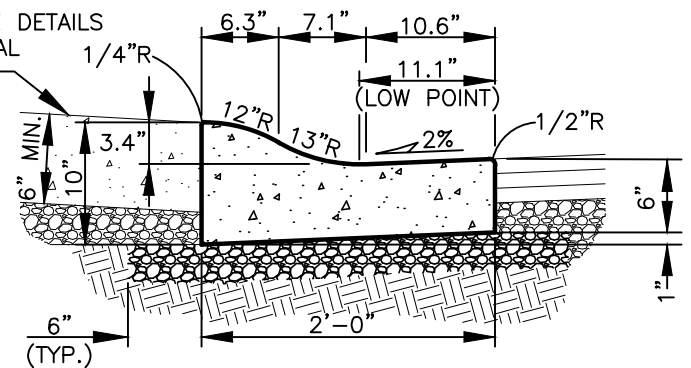
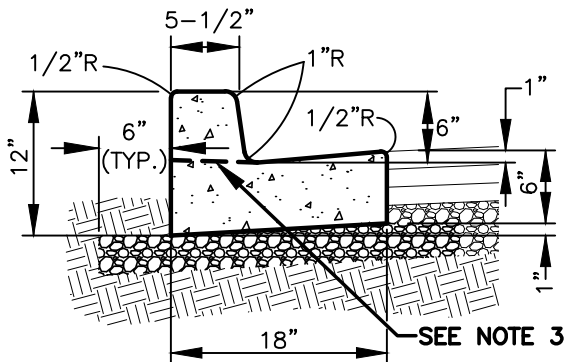


**TYPE E-1 CURB**

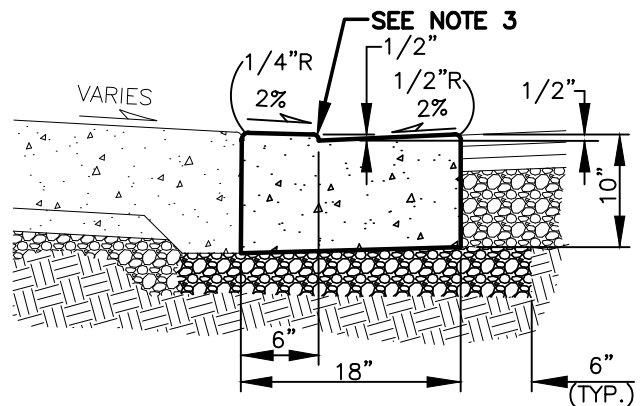


**ROLLED CURB AND GUTTER**

USE OF ROLLED CURB AND GUTTER PERMITTED IN CUL-DE-SAC BULBS ROAD MODIFICATION REQUIRED FOR OTHER USES.



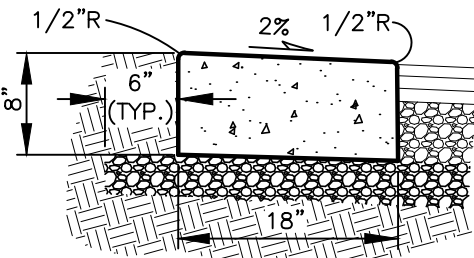
**TYPE A-1 CURB AND GUTTER**



**CURB AND GUTTER AT DRIVEWAY DROP**

**NOTES:**

1. CONCRETE CURBS SHALL BE 3000 PSI MIN. (CL 3000), 3-1/2" SLUMP (MAX.), DRIVEWAY OPENINGS SHALL BE 4000 PSI MIN. (CL 4000).
2. CURBS ADJACENT TO PAVEMENT OR SIDEWALK SHALL HAVE EXPANSION AND/OR CONSTRUCTION JOINTS TO MATCH EXISTING PATTERNS. 3/8" EXPANSION JOINTS SHALL BE PLACED ON BOTH SIDES OF CATCH BASINS, AT TOPS OF DRIVEWAYS, ALL CHANGES IN DIRECTION, AND AS DIRECTED BY THE INSPECTOR. CONTRACTION JOINTS TO BE PLACED AT 15' MAXIMUM SPACING.
3. FOR CURB DROPS AT DRIVEWAYS 1/2" LIP WITH A 1/2" TOOLED EDGE, SEE **RAMP LIP, DRIVEWAY LIP AND DETECTABLE WARNING PATTERN DETAIL T02-15**.
4. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% MAXIMUM DRY DENSITY (3" MIN. DEPTH).
5. SEE **PAVEMENT RESTORATION/WIDENING AT CURBS DETAIL T05-01A**.
6. CURB TO BE MEDIUM BROOM FINISHED, PARALLEL TO GUTTER LINE.
7. WHERE MATCHING EXISTING CURBS, ALL EXISTING EDGES SHALL BE SAWCUT.
8. WHEN ATTACHED SIDEWALKS ARE USED WITH ROLLED CURB AND GUTTER, THICKENED SIDEWALKS (6" MIN.) SHALL BE CONSTRUCTED UNDER THE SAME CONSTRUCTION CONTRACT.
9. FOR CURBS AROUND MEDIANS, USE 12" WIDE CURB OR CURB AND GUTTER. SEE **TYPICAL PEDESTRIAN REFUGE SECTION A-A DETAIL T02-17C** WHERE APPLICABLE.



