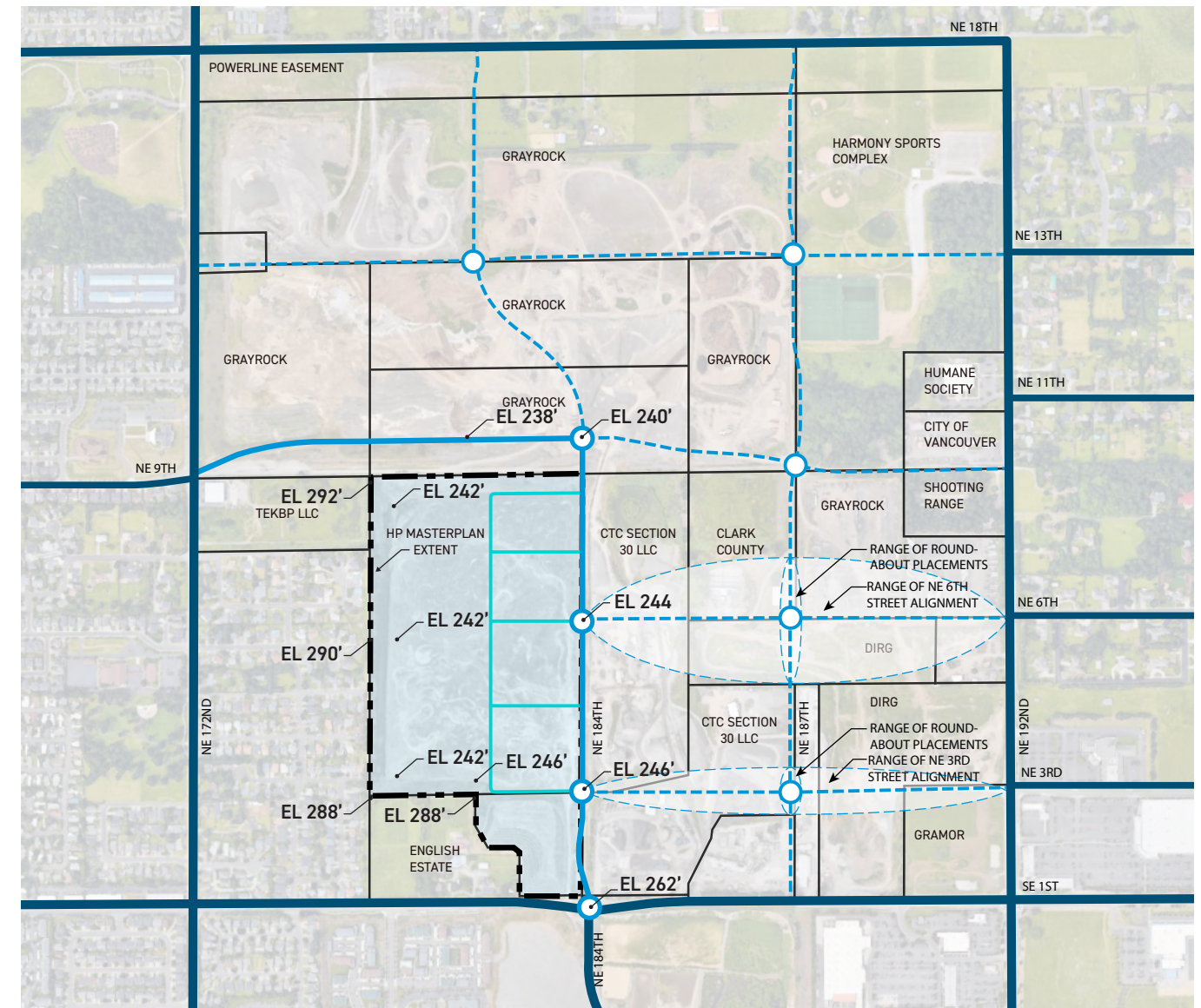
The floor of the remainder of the HP property is of variable elevation, and will be filled, leveled and compacted to a uniform elevation with appropriate grading for storm water drainage. Soils supercharging, as on Phase 1, will precede each phase of development. The proposed elevations and graded perimeter banks to the south and west will minimize the use of retaining walls, and provide landscaped slopes between building pads, pedestrian paths and streets. (SP TR-10).

Stormwater will be filtered through rain-gardens and other landscaping towards a groundwater recharge area near 9<sup>th</sup> Street, and included in the design of street improvements, sidewalks, and the recreational trail along 184<sup>th</sup> Avenue. The edge of former quarrying operations will be graded as 2:1 earthen banks along the south and west boundaries of the HP property. These will be landscaped for stabilization and appearance. The size of the HP property facilitates balancing of cut and fill materials across the site. (SP GE-1-5, OS-4, OS-5, OS-7) (DG A.1.1.a, b, d, e, A.1.2).

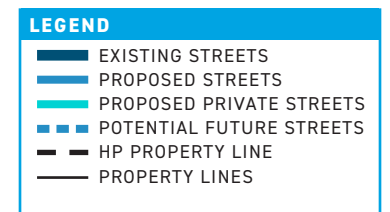
HP's property is substantially separated from properties to the west by a drop of 40 to 50 feet and by earthen banks that will be stabilized and landscaped. Both distance and the elevation difference, together with shade trees planted in the parking lots will attenuate noise and take into consideration the views for neighbors across the property.

The intersection of 1<sup>st</sup> Street and 184<sup>th</sup> Avenue will be the principal entrance to Section 30 from the southwest. It will also be the primary entrance to the HP property, with gateway landscaping following the gently sloping 184<sup>th</sup> topography towards Phase 1 building development and private street access near the intersection of 3<sup>rd</sup> Street. Subsequent HP development phases will continue 184<sup>th</sup> Avenue sloping gently north to 9<sup>th</sup> Street. (DG A.3.1.a-c) (SP TR-5, TR-6).

Finished grades will slope down approximately seven feet from the site of Phase 1 development to the intersection of 184<sup>th</sup> with NE 9<sup>th</sup> Street. This gentle slope will aid in directing stormwater towards ground recharge areas near 9<sup>th</sup> Street. The finished grades will also accommodate future extensions of 3<sup>rd</sup> and 6<sup>th</sup> Streets to the east, and development of property fronting the east side of 184<sup>th</sup> Avenue. Pedestrian and bicycle access to all parts of the developed Section 30 community are taken into consideration in these proposed finished elevations. Therefore, 3<sup>rd</sup> and 6<sup>th</sup> Streets, together with the multi-use recreational trail along 184<sup>th</sup> Avenue between 1<sup>st</sup> and 9<sup>th</sup> Street are being designed with usable slopes for multi-modal accessibility. (SP TR-10) (DA § 8(d), Ex. 6, p. 3).



**FIGURE 2.C - FUTURE GRADES**



## 2.3

### BUILDING LOCATION AND ORIENTATION

As visitors enter NE 184<sup>th</sup> Avenue a tree-lined street will open before them with a 12-foot wide recreational multi-use trail. HP's buildings along the west side of 184<sup>th</sup> Avenue will be set back 20 feet from the recreational trail. Access west from 184<sup>th</sup> between groups of buildings will be on private streets connecting to a tree-lined north-south street approximately parallel to 184<sup>th</sup> Avenue providing access to building entrances, screened service yards and west to parking lots shaded with trees bordering parking aisles and walkways. (see Master Landscape Plan, L.2.0) (DG A.2.1, A2.2.b).

The Full Site Utilization Plan summarizes how up to 1.5 million GSF of development previously defined will be constructed in phases. As HP plans each phase of development, a mix of uses will be determined to meet the company's employment space needs. The FSUP demonstrates a realistic assessment of future building types and footprint sizes, as well as parking space demand and accommodation of potential future increased density. (SP UD-4).

Section 30 has neither height limits nor floor area ratio (FAR) limits. However, HP recognizes the City's goal of achieving a distinctly urban form of development and compatibility with neighboring uses. Buildings will vary in height, providing for active and pedestrian scale street fronts through facade articulation and the inviting appearance of the new buildings. These building heights will take into consideration mountain views for residential neighbors to the west. Anticipated FAR at full build-out is 0.5 to 1.0. Boundary treatments will be compatible with surrounding uses and employ architecturally appropriate fencing. (DG A.2.2, E.2.1) (SP UD-1-3, UD-9) (VMC 20.690.040.B).

Building footprints depicted on the FSUP indicate the anticipated range of office, research and development, and other uses consistent with the Development Agreement, as well as the potential for occupant-supported small-scale retail, restaurant, and accessory space. Larger building footprints will accommodate research and development with limited office, while smaller building footprints will provide options for a mix of the other uses mentioned. The larger footprint buildings have the potential to be located on the interior of the HP property, with frontages along private roadways, allowing buildings with more office use as well as small-scale retail and accessory space to front 184<sup>th</sup> Avenue. (SP LU-4).

The Design Guidelines and VMC 20.690.040(D)(2) define maximum building setbacks from streets other than collectors and arterials, of 10 feet from the back of the sidewalk, with an exception allowed up to 20 feet. This will enable appropriate accommodation of pedestrian facilities and landscape. Consistent with the Development Agreement, this 10 foot minimum landscaped setback is measured from the back of the sidewalk, with the option to exceed the minimum to ensure architecturally sound design of landscaped setbacks taking into account factors including, but not limited to, pedestrian interface, vehicular circulation, and building design. Back of sidewalk also refers to back of a public recreational trail that may be co-located on a public rights of way or private street. (DA Ex. 4, p. 2) (VMC 20.690.040(D)(4)).

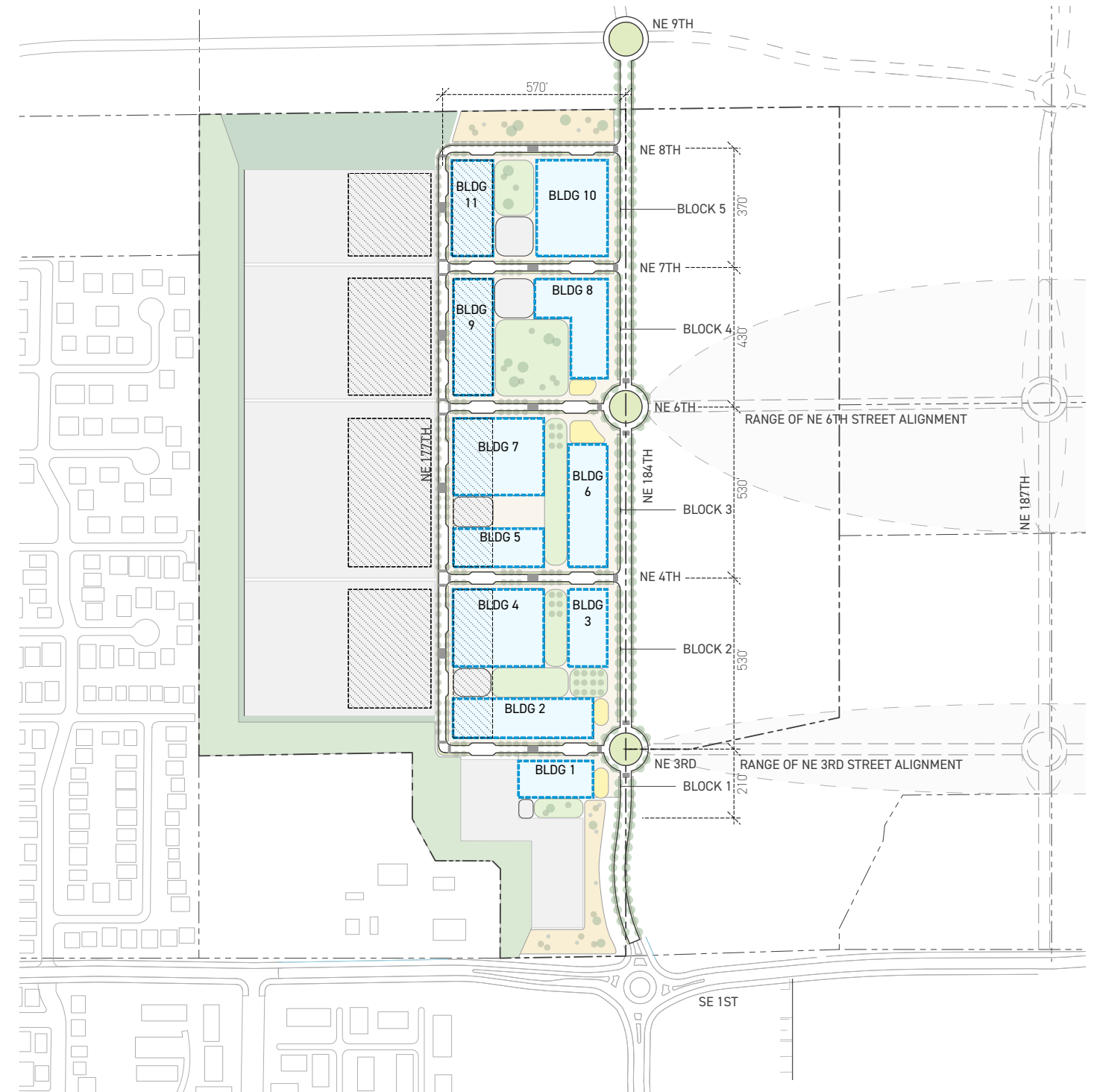
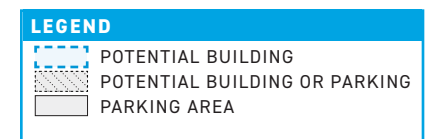


FIGURE 2.D - BUILDING LOCATION AND ORIENTATION



### BUILDING LOCATION AND ORIENTATION CONTINUED

Buildings will be arranged around open gardens and plazas, providing a secure environment for circulation, visitor access, employee rest and respite. Pathways will connect these spaces to sidewalks in surrounding streets as shown on the FSUP and phasing plans. A south-north footpath weaving through the gardens and plazas will connect facilities in all phases of development. (DG A.2.1.b, A.4.1, A.4.2) (SP LU-1, LU-10).

Streets encircle each phase of development, creating a series of city blocks. These blocks will vary in dimension based on the number and type of buildings and internal open spaces. Each block will have a distinctive character. Materials and design details will differ between buildings, although a "family resemblance" will present a unified appearance. The overall impression will be one of compact urban buildings interspersed with plazas and gardens bordered by tree lined streets. (DG D.1.1, D.1.2, D.1.3, D.1.4) (SP UD-13, UD-16).

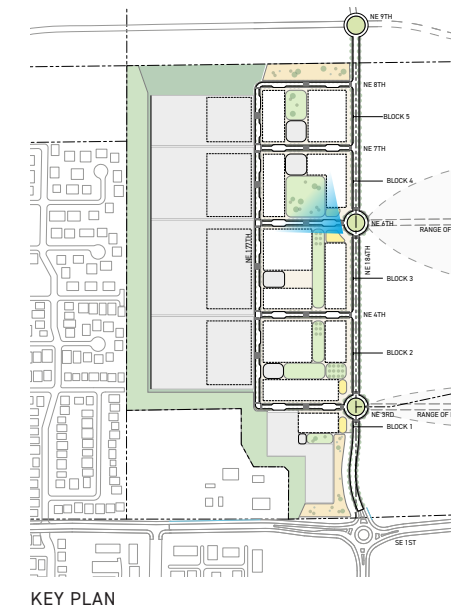
Parking lots west of the buildings will be partly screened by street trees, and partly by lines of shade trees that create separate parking areas. Occupants of taller buildings will look across tree canopies towards the landscaped banks at the western boundary. Trees will screen parked cars from most viewpoints. Services and utilities will be expanded as necessary to serve each new phase or block of development. (SP UD-12).

The Master Plan's proposed street alignments are based on the conceptual street layouts depicted in the Subarea Plan, and are also informed by HP's communication with surrounding property owners. Two roundabouts will be included between SE 1<sup>st</sup> Street and NE 9<sup>th</sup> Street, the first located on a potential extension of NE 3<sup>rd</sup> Street and the second aligned to a potential extension of NE 6<sup>th</sup> Street. SE 1<sup>st</sup> Street and NE 9<sup>th</sup> Street will become primary connections to neighborhoods to the west. Connections to the eastern properties in Section 30 will be via NE 3<sup>rd</sup>, NE 6<sup>th</sup>, and NE 9<sup>th</sup> Streets. Roundabouts calm traffic by maintaining slow and constant travel speeds, while minimizing the need for acceleration and breaking. Roundabouts will help make the streets safe for all modes of travel. Other traffic calming tools will be considered to maintain traffic flow and safety as design proceeds. (SP TR-2, 25, 26) (DG B.2.3.c) (DA Ex.6, p. 5) (VMC 20.690.040(E)).



**FIGURE 2.E - CONCEPTUAL VIEW OF BLOCK 3 AND BLOCK 4  
LOOKING NORTHWEST FROM NE 6TH STREET**

This illustration represents an anticipated relationship between built areas and open space and the connection from buildings onto the east-west private roadways.



## 2.4 SCALE AND BUILDING MASSING

Buildings will be designed to be compatible with one another in scale, orientation and massing, but will be expressive of different uses in their outward appearance. Variations in architectural details, materials and colors will be modulated to convey a "family resemblance" between buildings in each group or phase, but differences in appearance and grouping will create a distinctive identity for each phased block within the larger development, and will use a mix of details from the Design Guideline's details toolbox. The proposed scale and design will aid direction finding and will avoid the monotony that could otherwise dominate the 68-acre site. Details of the design will be directed principally by functional requirements and the wellbeing of building occupants – both inside and around the buildings. A footpath will weave through the open spaces in each block connecting all buildings in all phases. (DG 4.1.a-i).

Buildings will typically be urban in character thereby consistent with the recommendations of the Section 30 Design Guidelines. Buildings may be single or multi-story and configured to be compatible with the pedestrian-scaled spaces around them. Buildings will predominantly be a minimum of 24' in height. The design of buildings and open spaces will be coordinated to clearly define building entries, connecting pathways and other features. (DG A.2.2, D.2.1.a).

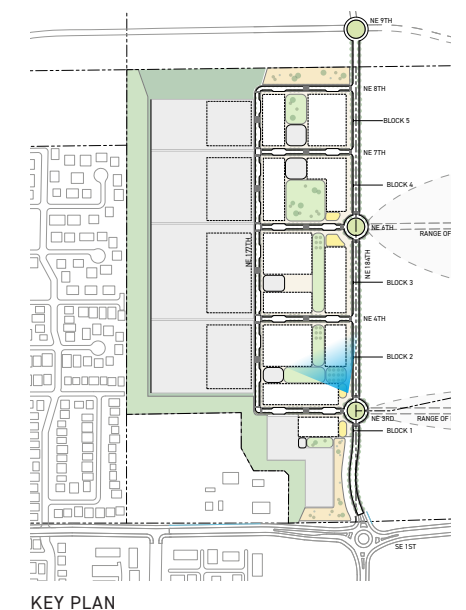
Any facade longer than 120 feet facing a public street will be interrupted with a setback, projection or other device at least 30 feet long and 20 feet deep for the full height of the building. Roof design will vary with building function but will be compatible between neighboring buildings. Rooflines visible from public streets longer than 100 feet will feature changes in profile or other features consistent with articulation of the facade. Blank walls will generally be avoided in locations visible from public streets and pedestrian routes. In cases where such walls are unavoidable, architectural features or landscape devices will be used to soften their appearance and harmonize with adjacent building details and open space features. (DG D.2.2, D.2.3, D.2.6, D.3.1).

Sustainability and longevity will influence the selection of architectural materials and finishes consistent with HP's sustainability goals. Architectural and landscape details will emphasize human scale, especially at building entries and around pedestrian walkways and seating areas. Signage, walls, fences and other landscape features will be compatible in scale and appearance with the architecture. Colors will be coordinated. Masonry or other durable materials will be used below ground floor windowsill levels. (DG D.5.1, D.5.2) (SP UD-6).



**FIGURE 2.F - CONCEPTUAL VIEW OF BLOCK 2 LOOKING NORTHWEST FROM NE 184TH AVENUE**

This illustration shows the potential relationship between buildings and internal open space with 184TH and the recreational trail running along the west side of the road. Buildings are anticipated to be scaled and massed such that they are able to frame open space and create a strong frontage along 184TH while also preserving a human scale and pedestrian orientation.



KEY PLAN

**PROVIDE USABLE OPEN SPACE BETWEEN BUILDINGS**



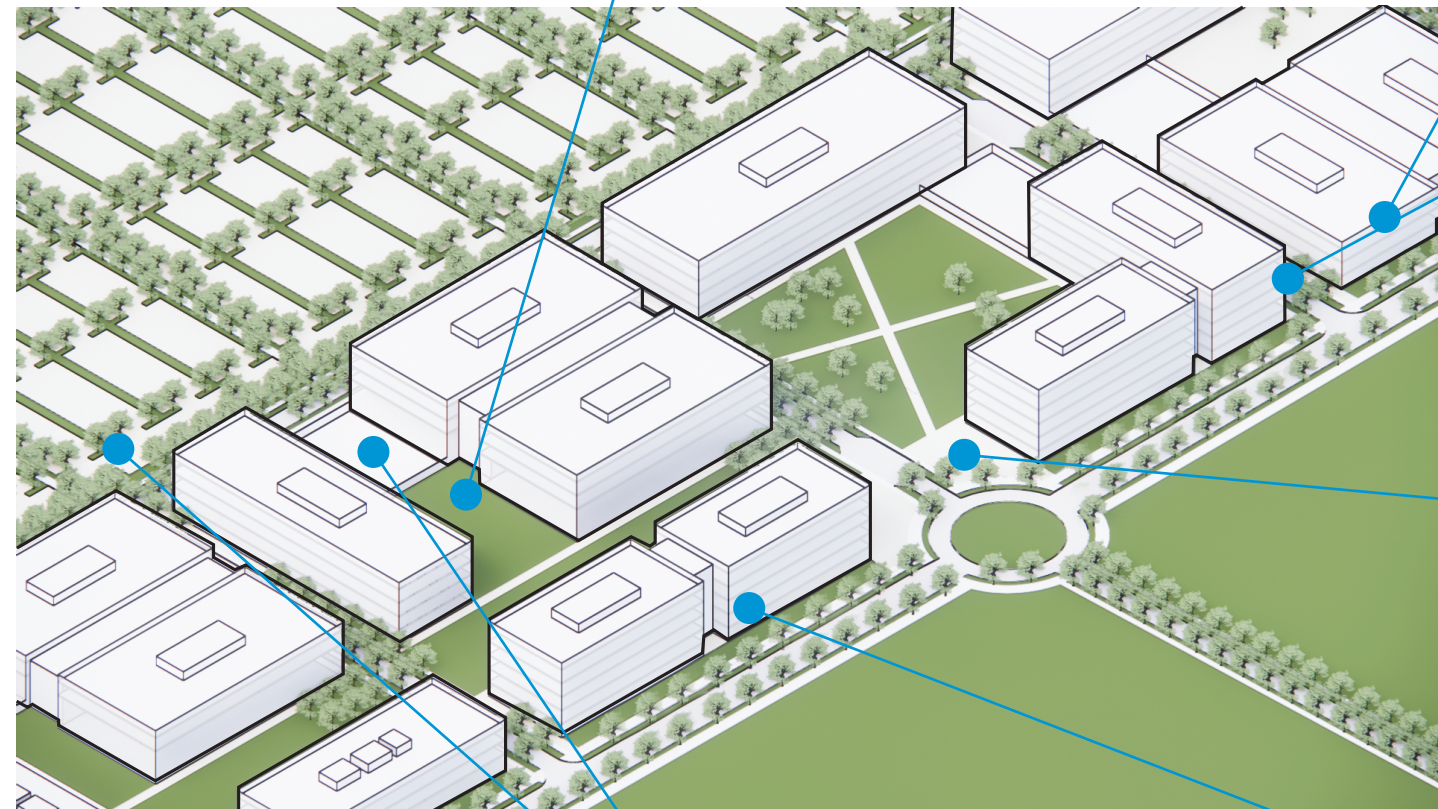
**CREATE A CONSISTENT STREET WALL ALONG 184TH**



**ADD LOCAL CONNECTIONS ALONG PRIVATE ROADWAYS**



**UTILIZE OPPORTUNITY FOR OPEN SPACE AT ROUNDABOUTS**

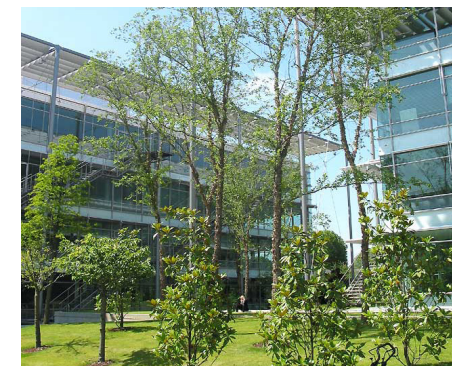


**FIGURE 2.6 - BUILDING MASSING CONCEPTS**

**ENSURE SERVICE AND PARKING AREAS ARE SEPARATED**



**BREAK DOWN SCALE OF BLOCK WITH ENTRIES AND THROUGH BLOCK PEDESTRIAN CONNECTIONS**





## 2.5 OPEN SPACE AND PLAZAS

Buildings will be interspersed with open spaces and plazas providing occupants with various outdoor opportunities for rest and recreation. Seating areas and informal gathering spaces will be designed to address building occupant needs for open space amenities and to provide welcoming entrances to buildings. Plazas will be designed for solar access and may include raised planters. The plazas and site open space will connect to the sidewalk network, private streets and the multi-modal recreational trail along 184<sup>th</sup> Avenue. (DG A.4.1, A.4.2).

Former quarry slopes between the site and properties to the south and west will be graded to an incline of 2:1 and will be planted to provide a green backdrop to the HP development. These slopes will offer a visually pleasing buffer to neighboring properties. The banks will only be accessed for periodic maintenance and will drain into a recharge zone along the toe of each slope. Slopes near the intersection of NE 184<sup>th</sup> Avenue with SE 1<sup>st</sup> Street will be reconfigured to achieve ADA compliant gradients and to provide an appropriate gateway landscaping at this principal entry to the site and to Section 30. (DG A.6.1, A.6.1.b).

Trees will line 184<sup>th</sup> Avenue and the private access streets to the west. Shade trees will line walkways throughout the parking areas and will also be used to separate parking aisles as shown on the Master Landscape Plan, Sheet L.2.0. These plantings will help to reduce heat island effects and improve air quality as well as add to the City's tree canopy cover. (DG E.1.4).

Entry to the site from the intersection of SE 1<sup>st</sup> Street and NE 184<sup>th</sup> Avenue will be marked by a monument sign and gateway landscaping. Proceeding north along 184<sup>th</sup> Avenue, visitors will see a sequence of open spaces between and beyond buildings forming a defining edge to the street. The multi-use recreational trail on the west side of 184<sup>th</sup> Avenue, and symmetrical sidewalks on the east side of 184<sup>th</sup> Avenue will connect the HP development to the network of neighborhoods, parks and other features planned for Section 30 development as the HP property extends north over the planning horizon. A dedicated, heritage monument marker will be located at the NW corner of the 3<sup>rd</sup> Street roundabout. (DG B.5.2) (DA § 6(e)).

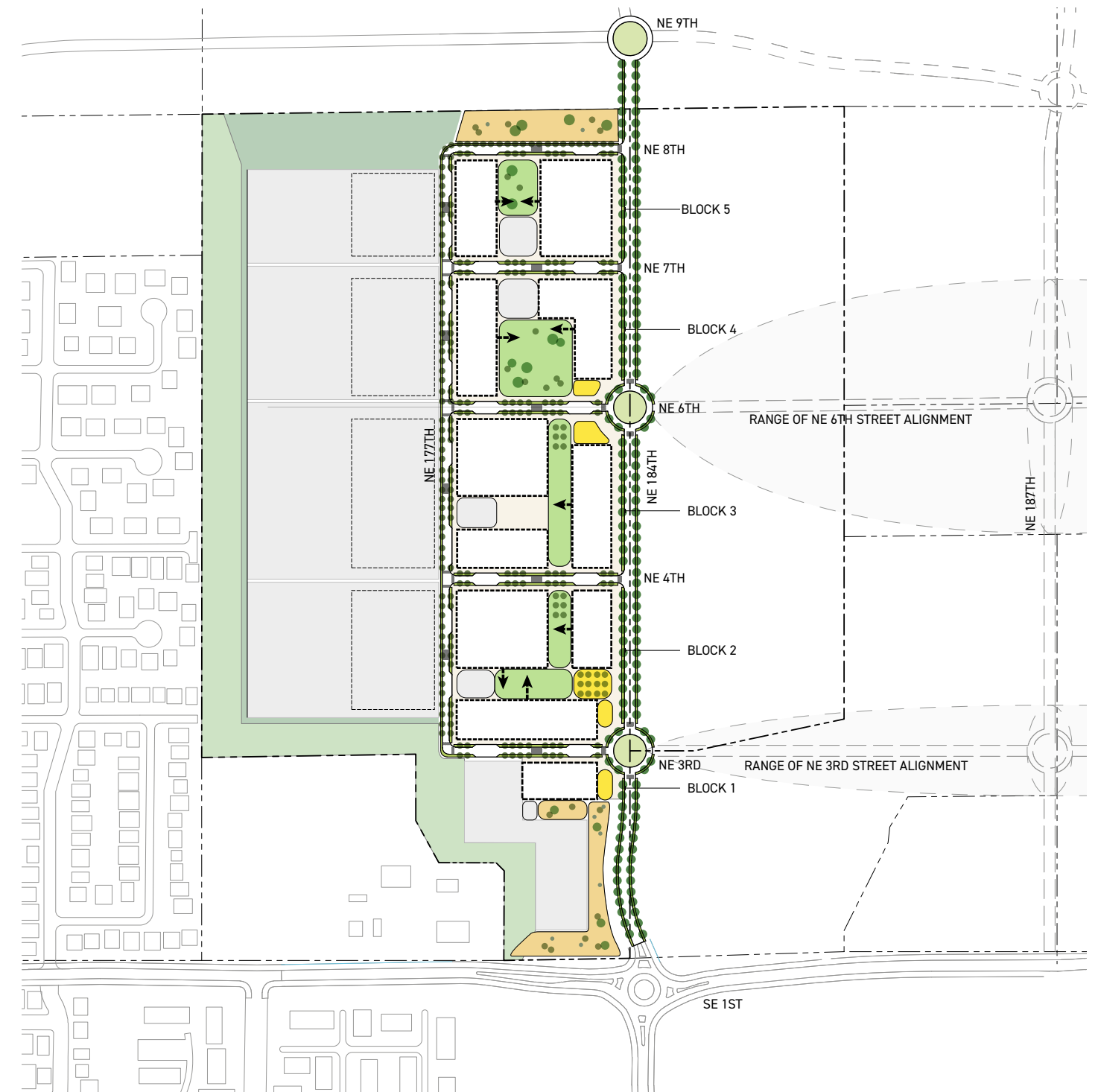


FIGURE 2.H - OPEN SPACE AND PLAZAS

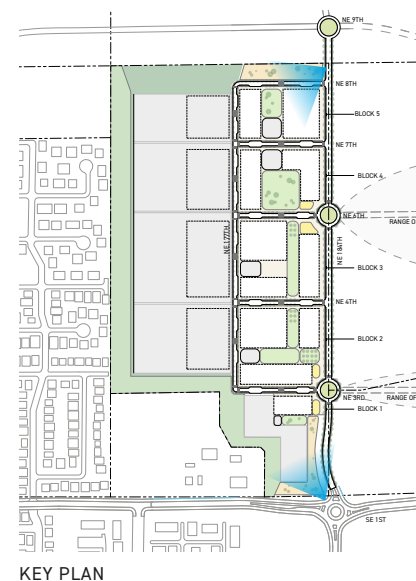
## 2.6 GATEWAY STREETS

The HP property is situated between two gateway entrances to Section 30 – the intersection of SE 1<sup>st</sup> Street and 184<sup>th</sup> Avenue, and NE 9<sup>th</sup> Street and 184<sup>th</sup> Avenue. Arrival at Section 30 from the south and west will be via SE 1<sup>st</sup> Street through a roundabout connection to NE 184<sup>th</sup> Avenue, sloping down to the HP property and connecting to the employment mixed-use community that will occupy the balance of the 553 acres comprising Section 30. Both entries will be signature gateways identified by HP monument signs. The City may incorporate area-wide identification as a public art piece within the 1<sup>st</sup> and 184<sup>th</sup> roundabout in connection with the work and effort of the City's Culture, Art and Heritage Commission, and subject to available City funding. (DG A.3.1.a-c).

The extension of 184<sup>th</sup> to be constructed as part of Phase 1 will curve slightly as it descends from 1<sup>st</sup> Street, and arrivals will emerge from gateway landscaping into a tree-lined street with a 12-foot wide recreational multi-use trail. Landscaping at the corner of 1<sup>st</sup> Street and 184<sup>th</sup> will incorporate predominantly native and drought resistant plant materials. The Phase 1 extension will end at the 3<sup>rd</sup> Street and 184<sup>th</sup> Avenue roundabout location with a short northern extension for fire and life safety vehicles. Through the trees on the west side of 184<sup>th</sup> Avenue, pedestrians and other visitors will see urban scaled buildings, and view a dedicated monument marker describing the heritage of the site on the NW corner of the roundabout at 184<sup>th</sup> Avenue and 3<sup>rd</sup> Street. (DG A.2.2) (SP UD-10).

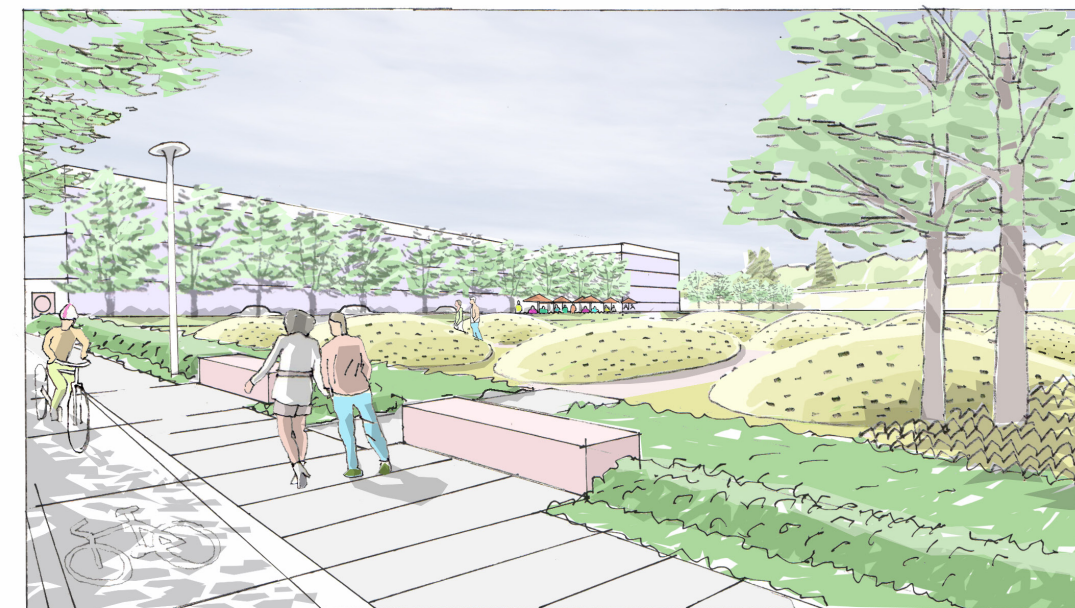
The approach to the HP property from the north will present a different gateway experience, since grade changes will be minimal, and views will be open. Here too, the formality of the straight, tree-lined street will be evident, its west edge contained by the frontages of urban scaled buildings. A monument sign will identify HP at the northern end of its property, where landscaping will favor predominantly native and drought resistant plant materials to establish continuity between phases of development.

Directional signage and lighting along 184<sup>th</sup> Avenue will be coordinated to the needs of those on foot or bicycle as well as drivers, and in coordination with C-Tran to meet public transit requirements. (DA Ex. 6, p. 5).



**FIGURE 2.I - CONCEPTUAL VIEW OF SOUTHERN GATEWAY LOOKING NORTH FROM SE 1ST STREET**

Southern gateway with roundabout on SE 1ST Street marking the principal entrance to the HP property via 184TH Avenue. Gateway landscape and a monument sign will announce the entrance to the HP property.



**FIGURE 2.J - CONCEPTUAL VIEW OF NORTHERN GATEWAY LOOKING SOUTH FROM NE 184TH AVENUE**

Northern gateway with stormwater facility and a potential for topographic landscaping.

## 2.7

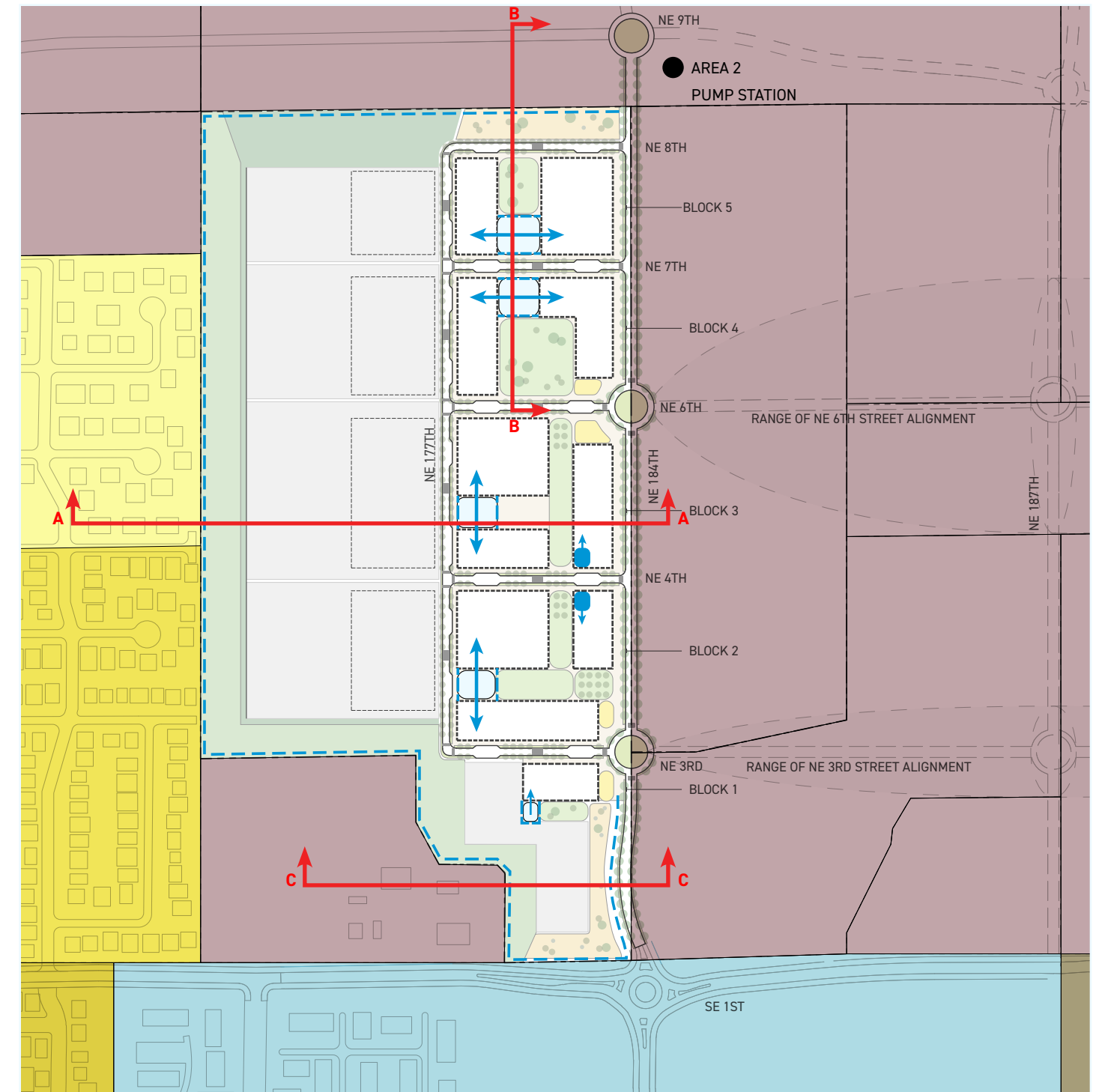
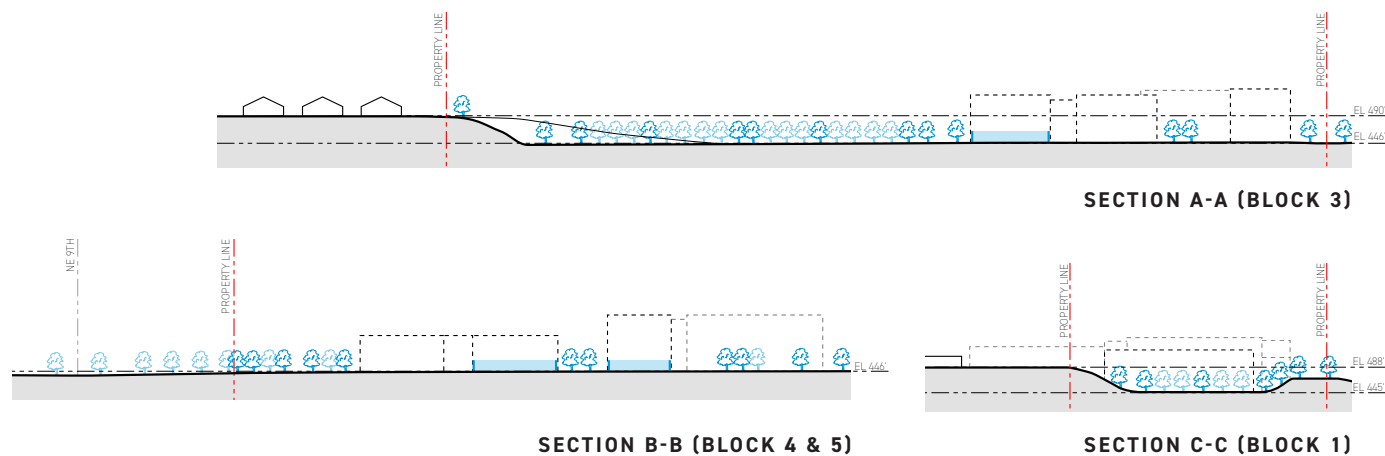
### SERVICE ELEMENTS IN THE CONTEXT OF SECTION 30 AND SURROUNDING ZONES

Service traffic and parking access will be via private streets west of 184<sup>th</sup> Avenue. The service elements on HP property will be connected through roundabouts at 3<sup>rd</sup> and 6<sup>th</sup> Streets, and at three intermediate right-in/right-out junctions; two located between the 6<sup>th</sup> and 9<sup>th</sup> Street roundabouts and one between the 3<sup>rd</sup> Street and 6<sup>th</sup> Street roundabouts.