**FLAT CURB EDGE DETAIL FOR NARROW ALLEY WAYS**

(SEE DETAILS T10-25A AND T10-25B)



**CONCRETE CURBS**

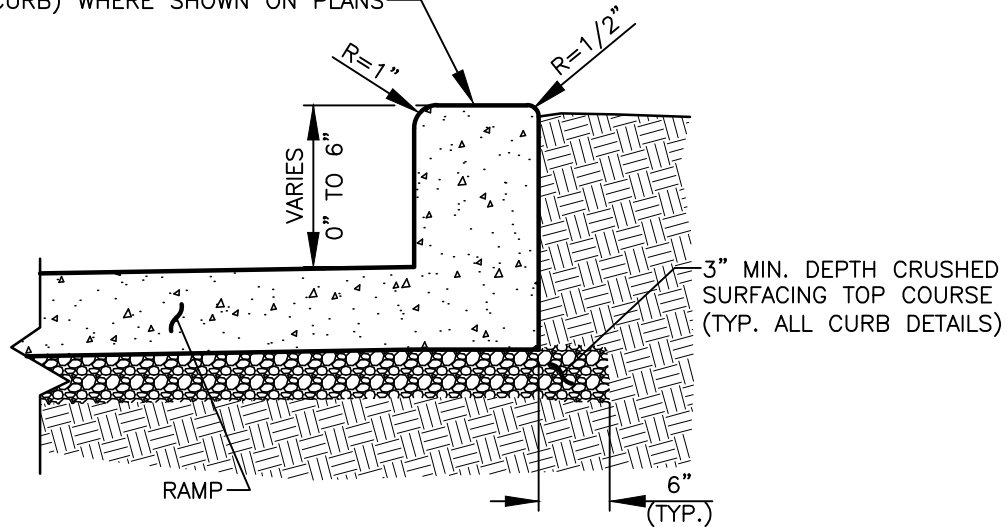
CITY OF VANCOUVER  
DEPARTMENT OF PUBLIC WORKS  
TRANSPORTATION DIVISION

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5	MAHE	3/24

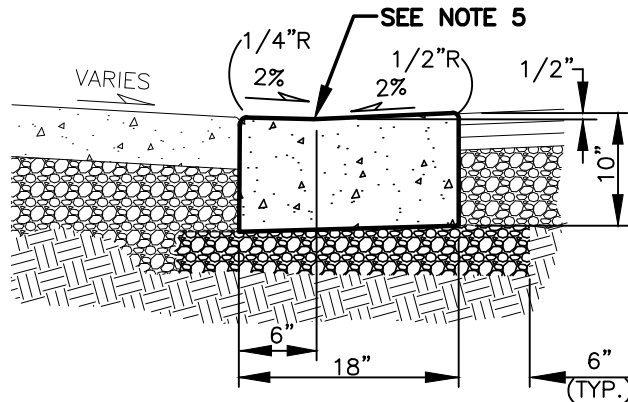
STD. PLAN NO.

**T01-01A**

CONSTRUCT 6" WIDE (NOMINAL) STD.  
VERTICAL CURB (GRADE CORRECTION  
CURB) WHERE SHOWN ON PLANS



**MONOLITHIC CURB AND ADA RAMP**



**CURB AND GUTTER AT ADA RAMP**

**NOTES:**

1. CONCRETE CURBS SHALL BE 3000 PSI MIN. (CL 3000), 3-1/2" SLUMP (MAX.).
2. TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
3. WHERE ADA RAMP REQUIRES GRADE CORRECTION CURB (BACK OR SIDES), CONSTRUCT CURB AND RAMP AS MONOLITHIC POUR. CURB ALONG ROADWAY OR PARKING LOT TO BE SEPARATE CURB POURS.
4. CURBS ADJACENT TO PAVEMENT OR SIDEWALK SHALL HAVE EXPANSION AND/OR CONSTRUCTION JOINTS TO MATCH EXISTING PATTERNS. 3/8" EXPANSION JOINTS SHALL BE PLACED ON BOTH SIDES OF CATCH BASINS, AT TOPS OF DRIVEWAYS, ALL CHANGES IN DIRECTION, AND AS DIRECTED BY THE INSPECTOR. CONTRACTION JOINTS TO BE PLACED AT 15' MAXIMUM SPACING.
5. FOR CURB DROPS AT ADA RAMP ZERO LIP FROM FLOW LINE, SEE **RAMP LIP, DRIVEWAY LIP AND DETECTABLE WARNING PATTERN DETAIL T02-15**.
6. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% MAXIMUM DRY DENSITY (3" MIN. DEPTH).
7. SEE **PAVEMENT RESTORATION/WIDENING AT CURBS DETAIL T05-01A**.
8. CURB TO BE MEDIUM BROOM FINISHED, PARALLEL TO GUTTER LINE.
9. WHERE MATCHING EXISTING CURBS, ALL EXISTING EDGES SHALL BE SAWCUT.
10. WHEN ATTACHED SIDEWALKS ARE USED WITH ROLLED CURB AND GUTTER, THICKENED SIDEWALKS (6" MIN.) SHALL BE CONSTRUCTED UNDER THE SAME CONSTRUCTION CONTRACT.