The roundabouts will be designed to accommodate large freight vehicles and will include pedestrian crossings. Service areas are located on the west, north and south side of building groups to screen them from 184<sup>th</sup> Avenue and maintain the pedestrian scale development along this collector street. All service areas, including garbage and recycling areas, will be screened from view by a six foot high wall or fence, have self-closing doors, and will be accessible to large vehicles. Service areas will be consistent in design with

the architecture of the buildings served, using similar materials, colors and detailing. Three foot wide planting strips will separate service enclosures from the sidewalks. Roof mounted equipment will be screened from public view by designing such equipment structures as part of the building architecture, and designed and placed to reduce intrusive noise. Utility boxes and meters will be accessible from the private street on the west side of the buildings. (DG A.7.2.a-c, A.7.3, A.7.4, A.7.5) (SP UD-15).

The Master Plan recognizes residential and agricultural land-uses adjacent to the project and seeks to minimize impact on these uses. Specifically, care will be taken to consider the impact of the Master Plan on the views to Mt. Hood from homes adjacent to the quarry slope on the west side of the property. (DG A.6.1.a).



**FIGURE 2.K - SERVICE ELEMENTS AND SURROUNDING ZONES**

Service yards will be accessed from private roadways and will be screened, as will service meters and other utility equipment.

LEGEND			
	SERVICE ACCESS		EMPLOYMENT CENTER MIXED-USE (ECX)
	SERVICE YARDS		SINGLE FAMILY RESIDENTIAL (R1-10)
	INTERNAL SERVICE		SINGLE FAMILY RESIDENTIAL (R1-6)
	SCREENING		LOW DENSITY RESIDENTIAL (R-9)
			LIGHT INDUSTRIAL (IL)
			MIXED USE (MX)



# 03/ ACCESS, CIRCULATION, AND PARKING

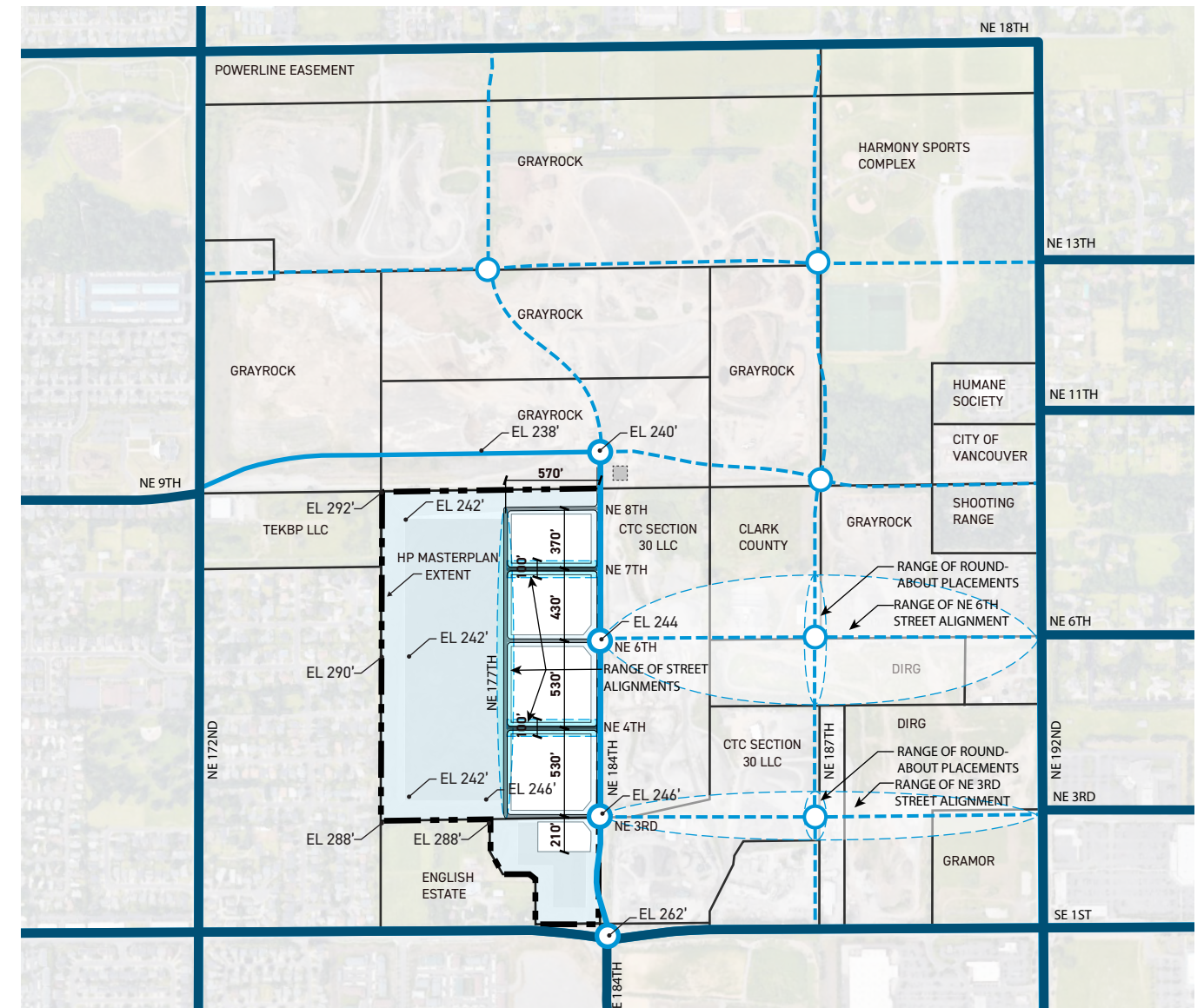
## 3.1 STREET PATTERN AND LAYOUT

The location and alignment of streets on and adjacent to the HP property are based on the conceptual locations in the Subarea Plan and discussions with impacted landowners within Section 30. The street connections provide the framework for the planned pedestrian oriented employment center, linking employees to commercial and residential neighborhoods elsewhere in Section 30. Frequent points of access from HP buildings to 184<sup>th</sup> Avenue will encourage walking and bicycling rather than automobile use for local trips. Bicycle lockers and associated facilities will be located within each group of HP buildings. (DG B.3, B.5.1.c-d).

Initially, access to the HP property will be located at the intersection of the arterial, SE 1<sup>st</sup> Street and NE 184<sup>th</sup> Avenue. A roundabout aligned with SE 3<sup>rd</sup> Street provides access to the private street network on the HP property and local street network to the east.

These will be the first elements of a district-wide system of streets, recreational trails and pathways favoring multi-modal transportation, inclusive of connectivity to public transit options. HP is not required to locate a transit facility on HP property. Nonetheless, the Master Plan will accommodate transit stops on or near arterial streets or local streets in coordination with C-Tran. 184<sup>th</sup> is located on HP and CTC Section 30, LLC properties. The northern portion of 184<sup>th</sup>, the 184<sup>th</sup>/9<sup>th</sup> St roundabout, and 9<sup>th</sup> Street from 184<sup>th</sup> to 172<sup>nd</sup> resides on Grayrock property. CTC Section 30, LLC and Grayrock agree to the roadway alignments and will coordinate grading plans to accommodate the streets and utilities. (SP TR-2, TR-3, TR-27, TR-28, PU-6) (DG A.1.1.c, B.4.2) (DA § 8(b)).

Truck routes off of 172<sup>nd</sup> Avenue from 9<sup>th</sup> Street to 184<sup>th</sup> Avenue and 1<sup>st</sup> Street to 184<sup>th</sup> Avenue will allow deliveries to the HP property. Today most of HP's deliveries use City-trucks, although other operations that may be located on the HP property may require travel truck deliveries. The design of 1<sup>st</sup> Street and 184<sup>th</sup> Avenue will accommodate a variety of truck sizes. (SP TR-23) (DA Ex. 6, p. 4).



**FIGURE 3.A - STREET PATTERN AND LAYOUT**

The illustration shows anticipated street layout and hierarchy of roadways. Note potential variation in roadway alignment indicated by filled regions above. In addition, there is a range of alignments anticipated for NE 4<sup>th</sup> Street, NE 7<sup>th</sup> Street, and NE 177<sup>th</sup> Avenue.

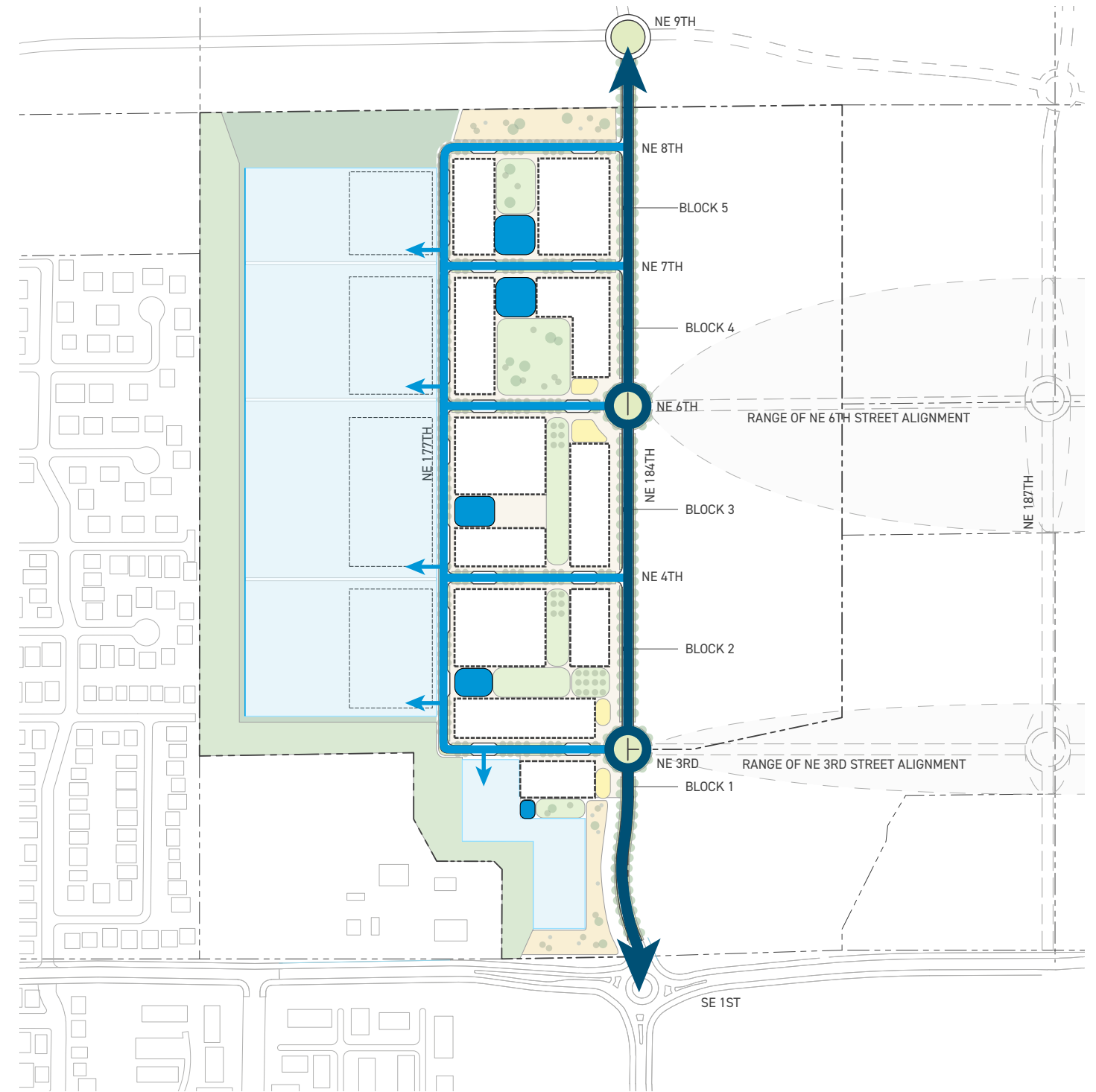


### SITE CIRCULATION

In anticipation of future Section 30 development east of 184<sup>th</sup> Avenue, roundabouts will be constructed at alignments approved for NE 3<sup>rd</sup> and 6<sup>th</sup> Streets. These roundabouts will lead to private access streets between HP's Phase 1 and Phase 2+ development and to parking and other facilities serving the buildings. The distances between the north-south streets varies between 370 feet and 530 feet, with intermediate pedestrian access between buildings, forming an irregular grid for convenient access by pedestrians and bicycle. HP reserves the right to reduce minimum separation consistent with the Development Agreement to 200 feet; any deviation in the range between 200 and 600 feet is subject to a City of Vancouver exception. (DG B.1.1.a-c, B.2.1.a, B.4.1) (SP TR-7-8, 20, UD-11) (DA Ex. 4, p. 4).

Consistent with the Development Agreement, maximum block face length shall be 600 feet generally, with the exception to allow maximum block lengths up to 700 feet; while Phase 1 does not currently intend to exceed a 600 foot block length, any potential lengths exceeding 600 feet will be reviewed as part of the site plan review. (DA Ex. 4, p. 4).

As the main collector street through the HP property, 184<sup>th</sup> Avenue will accommodate one vehicular travel lane in each direction, as well as a recreational multi-use trail on the west side of the street, and symmetrical sidewalks on the east side of the street. Each of these pathways will be separated from the roadway by a landscape strip planted with street trees that will not require irrigation once they are established. In addition to aesthetic appeal, the trees will reduce thermal stresses on the roadway and will improve air quality. On-street parking will not be permitted on 184<sup>th</sup> Avenue between 1<sup>st</sup> Street and 9<sup>th</sup> Street to maintain pedestrian safety, and ensure safe and efficient use of the road by vehicles. Pedestrian facilities will include crosswalks installed at the roundabouts, and if necessary to serve connectivity, mid-block pedestrian crossings may be considered as 184<sup>th</sup> Avenue extends north to SE 9<sup>th</sup> Street beyond Phase 1. (DG B.2.1, B.2.3.a, B.2.3.b-c) (SP TR-11-13, 17) (DA Ex. 6, p. 3) (VMC 20.690.040(H)).



**FIGURE 3.B -VEHICULAR NETWORK**

Illustration of vehicle routes and road hierarchy as well as parking and service yard locations.

LEGEND	
	PRIMARY STREET
	PRIVATE STREET
	PARKING ACCESS
	SERVICE YARDS
	SURFACE PARKING
	POTENTIAL BUILDING

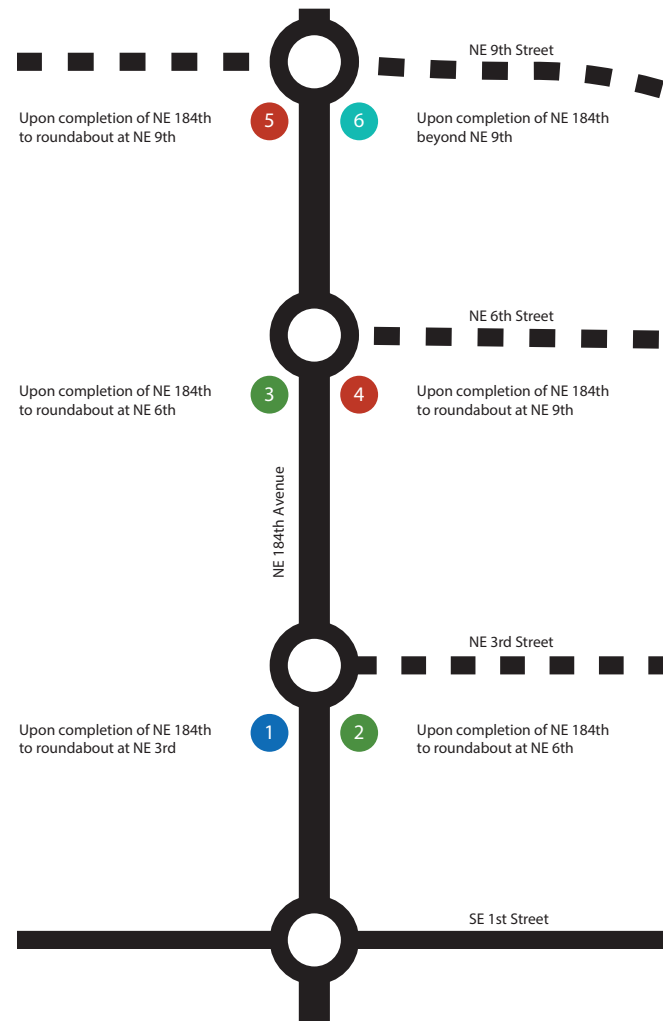
**SITE CIRCULATION CONTINUED**

As the principal south entry to the site, 184<sup>th</sup> Avenue will slope down approximately 20 feet between a new roundabout intersection on SE 1<sup>st</sup> Street and NE 3<sup>rd</sup> Street. Gradients will conform with ADA standards, as well as meet other relevant requirements. The embankments will be graded, amended, and planted to create a gateway landscape at this principal entrance to the site.

NE 9<sup>th</sup> Street will be constructed to connect 172<sup>nd</sup> Avenue to 184<sup>th</sup> Avenue at an elevation close to that of the HP property. This design will result in no significant grade change on 184<sup>th</sup> Avenue between HP's Phase 2+ development and the roundabout intersection with NE 9<sup>th</sup> Street to be constructed with Phase 2.

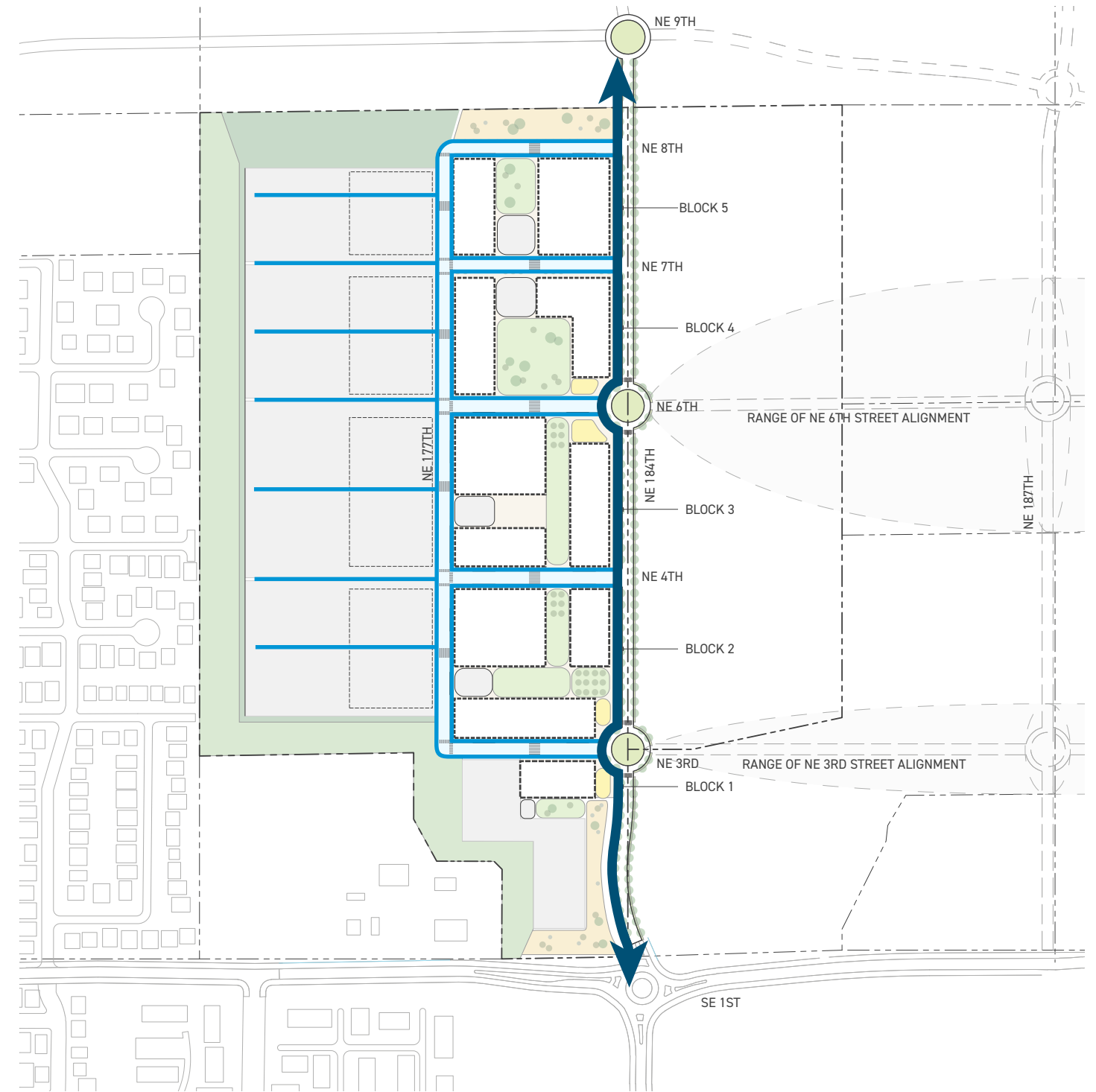
Through the Development Agreement, a traffic study was completed to determine average daily trips and pm peak hour trips generated by an initial 700,000 gross square feet of development. The traffic study estimated that the initial development will create 723 p.m. peak hour trips. Any development exceeding 700,000 gross square feet of building will be subject to the City's Section 30 Traffic Study. (DA § 4(g)).

The City's Section 30 Traffic Study will address the dispersal of trips to and from future housing and community centers to the east and north of HP property as development within Section 30 moves forward. The overall street pattern and layout proposed here, and conceptually laid out for the remainder of Section 30 in the Subarea Plan will accommodate this shift. (SP TR-3).



**FIGURE 3.C - CONCEPTUAL BUS STOP PHASING**

A conceptual bus stop phasing graphic has been provided by C-Tran depicting potential locations and a development-based timeline for implementation. Potential stop locations include the south side of roundabouts at NE 3RD, NE 6TH, and NE 9TH. South side bus stop placement may enable C-Tran to leverage the development schedule by using the roundabouts to turn service while subsequent phases are built out. C-Tran bus stops can be accommodated along NE 184TH by modifying the existing right-of-way planter area for the transit platforms. Transit usage will dictate the size and character of each bus stop location.



**FIGURE 3.D - PEDESTRIAN AND BIKE NETWORK**

Note that within the HP property, primary pedestrian circulation will be via sidewalks. Secondary pedestrian routes will link buildings across open space both in the north-south and east-west directions, however, primary access will be from the sidewalk.

**LEGEND**

- SECTION 30 RECREATIONAL TRAIL
- PEDESTRIAN SIDEWALK
- BIKE-FRIENDLY STREET

### 3.2 STREET DESIGN

#### NE 184TH AVENUE

Public access to and through the HP property will be provided by NE 184<sup>th</sup> Avenue. The 70 foot wide right-of-way allows for a 34 foot wide roadway bordered on each side by a six foot wide planting strip and a 12 foot wide multi-modal recreational trail on the west side of 184<sup>th</sup> Avenue for pedestrian, bicycles and other non-vehicular traffic, and 12 foot wide sidewalk on the east side of 184<sup>th</sup> Avenue. The center line of the 184<sup>th</sup> right-of-way as it extends north beyond Phase 1 is aligned with the eastern property line of HP and the western property line of CTC Section 30, LLC. Both parties have agreed to evenly split the alignment between their respective properties. (SP PU-6) (DG B.4.2).

Vehicular turns onto 184<sup>th</sup> Avenue from the HP property will be right-in, right-out only, as well at the roundabouts located at the intersections of 3<sup>rd</sup> Street and 6<sup>th</sup> Street. While the roundabout designs for 3<sup>rd</sup> and 6<sup>th</sup> Streets are currently planned to be compatible with the roundabout at the intersection of 1<sup>st</sup> Street and 184<sup>th</sup> Avenue, consistent with the Development Agreement, HP has the option to provide a different configuration based on measured traffic flow subject to City review and approval. (SP TR-9) (DA Ex. 6, p. 2).

Consistent with the Development Agreement, HP may provide 200 feet minimum separation between intersections of local collector streets that still meet the Subarea Plan policies, TR-7, TR-9, and TR-21, governing intersection spacing. Since the intent of these guidelines is met with the proposed layout, the right-in, right-out access at these locations will allow for safe, efficient access, and will provide necessary emergency access to future buildings and parking areas between roundabouts. (SP TR-7, 9, 21) (DA § 4(d)(vi)) (VMC 20.690.040(G)).

Consistent with the Development Agreement, this permission for right-in, right-out access is not construed as any exemption to the City's Street and Development Standards. (VMC Chapter 11.80). Further, the City has agreed it will review in good faith any standards modification requests (i.e. road modification requests) under VMC 11.80.160 that are deemed necessary to achieve sufficient access to 184<sup>th</sup> Avenue to effectively implement the Master Plan vision for the HP property.

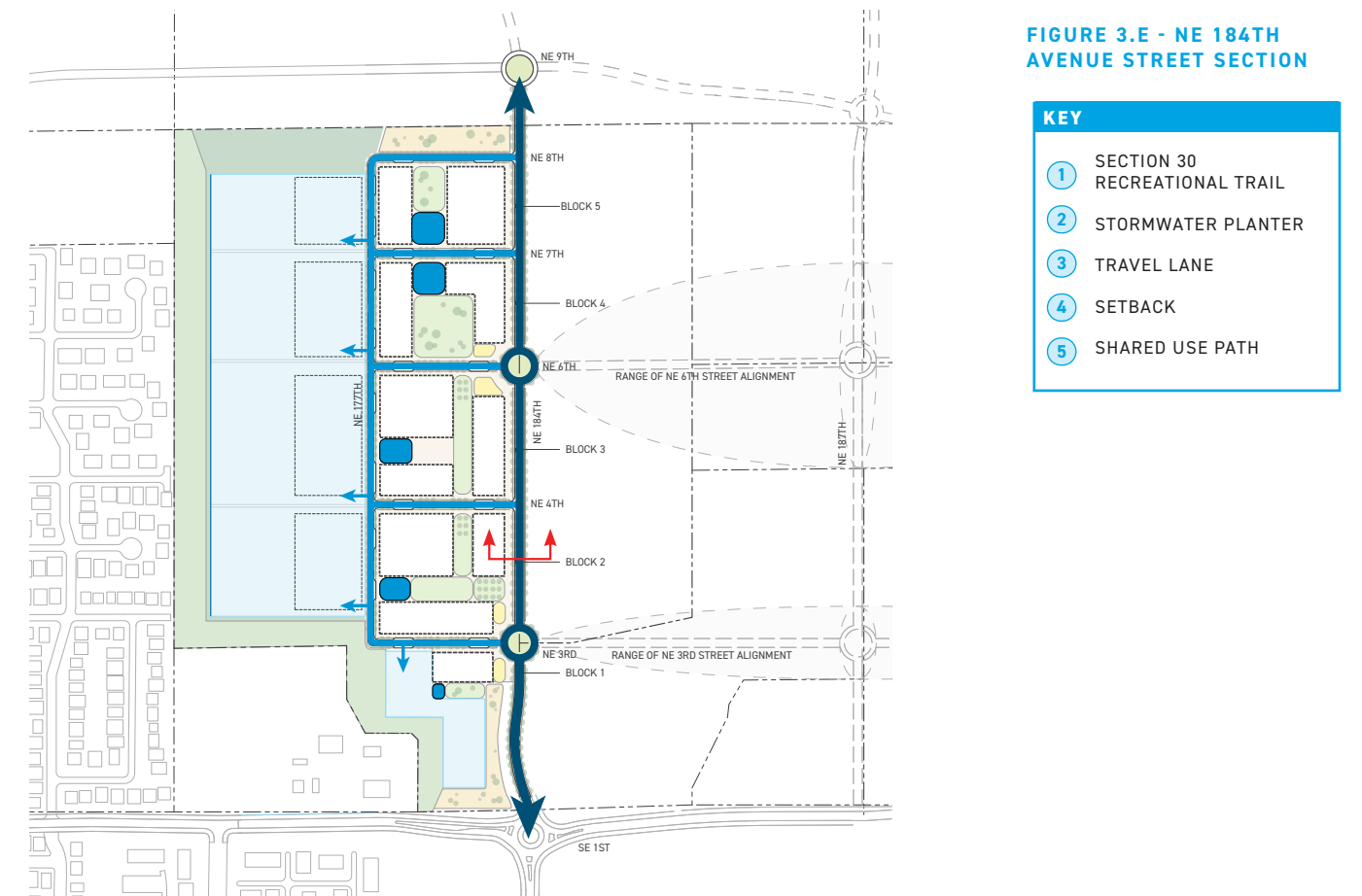
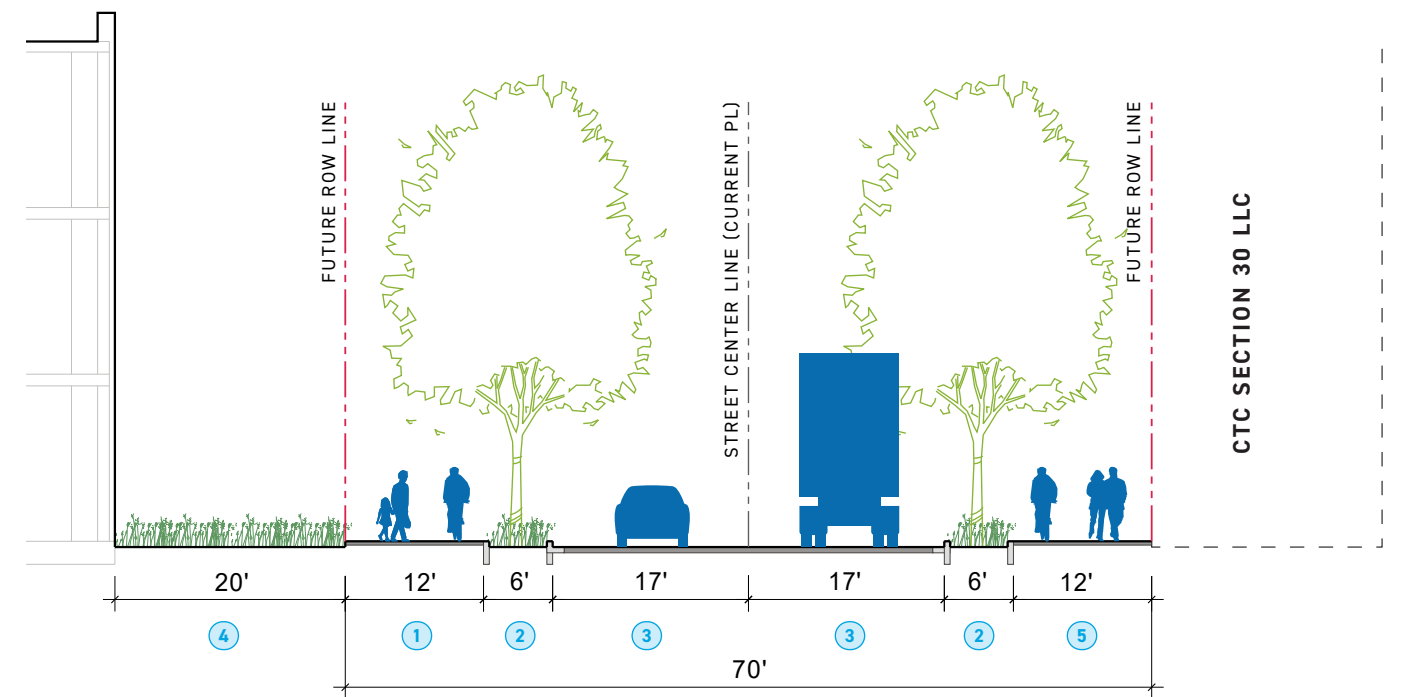


FIGURE 3.E - NE 184TH AVENUE STREET SECTION

KEY	
1	SECTION 30 RECREATIONAL TRAIL
2	STORMWATER PLANTER
3	TRAVEL LANE
4	SETBACK
5	SHARED USE PATH



**PRIVATE STREETS**

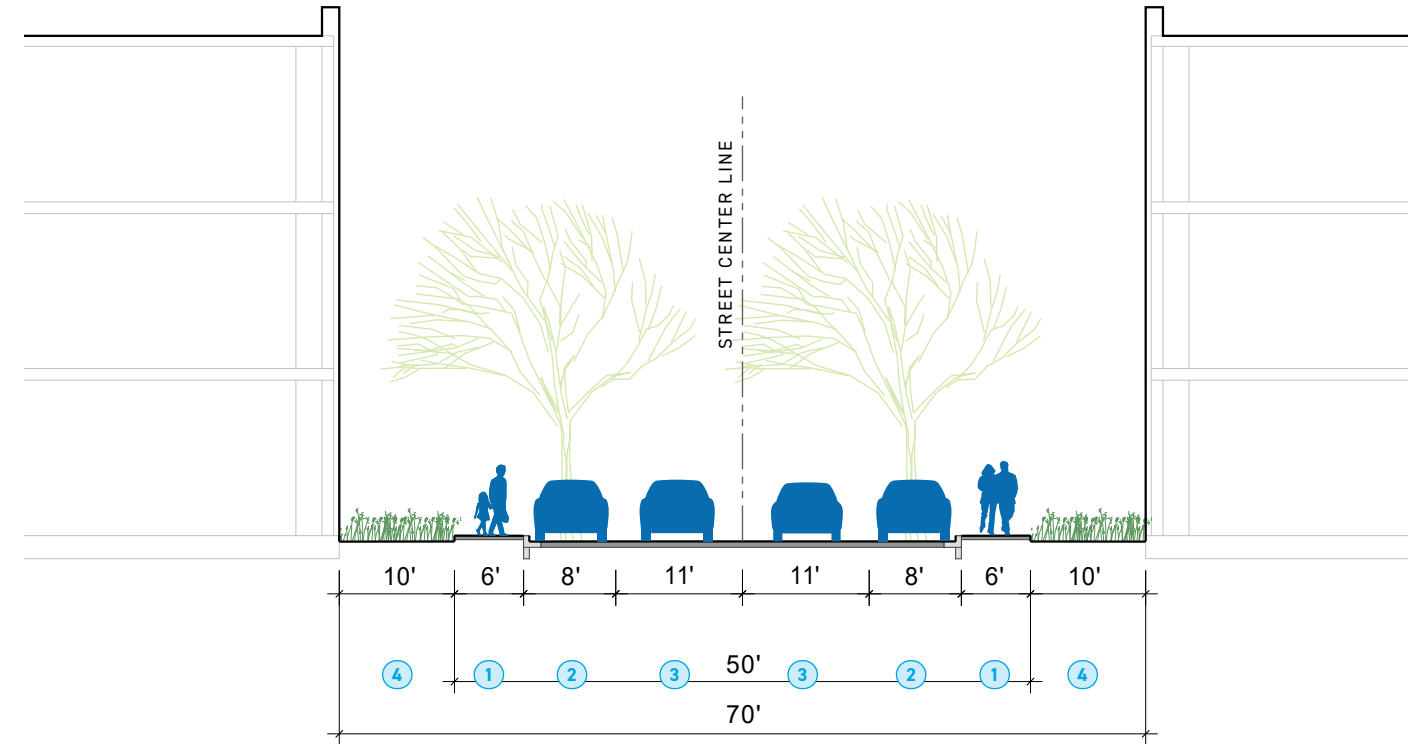
Five private streets across the HP property, designed to local standards, will connect 184<sup>th</sup> Avenue to parking and service areas to the west of the buildings. Low speeds on these private streets will allow bicyclists to share the road to access private bicycle parking and locker facilities within buildings or near service areas. Consistent with the Development Agreement local private streets for ingress and egress to parking areas for each building are included in the design.

Each block of buildings and open spaces will be separated from adjacent blocks by a 22 foot wide private street with a landscaped 10 foot setback to buildings to the north and south. The landscaped setback will include continuous and connected sidewalks. HP includes a planned exception for the road north of Block 5 because it will have fully connected sidewalks, but no buildings on the north side. To the extent that these private streets provide access to multiple phases of HP's development, joint access goals of the Subarea Plan are met. (DG B.5.1.a) (SP TR-19, 22) (DA Ex. 6, pp. 2, 4).

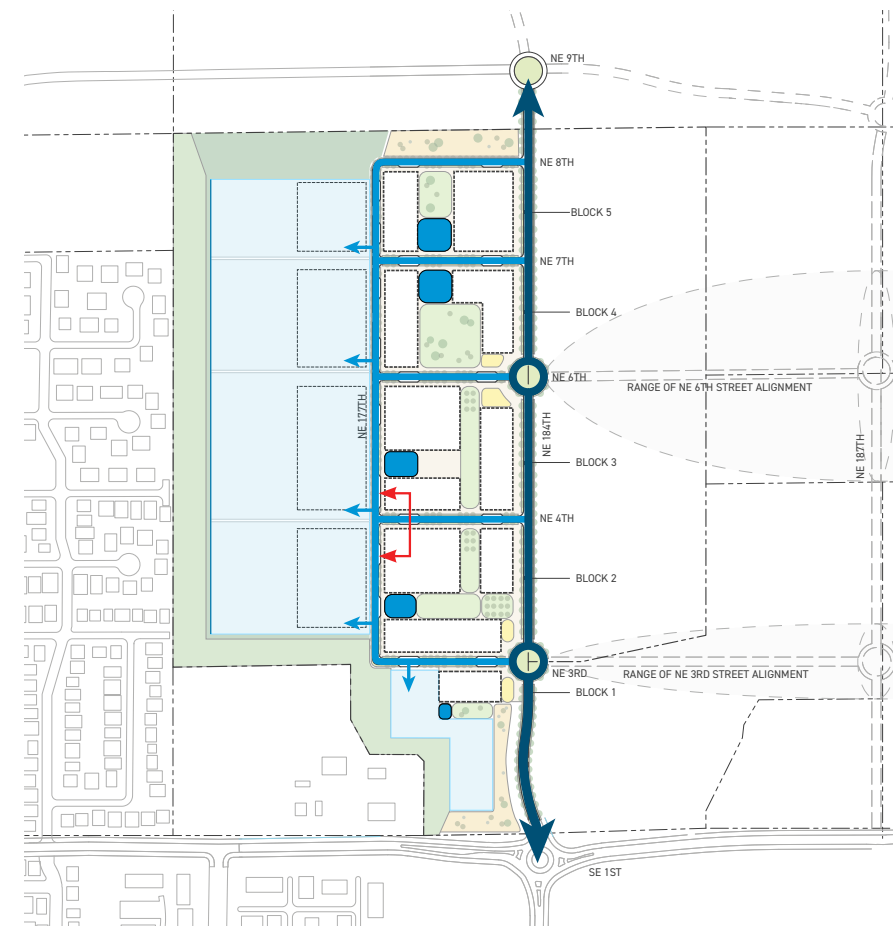
The five private east-west streets will connect to a private north-south street that may run parallel to 184<sup>th</sup> Avenue, or may curve to the east or west as may be required to accommodate building or parking

locations. This 22-foot wide roadway will accommodate two-way traffic and will be bordered by 14 foot wide setbacks from curb lines that include sidewalks, landscape, stormwater planters and street trees. This street will provide access to service areas in each of the five blocks of development, as well as provide internal bicycle access to buildings along the west side of the developed area. Access points on the west side of this private road will provide access to surface parking lots that extend to the toe of the west boundary bank. On-street parking will not be located along NE 184<sup>th</sup>, and is also not planned for the east-west streets extending to the west.

If necessary, structured parking may be added during later phases of construction as indicated on the FSUP (see Section 5.2 below). Street frontages for structured parking will not exceed 100 feet in length and entry design will be respectful of pedestrian circulation routes, will meet landscape and other design requirements, and design guidelines. HP's parking design will include 10-foot wide east-west pathways bordered by shade trees and six foot wide rain-gardens to provide safe and pleasant pedestrian access between parking areas and buildings. (SP LU-7, TR-3, 4, 11, 12, 14, 15, 16, 20, PU-21) (DG B.5.2, B.6.3.a, B.6.3.b, and E.1.5) (DA Ex. 6, p. 3).



**FIGURE 3.F - PRIVATE STREET (W-E) STREET SECTION**



KEY	
1	SHARED USE PATH
2	STORMWATER / DROP OFF
3	TRAVEL LANE
4	SETBACK

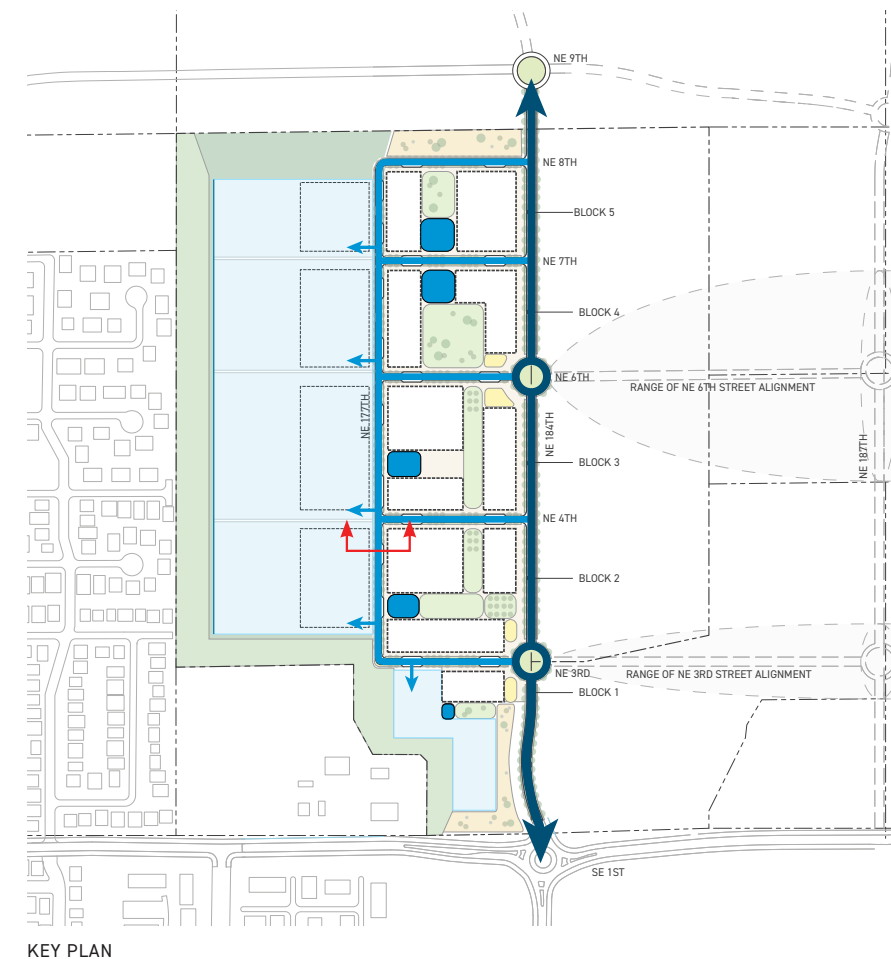
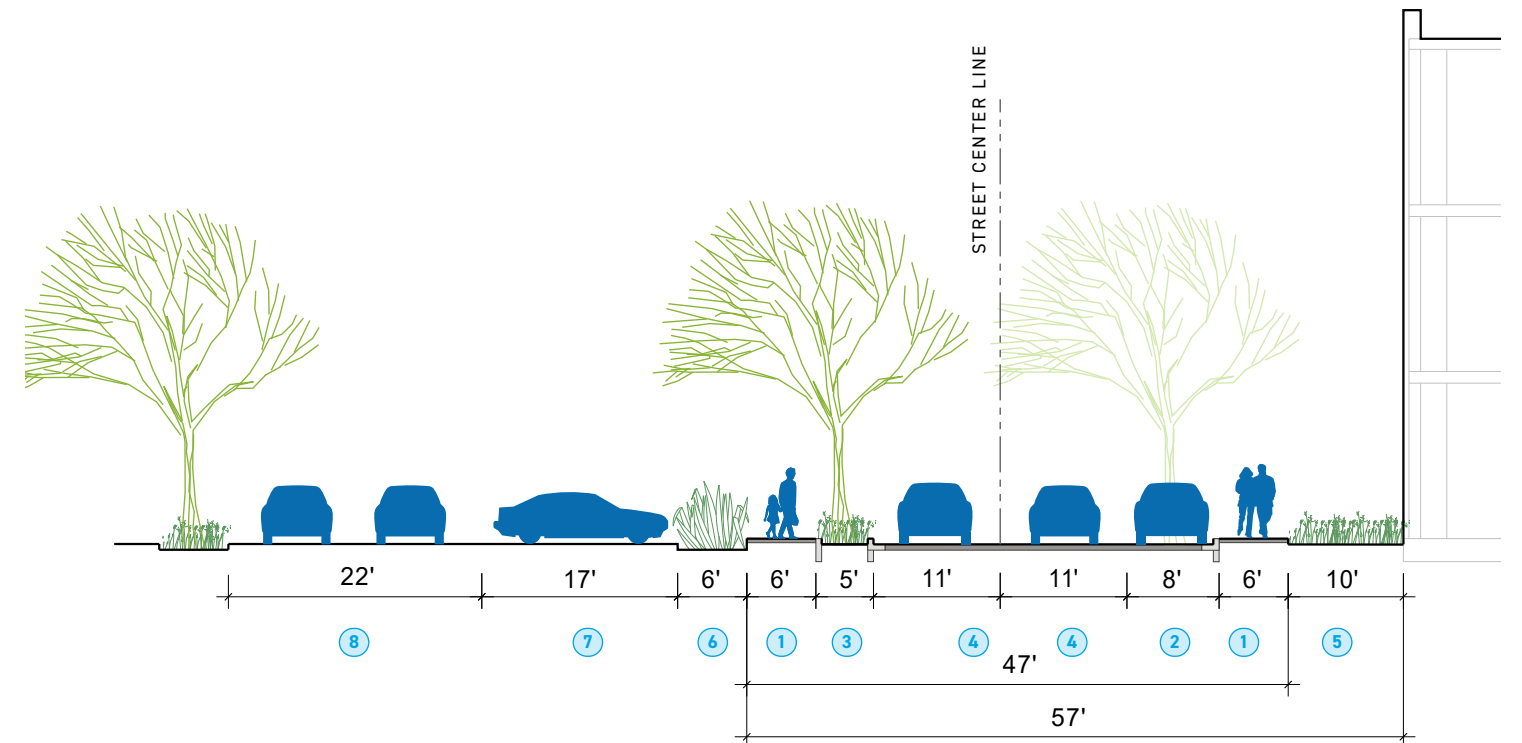
KEY PLAN

**PRIVATE STREET ACCESS**

Consistent with the Development Agreement, HP will continue to have the option to improve streets in a different configuration based on measured traffic flow subject to City review and approval. In addition, HP may seek approval to incorporate same-side ingress/egress for HP's research and development buildings at the northern most boundaries of the HP property for truck deliveries and low density (by head count) facility parking.

HP's proposed street design also accommodates large truck turns for service and delivery trucks. The roundabouts on 184<sup>th</sup> Avenue, and private streets are designed to accommodate truck access to service yards and loading bays.

HP will accumulate TIF credits for any qualifying road projects it constructs unless the City contributes City Reinvestment Funds to cover the full cost of a qualifying road project. HP shall be entitled to TIF credits for the portion it funds, and these will not be deducted from the City's Reinvestment Funds. (DA §§ 6(c), 7, Ex. 6, p. 8).



**FIGURE 3.6 - PRIVATE STREET (N-S) STREET SECTION**

KEY	
①	SIDEWALK
②	STORMWATER PLANTER / DROP OFF
③	STORMWATER PLANTER
④	TRAVEL LANE
⑤	SETBACK
⑥	PARKING SCREENING
⑦	PARKING STALL
⑧	PARKING DRIVE AISLE

### 3.3 PARKING

The overall parking plan in the FSUP is conceptual and shows that there is adequate room for parking to serve full build out of the HP property, and for access between parking lots to occur without having to access public streets. In the later phases, HP may propose deck or structured parking over the existing parking areas as shown on Figure 3.H. Any parking structure will include landscape screening to soften the appearance of the structure. Charging stations for electric vehicles will be provided, and will be designed and located to comply with the ADA and code requirements. HP is permitted, but not required to provide joint parking facilities. (SP TR-18, 19) (DG B.6.3.b) (DA Ex. 6, p. 4).

The HP Master Plan includes parking space allocation that is consistent with the Development Agreement. The parking space description in the VMC Table 20.945.070-2 lists the minimum number of parking spaces associated with a particular use. The City may interpret VMC 20.690.040(E) to suggest that a maximum parking space limit applies to Section 30 development, subject to exceeding the maximum if a parking study is submitted. In order to allow flexible, successful, and efficient development of the HP property, the City agrees that HP will undertake a parking study after Phase 1 development is operational to verify the sufficiency, accessibility and safety of the lots. The parking study could provide different use scenarios for

different buildings and how parking will be brought into compliance with the code as the HP property develops. The overall parking plan analyzed in the parking study could be conceptual with the understanding that it may change as the HP property develops. Notwithstanding the foregoing, the HP property has adequate room for parking, including through contemplated parking structures as discussed in this Master Plan, should they be needed in the future. Parking lots and spaces will be consistent with the Design Guidelines. (DG B.6.1, B.6.2, B.6.3.a, B.6.3.b, B.6.3.c, B.6.3.d, B.6.4) (VMC 20.690.040.E).

Consistent with the Development Agreement, parking spaces shall be provided for in the following ratios:

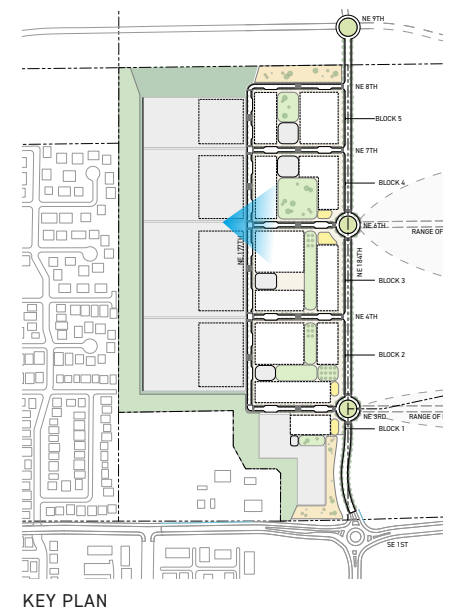
- › Office uses: 2.5 to 5 spaces per 1000 square feet.
- › Research and Development: 1.25 to 3 spaces per 1000 square feet.
- › Light industrial uses: 1.25 to 3 spaces per 1000 square feet.
- › Occupant-supported retail: 1 space per 250 square feet, restaurant, and accessory uses.

Note that occupant-supported uses are primarily meant to be open to HP employees, therefore no additional parking demand will be generated. Other uses incidental to an office building will meet the parking ratios for office uses.



FIGURE 3.H - CONCEPTUAL VIEW OF NE 6TH STREET LOOKING EAST FROM PARKING AREA

Parking spaces will be interspersed with north-south rows of trees to screen and shade vehicles and users, and to reduce heat build-up in paving. As shown on the FSUP, west to east pathways that align with the private street configuration offer pedestrian access to the HP buildings. The walkways will be bordered by shade trees set in rain-gardens designed to manage stormwater runoff through bio-filters prior to discharge into groundwater infiltration areas. (DG B.5.1.b, B.6.1.a, B.6.1.c, B.6.1.d) (SP UD-12, OS-4, OS-5, PU-21) (DA Ex. 4, pp. 3-4) (VMC 20.690.040(E)).



KEY PLAN

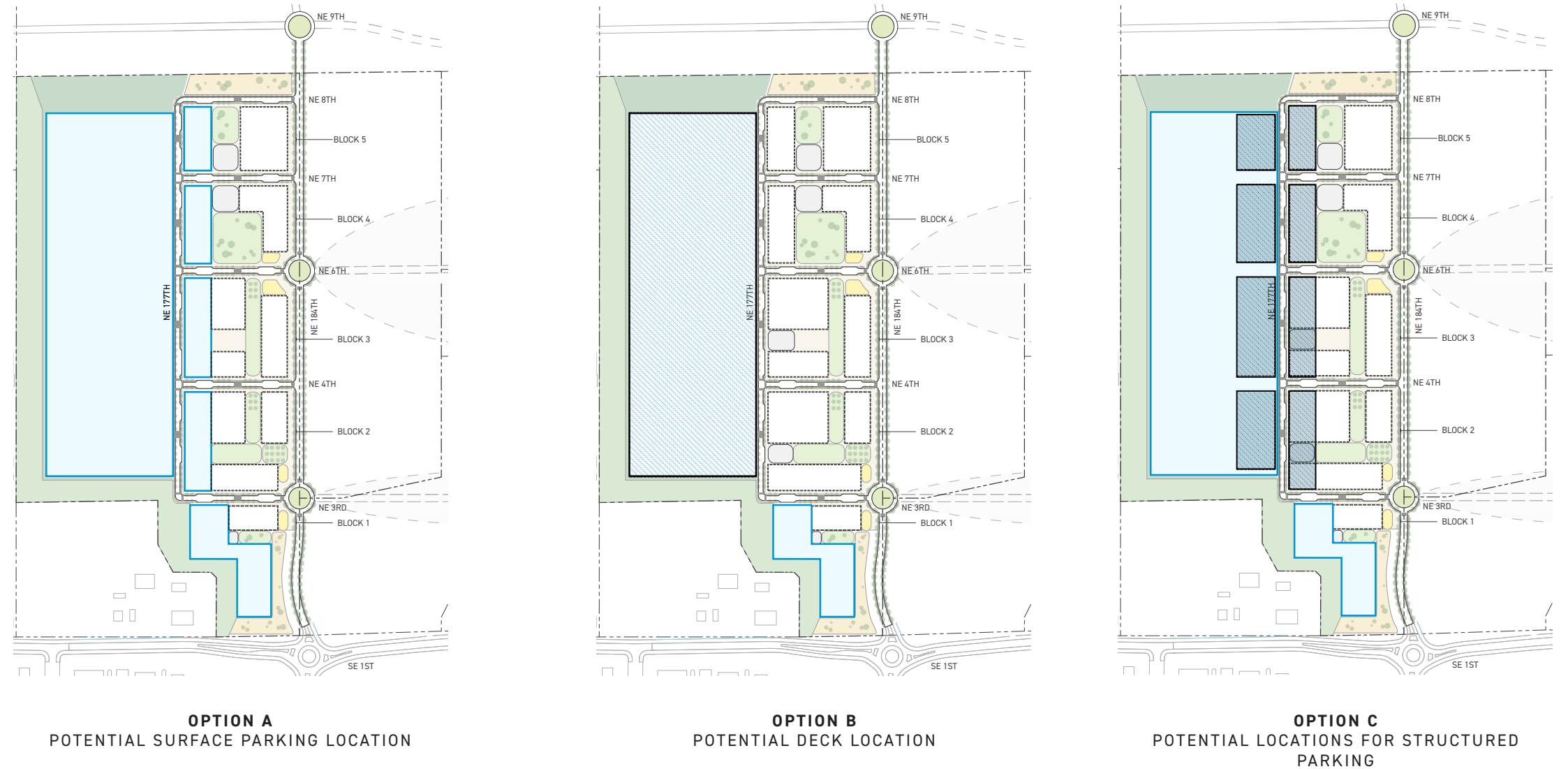
**PARKING DIAGRAMS**

The adjoining diagrams illustrate how the Master Plan can accommodate several types of parking on the western portion of the site. The ultimate parking design may incorporate some or all of these options as development of the HP property occurs over time.

The Option A diagram depicts the lowest density parking type with all surface parking on the western half of the site and local surface parking lots within the individual blocks. The all surface parking solution could be used with an ultimate build-out on the lower end of the density projected for the site.

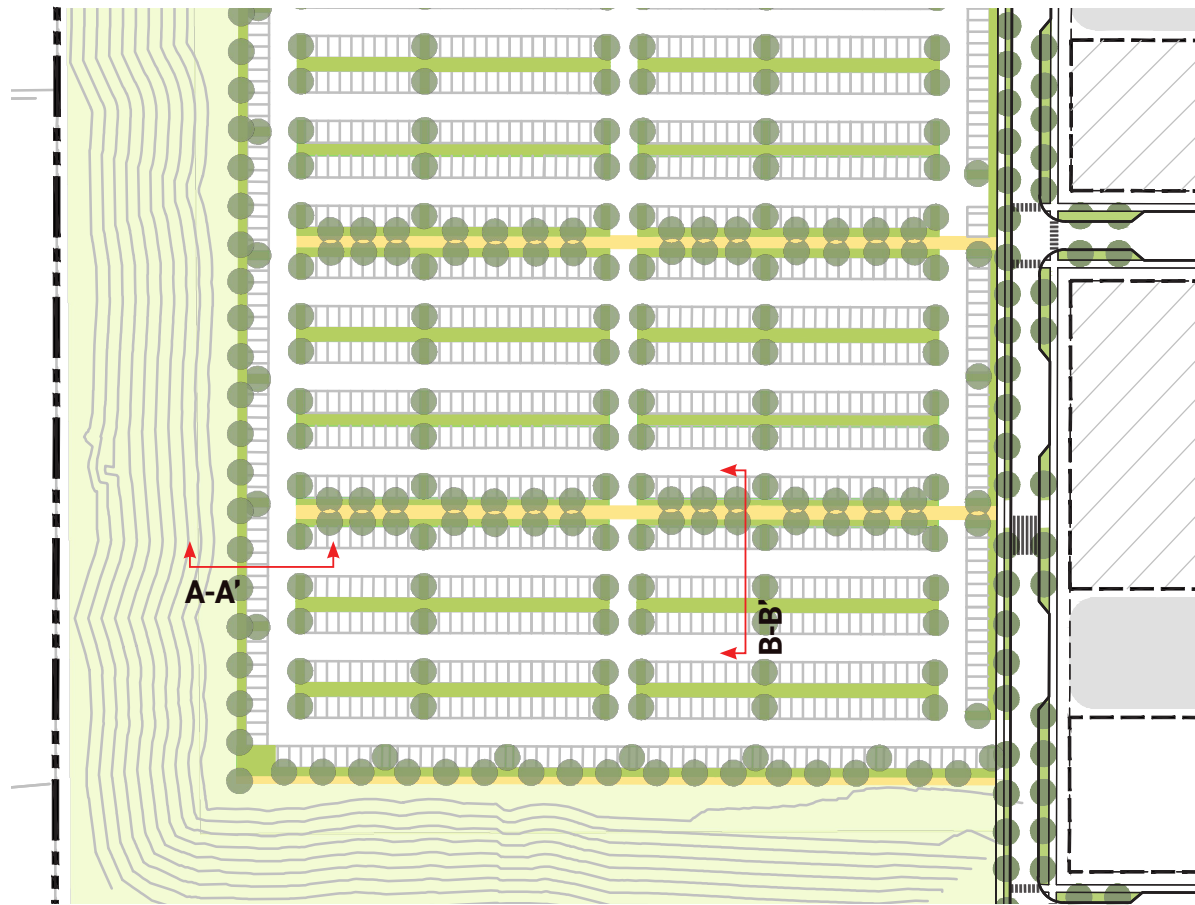
The Option B diagram shows the potential for inclusion of a single layer deck on top of surface parking. This option could be purpose built or retrofitted on top of an existing surface parking lot in the event that the density of development on the site exceeds the capacity of the surface parking areas.

The Option C diagram shows potential locations for multi-story structured parking. Structured parking could be used in conjunction with surface parking. (DG B.6.1.b).

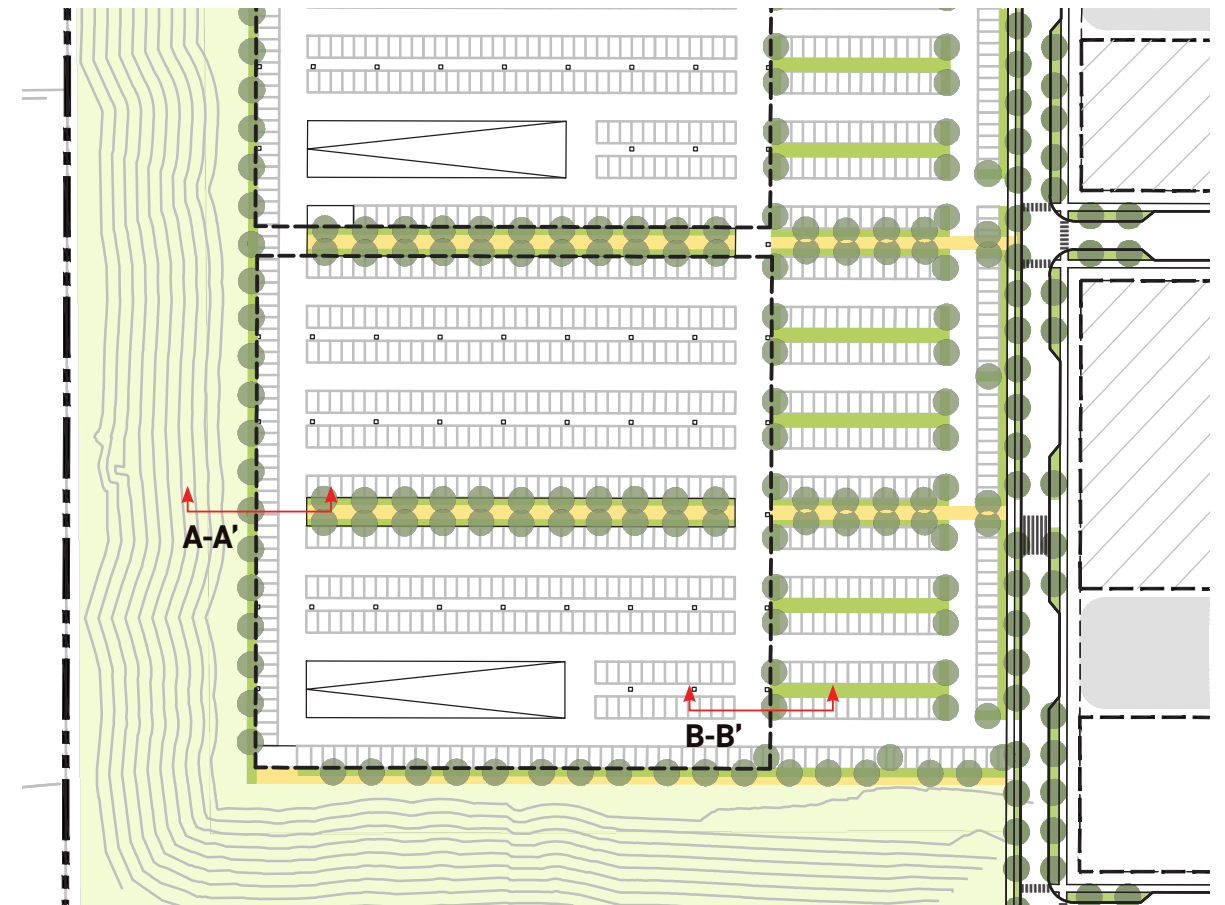


**FIGURE 3.1 - POTENTIAL PARKING OPTIONS**

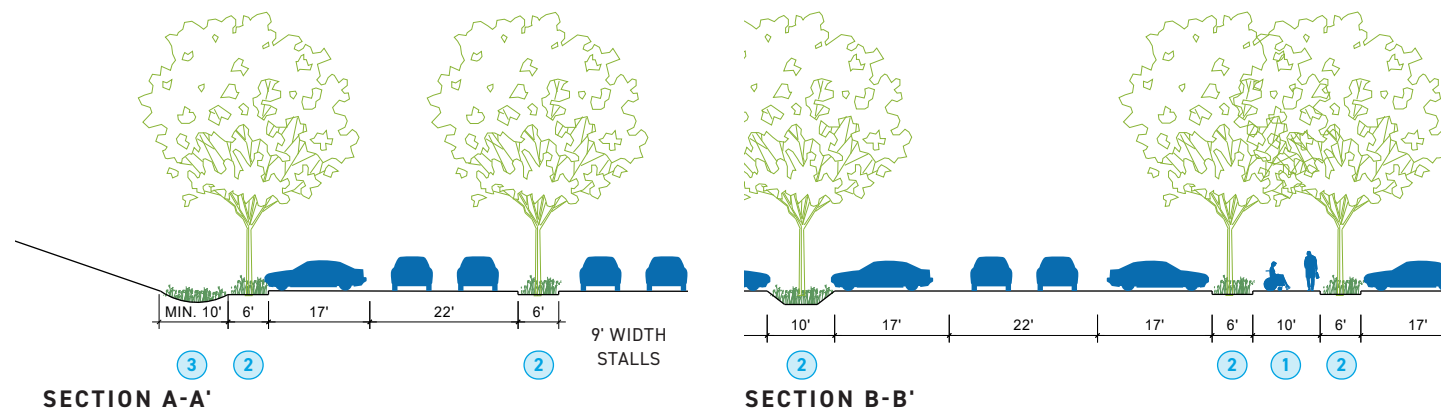
LEGEND	
<span style="display:inline-block; width:15px; height:10px; background-color:lightblue; border:1px solid black;"></span>	SURFACE PARKING
<span style="display:inline-block; width:15px; height:10px; background: repeating-linear-gradient(45deg, transparent, transparent 2px, blue 2px, blue 4px); border:1px solid black;"></span>	DECK PARKING
<span style="display:inline-block; width:15px; height:10px; background-color:blue; border:1px solid black;"></span>	STRUCTURED PARKING



TYPICAL SURFACE PARKING PLAN



TYPICAL DECK PARKING PLAN



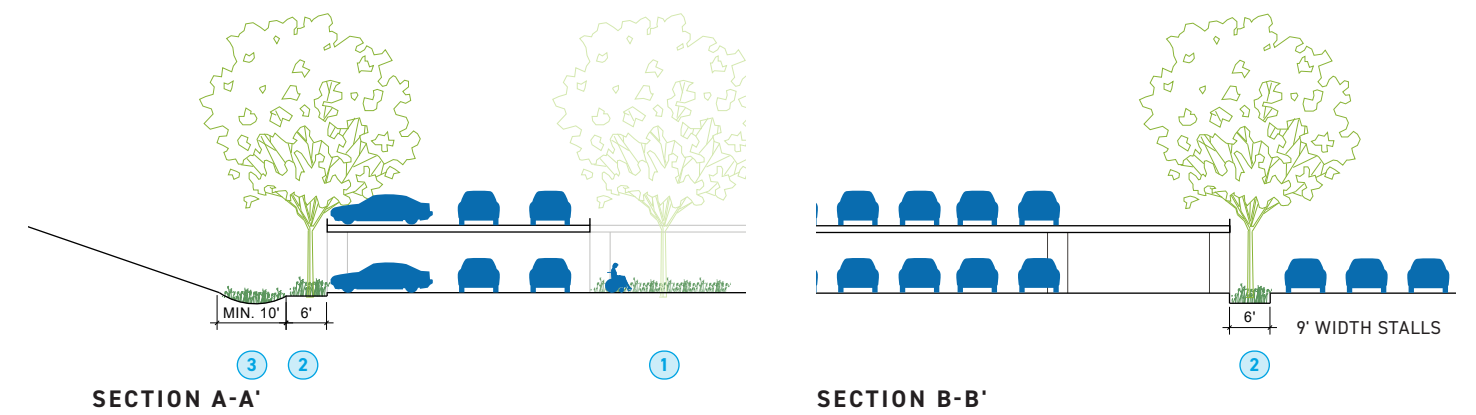
SECTION A-A'

SECTION B-B'

KEY	
1	PATHWAY
2	RAINGARDEN
3	INFILTRATION SWALE

FIGURE 3.J - TYPICAL SURFACE PARKING

Surface lots will be screened from view for building occupants by the canopies of shade trees that will also reduce heat-island effects and shade parked cars.



SECTION A-A'

SECTION B-B'

FIGURE 3.K - TYPICAL DECK PARKING

### 3.4

## TRAFFIC STUDY RESULTS

In preparation for the Development Agreement, HP prepared a traffic study to analyze traffic impacts associated with the construction of 700,000 GSF of building area (see Appendix B). Consistent with the Development Agreement, HP has vested 723 p.m. peak hour trips. The traffic study identifies certain offsite improvements that will be considered as NE 9<sup>th</sup> Street is extended to 172<sup>nd</sup> Street. HP will be required to address these improvements in its site plan submittals as development moves north from Phase 1 through the 700,000 GSF contemplated in the traffic study. (DA Ex. 7).

In cooperation with the City of Vancouver and other Section 30 property owners, HP plans to participate in the City's Section 30 Traffic Study, that is anticipated to define required off-site improvements and methodology for proportionate cost sharing. Consistent with the Development Agreement, the City Traffic Study is going to use ITE Trip Generation Manual, 10<sup>th</sup> Edition (2019) land use designation 714 to measure the traffic impacts for HP's further development beyond 700,000 GSF up to the 1.5 million GSF contemplated in this Master Plan. (SP TR-1) (DA § 6(a)).

### 3.5

## TRAFFIC DEMAND MANAGEMENT APPROACH

HP may employ the following strategies to reduce impact on off-site roadways resulting from development of up to the 1.5 million GSF contemplated in this Master Plan.

1. Allow commute trips to avoid peak transportation system demand

- Use staggered shift operations and schedule employee shift changes outside of the typical commuter peak time periods (7:00 to 9:00 AM and 4:00 to 6:00 PM)
- Offer telework options and/or flexible work hours that eliminate the need to commute to the HP property one or more days per week

2. Provide employee kitchen and café/deli services on site that are available to HP employees within the HP development, reducing the need for employees to travel off site for meals and/or break periods

3. Encourage bicycling and walking as viable commute options, including but not limited to providing on the HP property:

- secure, covered bike parking in strategic locations on-campus
- self-service bike tune-up/repair areas
- sponsor monthly (or bi-monthly) bike mechanic to provide free tune-up services to cyclists
- employee lockers for storage of change clothing and personal items to provide more convenience to bicycle and walking commuters
- form an employee-led cycling group (self-selected by participants) to offer additional strategies to encourage cycling over time

4. Encourage participation in commuter challenges by employees.

5. Provide and disseminate information for alternate transportation options including:

- address commuting options as part of the employee orientation program

- document information on the company/ employer intranet about commute options
- disseminate C-Tran schedule and service information to employees
- encourage employee carpools through carpool matching services and provide preferential parking within parking lots.

6. Provide an on-site employee transportation coordinator (ETC) who can assist and be responsible for promoting, facilitating, and coordinating transit service and carpools for employees with similar shift patterns

7. Promote C-Tran transit service as a commuting option, that may include:

- incorporate convenient C-Tran service stops with amenities at/near the HP property
- provide pre-tax transit passes and/ or other financial incentives
- subsidize C-Tran passes at 50 percent
- provide enhanced connectivity to the Fisher's Landing Transit Center, the potential Columbia Tech Center Bus Rapid Transit Center, and/or the nearest MAX Light Rail station (in Oregon).

8. Provide emergency ride home services for employees who do not commute in a personal motor vehicle.

9. Partner with a shared vehicle provider such as ZipCar to have one or more vehicles on campus available for employee use.

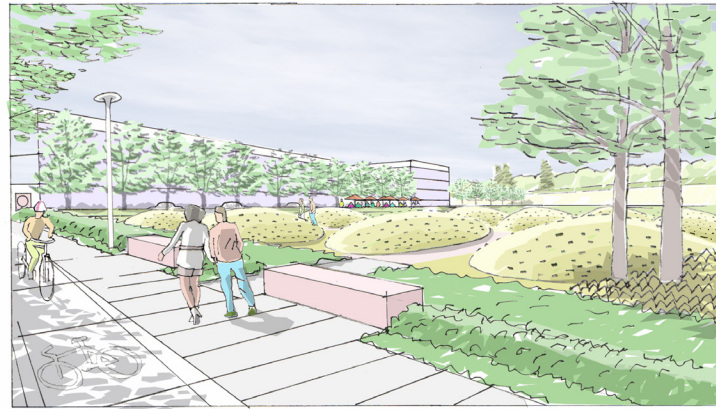
10. Consider incentives encouraging employee participation in non-single occupant vehicle commutes.

11. Provide periodic monitoring and updates to the TDM program in partial fulfillment of Washington state's Commute Trip Reduction (CTR) law in response to surveys sent by WSDOT and Clark County. (VMC 20.690.050(B)(5)).



# 04 / PUBLIC FACILITIES AND SERVICES

HP has worked with the major property owners in Section 30, including CTC Section 30, LLC, and Grayrock to identify Section 30 wide utility planning to serve the major property owners. In addition, HP has coordinated extensively with City staff to encourage development of utility systems in a cost-efficient manner consistent with the City's existing plans, including, but not limited to, coordinating underground utilities as 1<sup>st</sup> Street is upgraded and 184<sup>th</sup> Avenue is extended north from 1<sup>st</sup> Street to 9<sup>th</sup> Street. This section of the HP Master Plan provides details about the stormwater design, sanitary sewer, and other utilities that will be constructed as HP develops its property, as well as the anticipated dedications of public facilities in Phase 1 and in future phases. (SP PU-1) (DA § 6(e)).



## 4.1 STORM WATER FACILITIES AND GREEN DESIGN

Development of the HP property will be consistent with the City of Vancouver Stormwater Management standards. Each property owner in Section 30 is to manage stormwater quality on its own property prior to releasing it to a public stormwater collection facility. Native gravels provide the best infiltration sites, and the City will prioritize reservation of areas that still have native gravel for stormwater infiltration especially near NE 9<sup>th</sup> Street. If the City constructs a public stormwater facility or accepts dedication of a stormwater facility, then HP may connect to it provided that all applicable code provisions and payment of any connection charges are made. HP's property has adequate infiltration capacity to handle onsite stormwater. Should it prove that onsite capacity is insufficient, HP's connection to the City maintained public stormwater facility is allowed. In combination, onsite infiltration and offsite public facility with onsite management of water quality, will result in a stormwater facility design that does not affect other surrounding properties.

Consistent with the Development Agreement, Subarea Plan policies PU-23 and PU-24 will be addressed during site review if HP connects to the public stormwater system. (SP PU-16, 17, 18, 19, 20, 22) (DG C.1.1.c) (DA §§ 4(d)(iii), 6(e), Ex. 6, pp. 7-8).

Finished grades across the HP property will slope down approximately seven feet between Phase 1 and NE 9<sup>th</sup> Street. This slope will direct stormwater through filtering landscape materials towards the infiltration zone on the south side of 9<sup>th</sup> Street. Raingardens and other passive landscaped areas will be used to collect and filter stormwater on the HP property. Filtered stormwater will be conveyed to infiltration zones between parking lots and the toe of the earthen banks located at the south and west of the HP property, and to the designated infiltration field near NE 9<sup>th</sup> Street. All filtration landscape will be accessible for periodic maintenance. (SP PU-17, 21) (DG A.2.1.a, B.2.3.a, C.1.1.a, C.1.1.b).

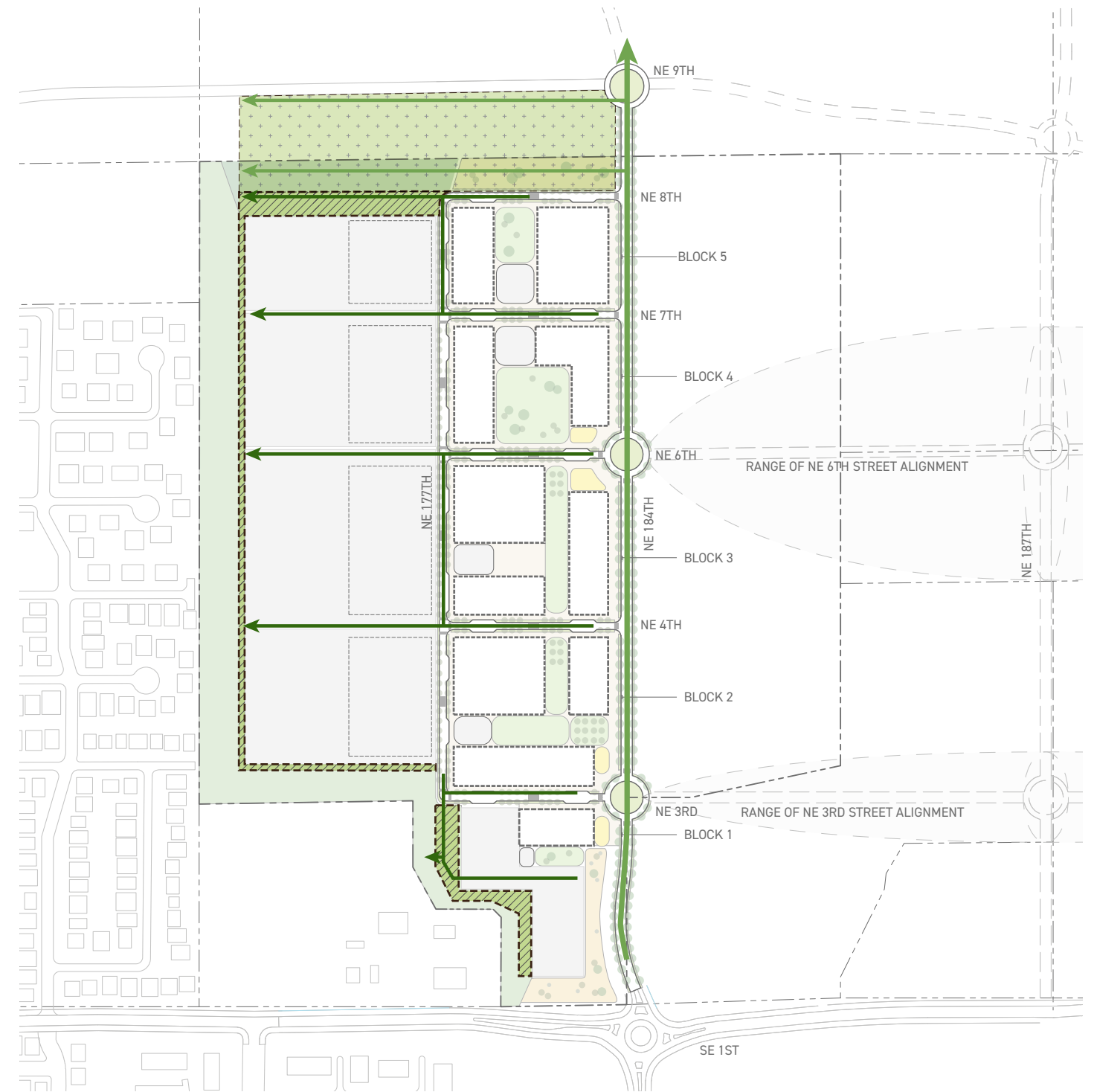


FIGURE 4.A - STORMWATER MANAGEMENT

LEGEND	
	PUBLIC TREATED STORMWATER CONVEYANCE
	PRIVATE TREATED STORMWATER CONVEYANCE
	STORMWATER TREATMENT AREA
	INFILTRATION FACILITY FOR PUBLIC IMPROVEMENTS



## 4.2

### REGIONAL SANITARY SEWER FACILITIES

Consistent with the Development Agreement, a short-term sewer system is proposed to serve Phase 1. This system will allow HP to connect to a lift or pump station outside Section 30. Ultimately, HP's Phase 1 development will be required to connect to the Area 2 Pump Station within Section 30, in the potential location depicted on the FSUP. While this short-term solution is permitted, HP will install all required sanitary sewer infrastructure in the 184<sup>th</sup> z right-of-way extension constructed as part of Phase 1 that will be necessary to ultimately connect to the Area 2 Pump Station. This infrastructure will include gravity lines that will carry sewer flows from Phase 1 to the sanitary sewer pump station and a force main to carry sewer flows from the sanitary sewer pump station to gravity lines in SE 1<sup>st</sup> Street. (SP PU-7, PU-8, PU-10) (DA Ex. 6, p. 6).

HP may construct the Area 2 Pump Station as part of Phase I, or temporarily connect to a sanitary sewer pump station outside of Section 30 to service Phase 1 provided, however that HP agrees that the Area 2 Pump Station shall be constructed within three years of issuance of the first certificate of occupancy for a Phase 1 building. The deadline to construct the Area 2 Pump Station may be extended in the City's sole discretion in the event HP demonstrates good faith progress towards eventual construction of the station. Upon completion of the Area 2 Pump Station HP will connect Phase 1 to the sanitary sewer pump station. (DA § 8(c)).

Consistent with the Development Agreement, HP

has reserved 143,000 gallons per day of capacity in the Area 2 Pump Station and associated public infrastructure, equal to the anticipated sanitary sewer demand for the 1,500,000 gross square feet of development associated with HP's development; provided, however, that the reserved capacity shall not be available unless (i) the Area 2 Pump Station is constructed; (ii) Developer provides the City with a minimum twenty-four (24) months advance written notice of its need to utilize the capacity that was accomplished by HP's request for a pre-application meeting for the master planning process for the Project; and (iii) HP pays all applicable fees and charges required by the City to use the reserved capacity and that HP shall pay these amounts in proportionate share as the subsequent phases are developed and sewer capacity is used by HP. (DA § 4(d)(v)).

In addition, the size of the Area 2 Pump Station is contemplated with input from other major property owners within Section 30 to meet most, if not all, sanitary sewer pump station demand for Section 30. The Area 2 Pump Station will be dedicated to the City and the City will provide sanitary sewer service. Estimated capacity for the Area 2 Pump Station will be in the order of 400,000 GPD. (SP PU-3) (DA § 6(e)).

At this time HP does not have industrial wastewater planned as part of Phase 1. The City shall review any proposed industrial wastewater improvements through the Master Plan modification process. (DA Ex. 6, p. 6) (SP PU-9).

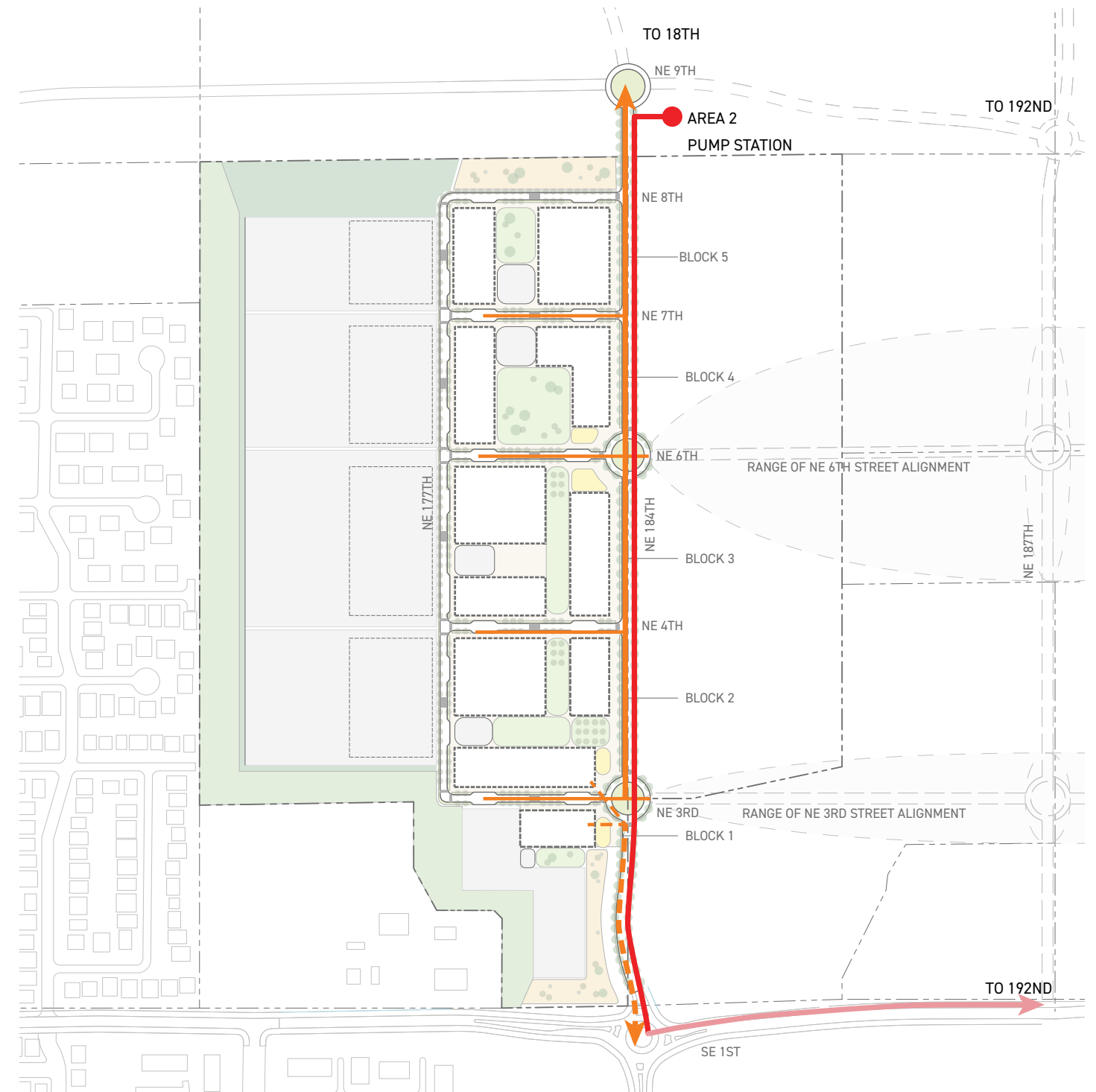


FIGURE 4.B - SEWER FACILITIES

LEGEND	
<span style="color: red;">—</span>	FORCEMAIN SEWER
<span style="color: red;">- - -</span>	CITY PLAN FORCEMAIN SEWER
<span style="color: orange;">—</span>	GRAVITY SEWER
<span style="color: orange;">- - -</span>	TEMPORARY PHASE 1 GRAVITY SEWER

### 4.3

## WATER, ELECTRICITY, DATA, TELECOMMUNICATIONS ROUTING AND CONNECTIONS

### WATER

The municipal water system will be extended into the site with appropriately sized mains as development progresses. Each building will have a water meter and fire hydrants sufficient to meet emergency needs. Any unique water service needs will be addressed separately as they are identified. Irrigation water demand will be kept to a minimum through predominant use of native and drought resistant plant materials. However, some irrigation will be necessary until trees, shrubs and other plantings become established. (SP PU-4, PU-15) (DA § 4(d)(iv)).

HP will make commercially reasonable efforts to design the development for sustainable water conservation, thereby pursuing the sustainable water use aspiration of the Subarea Plan. HP may choose to employ green roofs, and to decide at its sole discretion to use public water, rainwater collection, reclaimed greywater, or water from existing wells for non-potable purposes (although no wells are located on HP's property). (SP PU-11) (DG E.1.6) (DA Ex. 6, p. 6).

HP may not be required to use internal water line systems to maximize looping for Phase 1 if there is no looping to maximize. However, HP will be required to provide system looping and design for sequential development subsequent to Phase 1. (SP PU-12-13) (DA Ex. 6, p. 7).

Domestic water and fire water mains are assumed to be sized at 12". The City shall review HP's proposed water mains for Phase I during the site plan review to determine that new water mains will be installed to meet the need for future expansion of development on the HP property and Section 30. All other water main size choices and plans to connect gaps in the existing system around the boundary of Section 30 shall be designed to provide adequate service through collaboration with adjacent landowners within Section 30. (DA Ex. 6, p. 7) (SP PU-14).

### UTILITIES

Private utilities including telecommunications, power and natural gas will be looped from existing nearby service lines and supplemented as necessary to meet the needs of each successive phase of development. Consistent with the Development Agreement, fiber optic cables are part of "cabling needs" and Developer may install such cabling. (SP PU-5) (DA Ex. 6, p. 5).

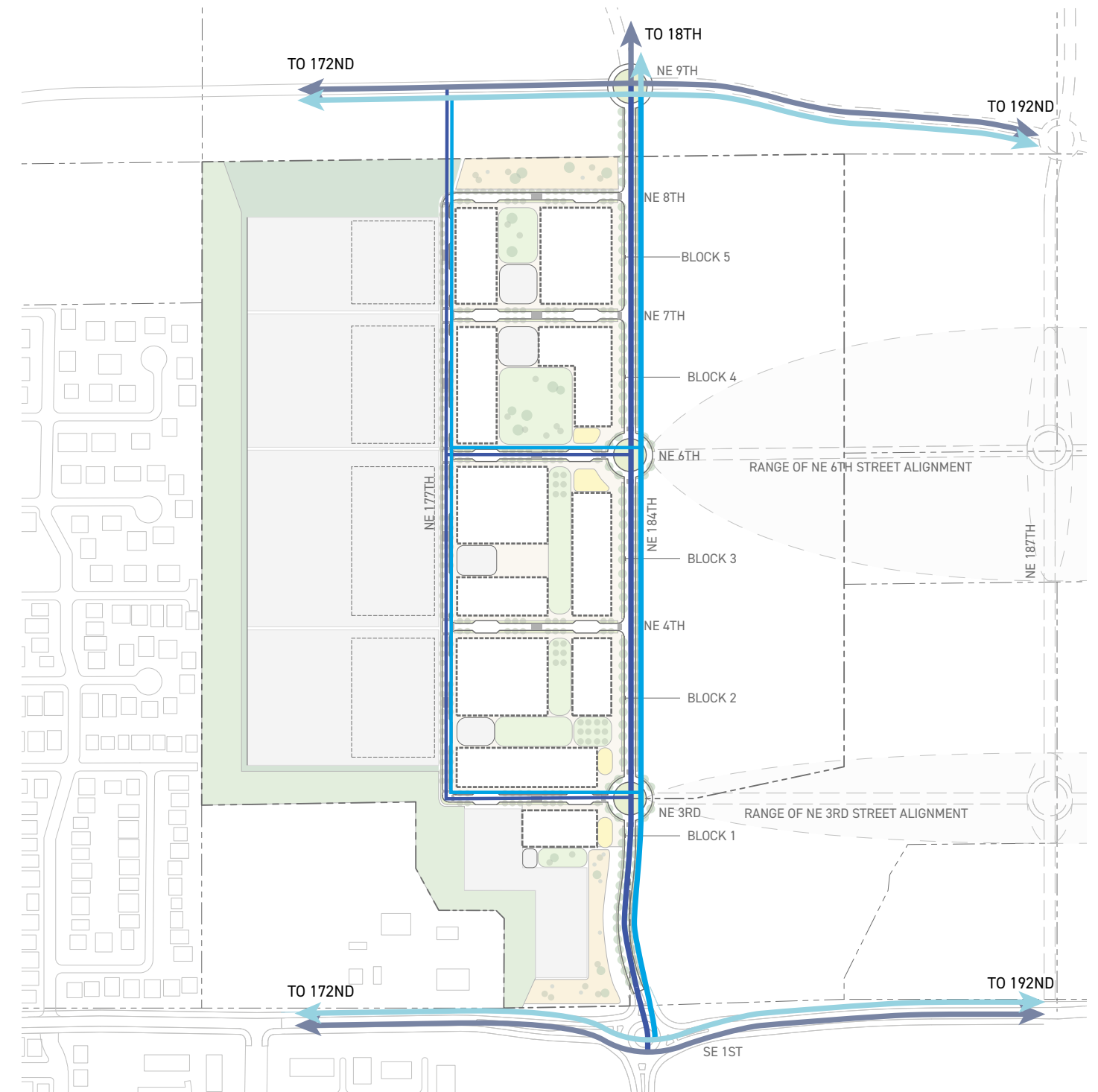


FIGURE 4.C - WATER, ELECTRICITY, DATA, TELECOMMUNICATIONS ROUTING AND CONNECTIONS

LEGEND	
<span style="color: darkblue;">—</span>	DOMESTIC WATER
<span style="color: lightblue;">—</span>	CITY PLAN DOMESTIC WATER
<span style="color: red;">—</span>	ELECTRICITY, DATA, TELECOMMUNICATIONS
<span style="color: lightred;">—</span>	CITY PLAN ELECTRICITY, DATA, TELECOMMUNICATIONS



# 05 / FULL SITE UTILIZATION PLAN AND PHASING

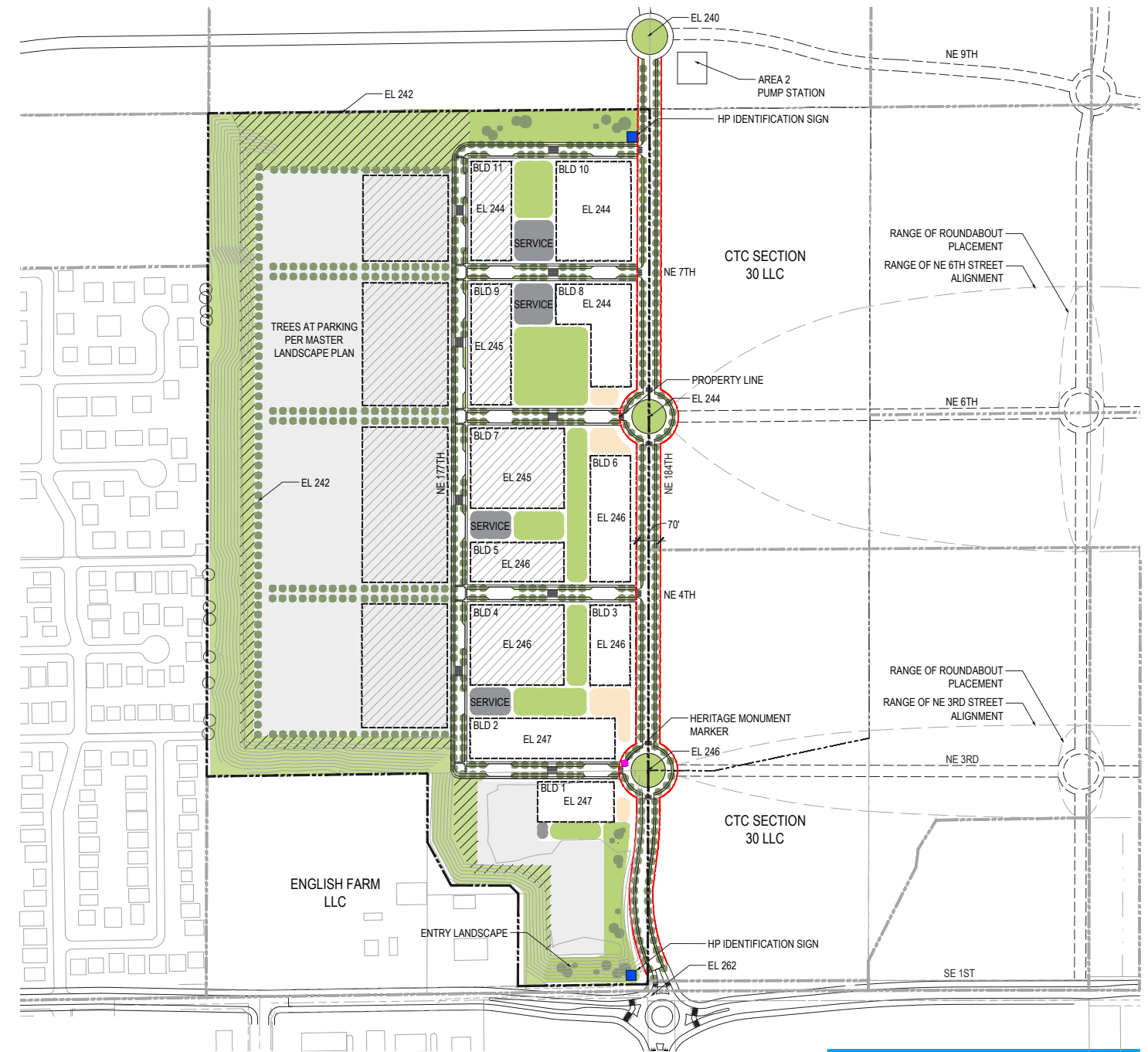
## 5.1 FULL SITE UTILIZATION PLAN

The function of the FSUP is to provide an overall impression of proposed development, and to show how the development plan can flexibly respond to changing circumstances over the life of the Master Plan. The FSUP is designed to incorporate both the Full Site Utilization Plan and Sheet L1.1, included in Appendix E and reproduced in smaller scale here, as well as the phasing plan described in Section 5.2 below.

The FSUP indicates both the potential building and integral open space contemplated for Phase 1, as well as a plan for full build-out of 1.5 million GSF. If future buildings occupy part of the area designated for

parking, then a parking structure may be necessary to accommodate displaced spaces. Zones where such changes might occur are shown on the plan.

Building footprints on the FSUP and accompanying phasing diagram show a possible configuration of buildings, parking and open space in each of five blocks of development. (VMC 20.690.060).



**FIGURE 5.A - FULL SITE UTILIZATION PLAN**

The Full Site Utilization Plan shows building footprints and the open spaces between them conceptually. If any buildings need to be sited west of the service road, they will be confined to the area indicated on the west half of the parking area. The size and configuration of each building will be determined before each phase and will be submitted to the City for building permits. Until then, the building footprints shown illustrate the mix of potential buildings.

LEGEND	
	HP 68 ACRES PROPERTY LINE
	FUTURE ROW LINE
	POTENTIAL BUILDING
	POTENTIAL BUILDING OR PARKING
	PARKING AREA
	SERVICE YARD
	OPEN SPACE
	STORMWATER TREATMENT
	PLAZA
	EXISTING TREE
	PROPOSED FUTURE TREE
	HP IDENTIFICATION SIGN
	HERITAGE MONUMENT MARKER (AT 3RD ST AND NE 184TH AVE)

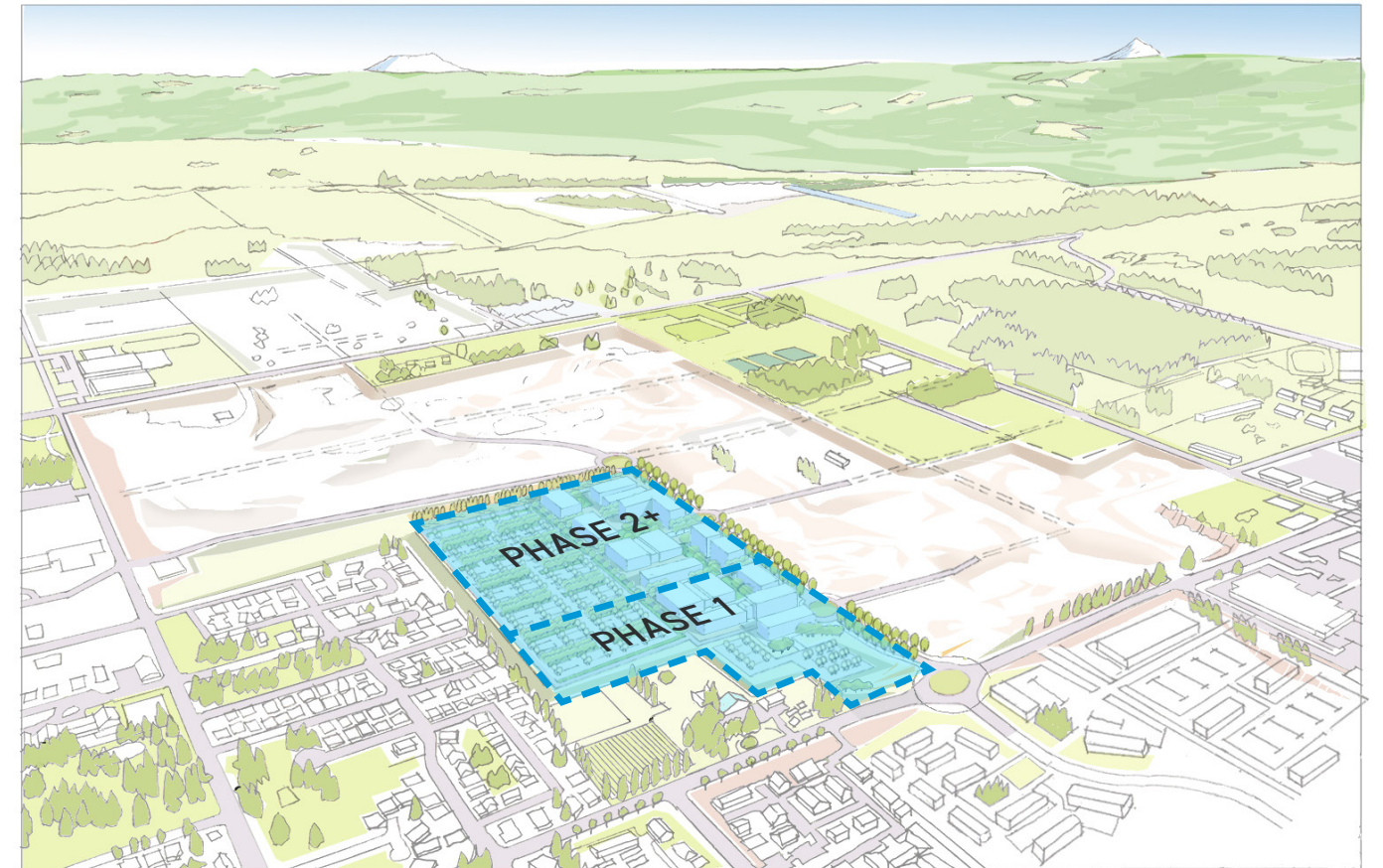
## 5.2 PHASES OF DEVELOPMENT

Buildings and associated improvements have been planned for construction in multiple phases and five blocks. These are shown conceptually in the accompanying graphics. HP will make determinations about the building and use mix for each phase based on market conditions, internal employee demand and other factors that will inform HP's decision-making in the future. Consequently, the potential building footprints could vary in size and position. These phasing diagrams show the approximate disposition of the 1.5 million GSF that may be built. HP may change phases, provided the development is fully identified in the FSUP and this Master Plan.

As each successive phase is programmed,

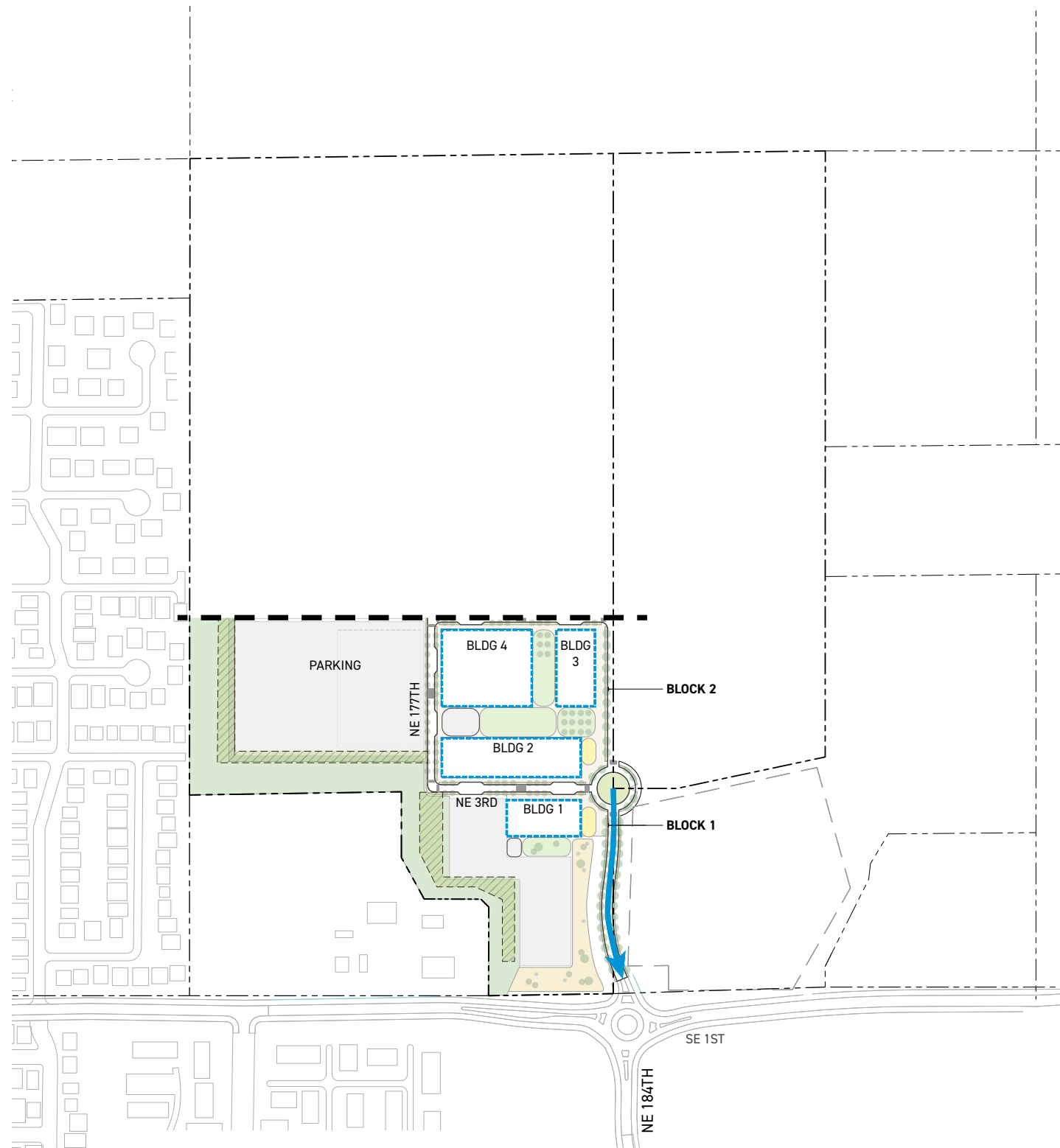
arrangements of buildings and open spaces can be refined and will be presented as part of the building permit process. However, the conceptual phasing plan shown here indicates the general principles of block dimensions and distribution of anticipated developed space. (VMC 20.690.060).

Phase 1 will comprise the initial 330,000 GSF of development as described in the Development Agreement.



**FIGURE 5.B - PHASES OF DEVELOPMENT**

Phase 1 will be located on the southern portion of the site adjacent to the intersection of SE 1ST and 184TH. Subsequent phases of development will occur in the northern portions of the site.



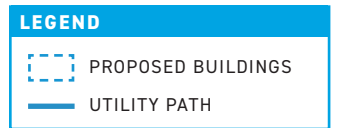
**FIGURE 5.C - PHASE 1**

In Phase 1, all utilities and services, as well as vehicle access are provided from the short extension of 184TH extending north from the intersection of 1ST Street.



**FIGURE 5.D - PHASE 2+**

Development exceeding the 330,000 GSF in Phase 1 will require construction of infrastructure including roadways and utilities north to NE 9TH Street.





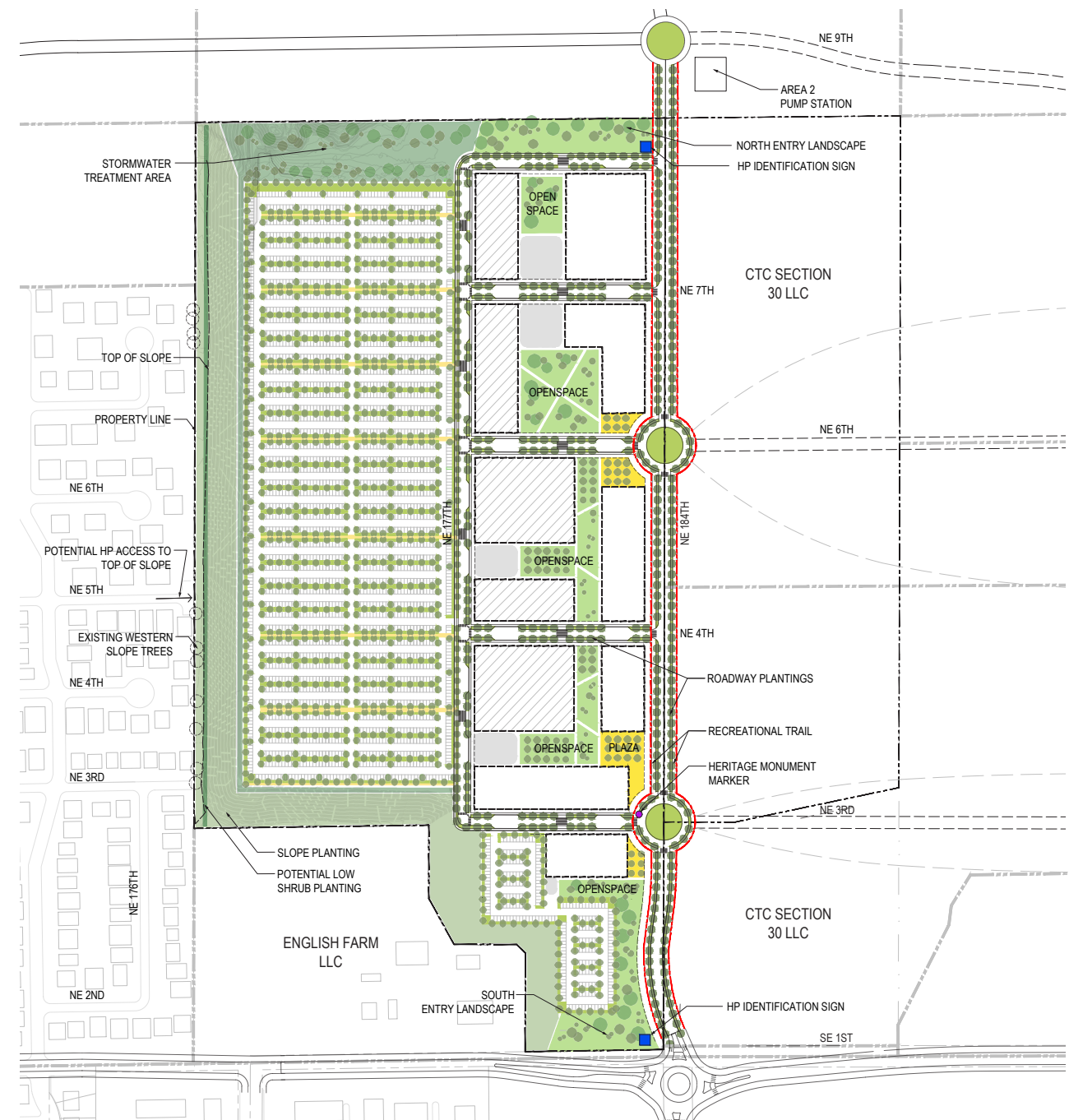
# 06/ LANDSCAPE PLANNING

## 6.1 LANDSCAPING

The intended character and identity of Section 30 portrayed in the Plan and Design Guidelines will be respected by landscape design of the HP site. Landscape design will favor drought-tolerant native plant materials and will create an environment that encourages walking. Generous use of trees on streets and parking lots will modify microclimates and define distinct outdoor spaces. In addition to providing shade, canopy trees will screen views of parked cars from within the buildings as well as from public streets. Raingardens, swales, and other landscaped areas will be designed in conjunction with the stormwater system, filtering runoff and directing cleaned water to designated groundwater infiltration areas. All non-paved areas not occupied by buildings will be treated with planted landscaping or hardscape. If security walls or fences are found to be necessary, their design will be closely integrated with that of adjacent architecture and landscaping. (SP UD-14) (DG B.2.3.a, E.1.3, E.2.1).

Retaining walls taller than 6' are not anticipated to be required, as perimeter grade transitions are managed with planted sloped terrain. Landscape design will address plant and vegetation criteria for building setbacks as defined in the Design Guidelines. (DG E1.1, E2.2).

Building setbacks will be planted with a variety of largely native groundcovers, shrubs and trees, using higher maintenance annuals and perennials only at places of special emphasis such as building entrances. Drought tolerant plants will be preferred in order to minimize water use and maintenance, once they have become established. Rain-gardens and recharge zones for cleaned stormwater will generally be inaccessible except for periodic maintenance.



**FIGURE 6.A - MASTER LANDSCAPE PLAN**

The landscape on the HP property will establish a distinctive sense of place. Street trees will act as screens across the extent of development. The tree canopy will mature to cover much of the property, provide shade, give some weather protection, improve microclimates and 'clothe' the streets, buildings and parking lots. Landscaping will also integrate the stormwater management system.

LEGEND	
	HP 68 ACRES PROPERTY LINE
	FUTURE ROW LINE
	OPEN SPACE
	PLAZA
	EXISTING TREE
	PROPOSED FUTURE TREE
	HP IDENTIFICATION SIGN
	HERITAGE MONUMENT MARKER (AT 3RD ST AND NE 184TH AVE)



**TREES**

The west side of the property has an existing stand of evergreen trees and a steep embankment. Some of these existing evergreen trees were permitted for removal because of their poor health. The replacement trees will be planted in the gateway entrance areas to the site and within the HP property. If there are areas along the top of slope to plant these types of trees that are not blocking any residence's views to the east, or raise concerns related to high wind events, those locations will be taken into consideration. To meet the screening requirements on the western boundary of the HP property, it is proposed to use a continuous evergreen hedge along the eastern edge of the area with a fence along the western boundary. Preserving views to the east for the existing residences is one Subarea Plan goal, while tree canopy along HP's western boundary is a separate goal. In order to resolve the competing goals, HP proposes this evergreen hedge screening layer. Ample trees are planted across the site as identified in the Master Landscape Plan to achieve

the City's canopy goals. The western slopes will be planted with an adaptive native grass seed mix to provide a green seasonal variety buffer and provide erosion control to the slope. (SP UD-7).

Each entry landscape will be a mix of large and medium canopy trees. Some areas will be dedicated to replacement trees for the mitigation of the trees removed along the western top edge of the property. Shrubs and groundcovers will surround the main entry features and the surrounding landscape areas will utilize a mix of drought tolerant low maintenance grasses. The entries will provide gateways to the property utilizing the natural topography and man-made forms to create a unifying entry to the HP property. (DG A.6.1.d).

Each open space and plaza area will incorporate an array of tree types that are consistent with the scale of the outdoor spaces. A mix of shrubs and groundcovers will be utilized to form the shapes of these spaces and allow generous areas for pedestrians to circulate throughout the seasons. (SP UD-3, 14, 16).

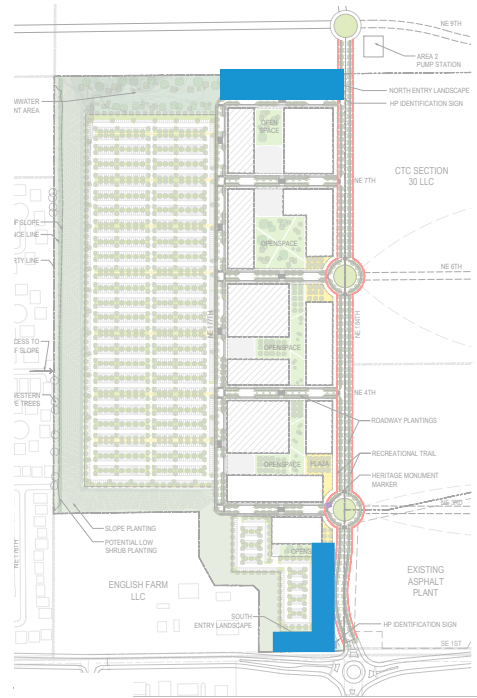
**TREES**

	SCIENTIFIC NAME	COMMON NAME	HEIGHT	WIDTH
EXISTING TREES	<b>WESTERN SLOPE</b>			
	<i>Pseudotsuga menziesii</i>	Douglas Fir		
LARGE CANOPY TREES	<b>SOUTH AND NORTH ENTRIES</b>			
	<i>Cupressus nootkatensis</i>	Alaskan Yellow Cedar	120 ft.	20 ft.
	<i>Pseudotsuga menziesii</i>	Douglas Fir	100 ft.	20 ft.
	<i>Quercus garryana</i>	Oregon White Oak	65 ft.	50 ft.
	<i>Sequoiadendron giganteum</i>	Giant Sequoia	160 ft.	30 ft.
MEDIUM CANOPY TREES	<i>Acer x freemani 'Jeffersred'</i>	Autumn Blaze Maple	50 ft.	40 ft.
	<i>Betula nigra 'Cully'</i>	Heritage River Birch	40 ft.	30 ft.
	<i>Cupressus nootkatensis</i>	Alaskan Yellow Cedar	120 ft.	20 ft.
	<i>Quercus palustris</i>	Pin Oak	55 ft.	40 ft.
	<i>Ulmus 'Pioneer'</i>	Pioneer Elm	55 ft.	40 ft.
MEDIUM CANOPY TREES	<b>184TH AND SIDE STREETS</b>			
	<i>Carpinus betula 'Fastigata'</i>	European Hornbeam	35 ft.	25 ft.
	<i>Nyssa sylvatica 'Haymanred'</i>	Red Range Tupelo	35 ft.	20 ft.
	<i>Tilia americana x eucholora 'Redmond'</i>	Redmond Linden	35 ft.	25 ft.
	<i>Zelkova serrata 'Village Green'</i>	Village Green Zelkova	40 ft.	38 ft.
MEDIUM CANOPY TREES	<b>PARKING LOT TREES</b>			
	<i>Carpinus betula 'Fastigata'</i>	European Hornbeam	35 ft.	25 ft.
	<i>Celtis occidentalis</i>	Hackberry	45 ft.	35 ft.
	<i>Gleditsia triacanthos 'Shademaster'</i>	Shademaster Honey Locust	45 ft.	35 ft.
	<i>Ulmus 'Frontier'</i>	Frontier Elm	40 ft.	30 ft.
ALL CANOPY TYPE TREES	<b>OPEN SPACE AND PLAZA TREES</b>			
	<i>Acer griseum</i>	Paperbark Maple	25 ft.	20 ft.
	<i>Cornus kousa x nutalli 'KN 30-8'</i>	Venus Dogwood	25 ft.	20 ft.
	<i>Cupressus nootkatensis</i>	Alaskan Yellow Cedar	120 ft.	20 ft.
	<i>Maackia amurensis</i>	Amur Maackia	30 ft.	30 ft.
	<i>Parrotia persica</i>	Persian Parrotia	30 ft.	20 ft.
	<i>Pseudotsuga menziesii</i>	Douglas Fir	100 ft.	20 ft.



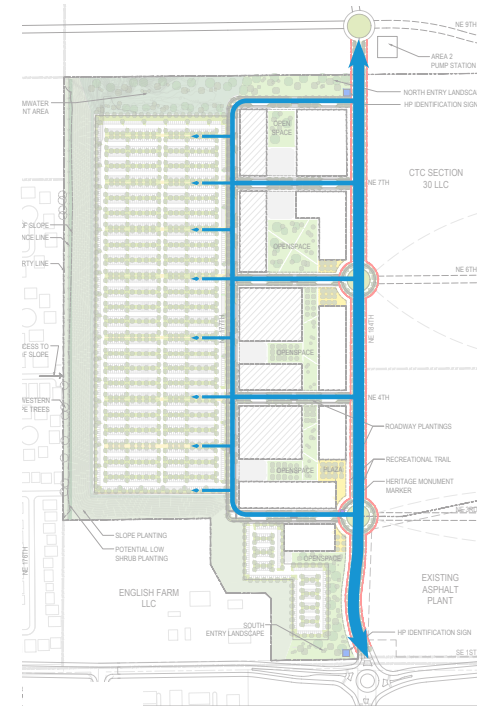
**FIGURE 6.B - TREES**

The proposed tree master plan characterizes the intent of tree density city code requirement. As designs are implemented at this site, the tree density requirement will be addressed at each phase.



**FIGURE 6.C - NORTH AND SOUTH ENTRY**

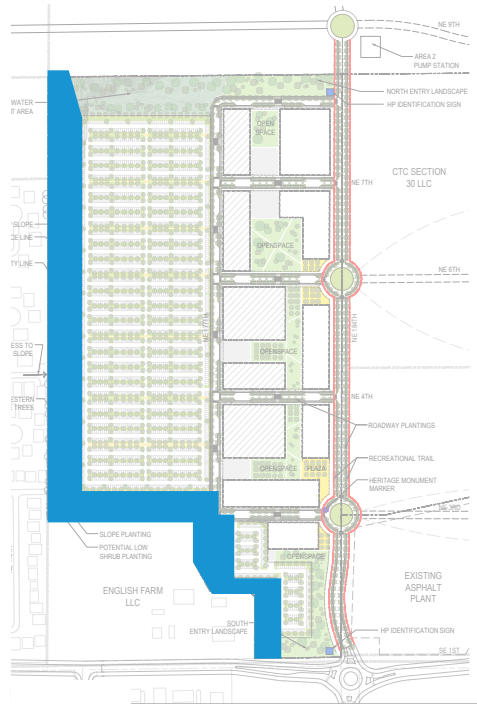
The north and south entry will mark entry and arrival to the Master Plan areas. In addition, the north entry area will be part of the stormwater management system. The adjacent images showing larger land forms that illustrate the opportunity to combine the functional requirements of landscape with a unique and identifiable design.



**FIGURE 6.D - STREETS**

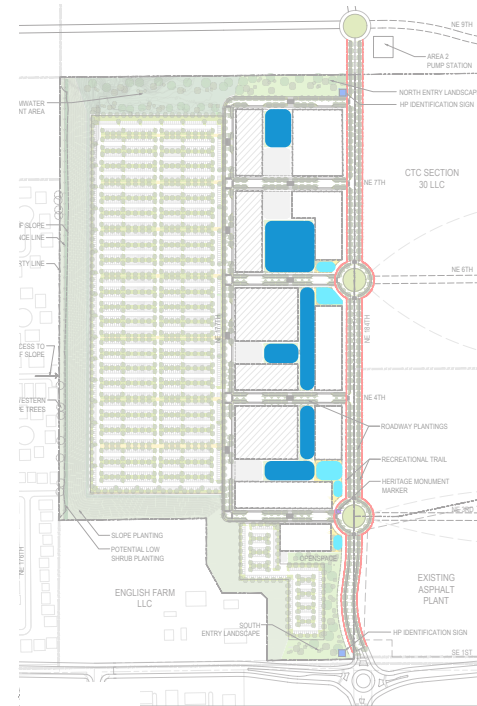
Street trees will line public and private streets, as well as delineate circulation routes through the parking area.





**FIGURE 6.E - SOUTHERN AND WESTERN SLOPE**

Hillsides along the south and west edges of the HP property will be graded to a uniform and stable 2:1 slope and planted with meadow grasses and wild flowers. Access will be provided for maintenance, potentially along NE 5TH Street. Native and drought resistant plant materials will be used to minimize maintenance and water consumption. The existing 2:1 slopes are too steep for successful long-term tree planting due to the required maintenance access, poor soil conditions, and possible impacts caused by high wind events. Other locations have been identified for tree planting that offer a higher likelihood for successful tree health and longevity.



**FIGURE 6.F - INTERNAL OPEN SPACE CONCEPTS**

Spaces between buildings will be designed as extensions of the workplace as well as providing for amenity and recreation use. Internal open space will also help tie together buildings with pedestrian circulations paths.



## 6.2 SIGNAGE

The design and location of signage throughout the HP property will be consistent, and closely coordinated with design of the buildings and landscape. Monument signs at the intersections of NE 184<sup>th</sup> Avenue with SE 1<sup>st</sup> Street and NE 9<sup>th</sup> Street will be designed with gateway plantings at each of these principal points of entry. Directional signs will be located where sight lines from primary approaches are clear for all modes: drivers, pedestrians and bicycle riders. Identification of buildings will typically be attached to the buildings. Sign design will occur along with the building design to encourage architectural compatibility. Graphic uniformity between signs will be addressed as Phase 1 commences.

Quantity of signs will be permitted to vary. Building mounted signage will be permitted on all public and private street facing sides; and it may be lighted or back-lighted with steady light. Directional and wayfinding signage will be placed at junctions of sidewalks, streets, plazas, service and any combination of these. Code required signage will be adopted as required or appropriate.

An exception is the dedicated, heritage monument marker describing the history of the site proposed for a location near the northwest corner of the 3<sup>rd</sup> Street roundabout on 184<sup>th</sup> which may not be governed by master plan criteria. The heritage monument marker will be an improved area not to exceed eighty square feet and will include landscaping, one or two benches and an interpretative sign describing the history of the property that will be dedicated to the City. (DA § 6(e), Ex. 4, p. 5).



HP IDENTIFICATION SIGN



DIRECTIONAL SIGN



BUILDING IDENTIFICATION SIGN



HERITAGE MONUMENT MARKER

### FIGURE 6.G - SIGNAGE

Monument signs will announce the HP property at the main points of entry from SE 1ST Street and NE 9TH Street. Within the property, direction signs will be coordinated with lighting to be visible to pedestrians and cyclists as well as drivers after dark. Signs identifying individual buildings will typically be attached to them and designed to be compatible with the architecture.

### 6.3 SCREENING AND BOUNDARIES

Views of the HP site from 184<sup>th</sup> Avenue will be limited to building frontages and glimpses along the driveways between building groups. Parking lots will be partially screened by street trees west of the buildings and will be further obscured by rows of trees planted in the parking lots, providing a substantial increase in tree canopy over the site. Parking lots will be obscured from view from properties west of the site by the 40' to 50' elevation difference and by the trees and other plantings that separate them. (SP TR-22) (DA Ex.6, p. 4).

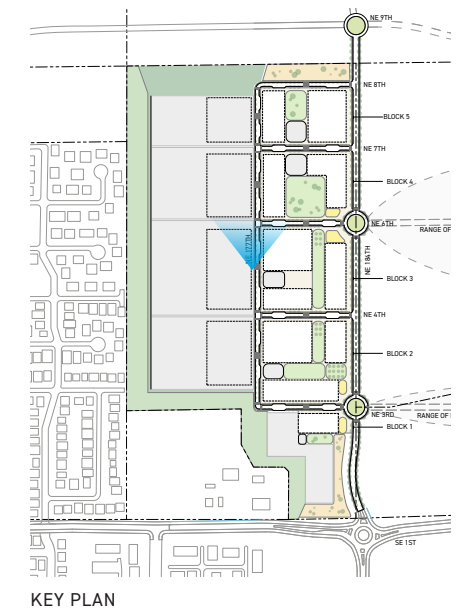
Service yards will all be located on the west side of the buildings and will be architecturally screened. In some cases, screening may include landscape materials such as pergolas. Utility meters and other equipment will similarly be screened. Any blank walls visible from public streets will be screened using a variety of landscape and architectural materials depending on particular circumstances at each site. (SP UD-15)(DG D.3.1).

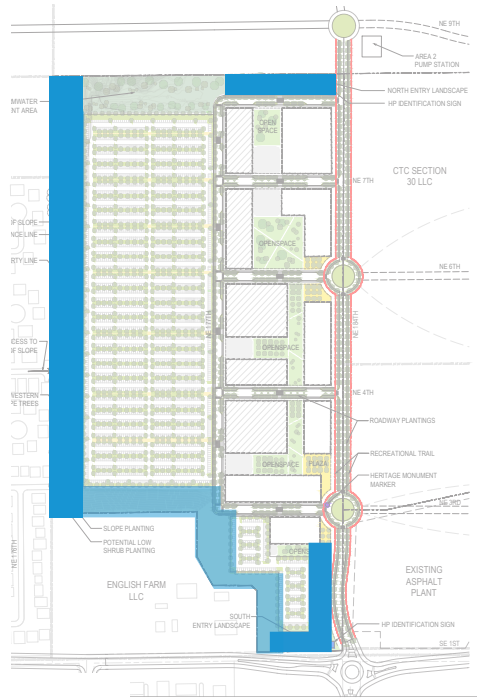
Walls and fences near the buildings will be of materials and design appropriate to adjacent architecture. Those elsewhere on the site will be integrated with landscape design, their effect on screening or focusing views being carefully considered.



**FIGURE 6.H - CONCEPTUAL VIEW OF PARKING AREA AND BLOCK 3 LOOKING NORTH FROM NE 177TH AVENUE**

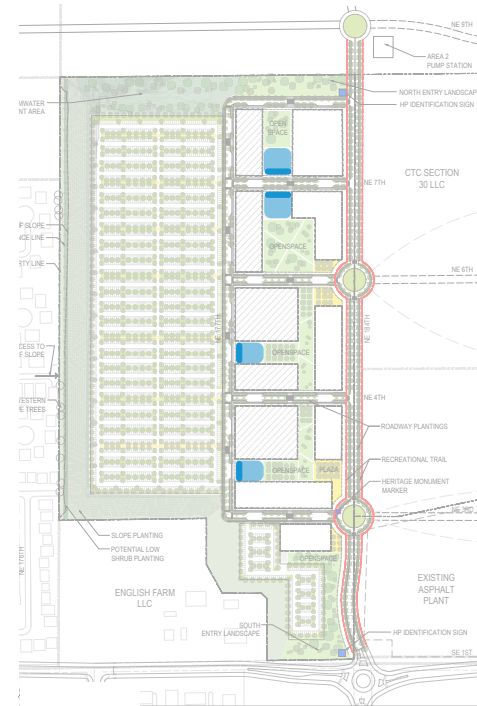
Hedges, walls and fences will be designed integral to the buildings and landscape to which they relate. For users of 184<sup>th</sup>, parking west of the buildings will be partially screened by street trees and trees within the lots. Service areas will not be visible from 184<sup>th</sup> Avenue.





**FIGURE 6.I - SLOPE AND ENTRY SCREENING**

Examples of potential landscape treatment, including a small evergreen hedge proposed along the western boundary, and gateway landscaping are provided here.



**FIGURE 6.J - SERVICE YARD SCREENING**

The service yard areas throughout the site will be screened with complementary materials that are consistent with the building enclosure. City standards and Section 30 design guidelines will be followed and each area will be buffered with appropriate landscape materials where those materials can be accommodated. These service areas will be as inconspicuous as possible and not impede the circulation patterns of the HP property.