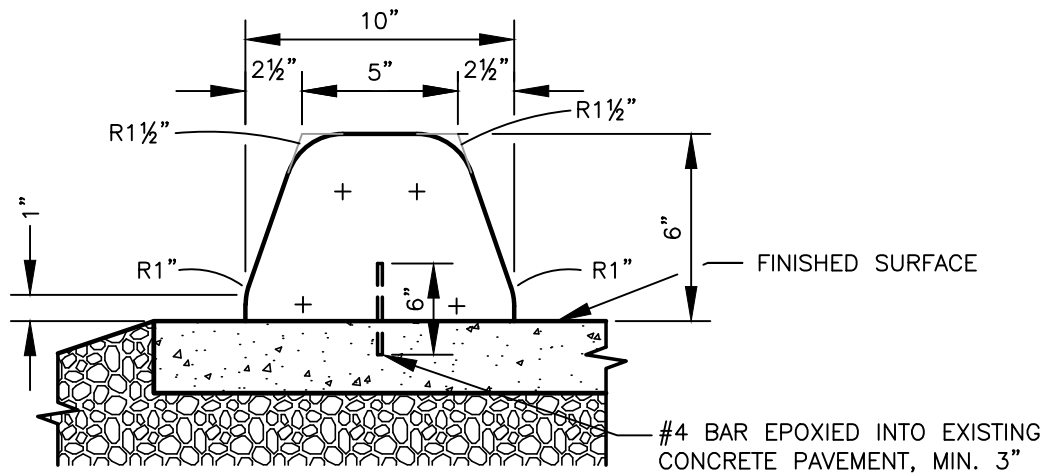
**ADA RAMP CURBS**

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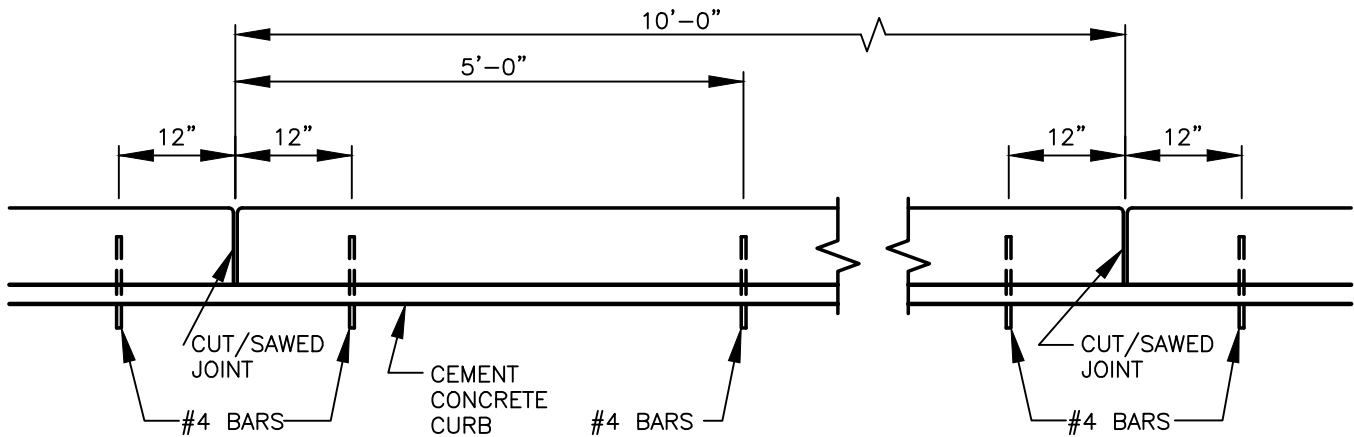
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CDC	MAHE	3/17
REVISION	APPROVED BY	APPROVAL DATE
4	MAHE	3/24

STD. PLAN NO.

**T01-01B**



### EXTRUDED CONCRETE DOWELED CURB



### SPACING OF ANCHOR BARS

#### **NOTES:**

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3-1/2" SLUMP (MAX.).
2. FOR CONCRETE CURBS, 3/8" EXPANSION JOINTS SHALL BE PLACED ON BOTH SIDES OF CATCH BASINS, AT TOPS OF DRIVEWAYS, ALL CHANGES IN DIRECTION, OR AS DIRECTED BY INSPECTOR. 1 1/2" CONTRACTION JOINTS TO BE PLACED AT 10' INTERVALS. SEE **CONCRETE JOINTS DETAIL T05-02**.
3. THE PAVEMENT SHALL BE DRY AND CLEANED OF LOOSE AND DELETERIOUS MATERIAL PRIOR TO PLACEMENT.
4. CONTRACTOR TO PROTECT EXTRUDED CURB UNTIL FULLY CURED.
5. TO BE USED FOR TEMPORARY USE OR ENGINEERS APPROVAL.



### **EXTRUDED CONCRETE DOWELED CURB**

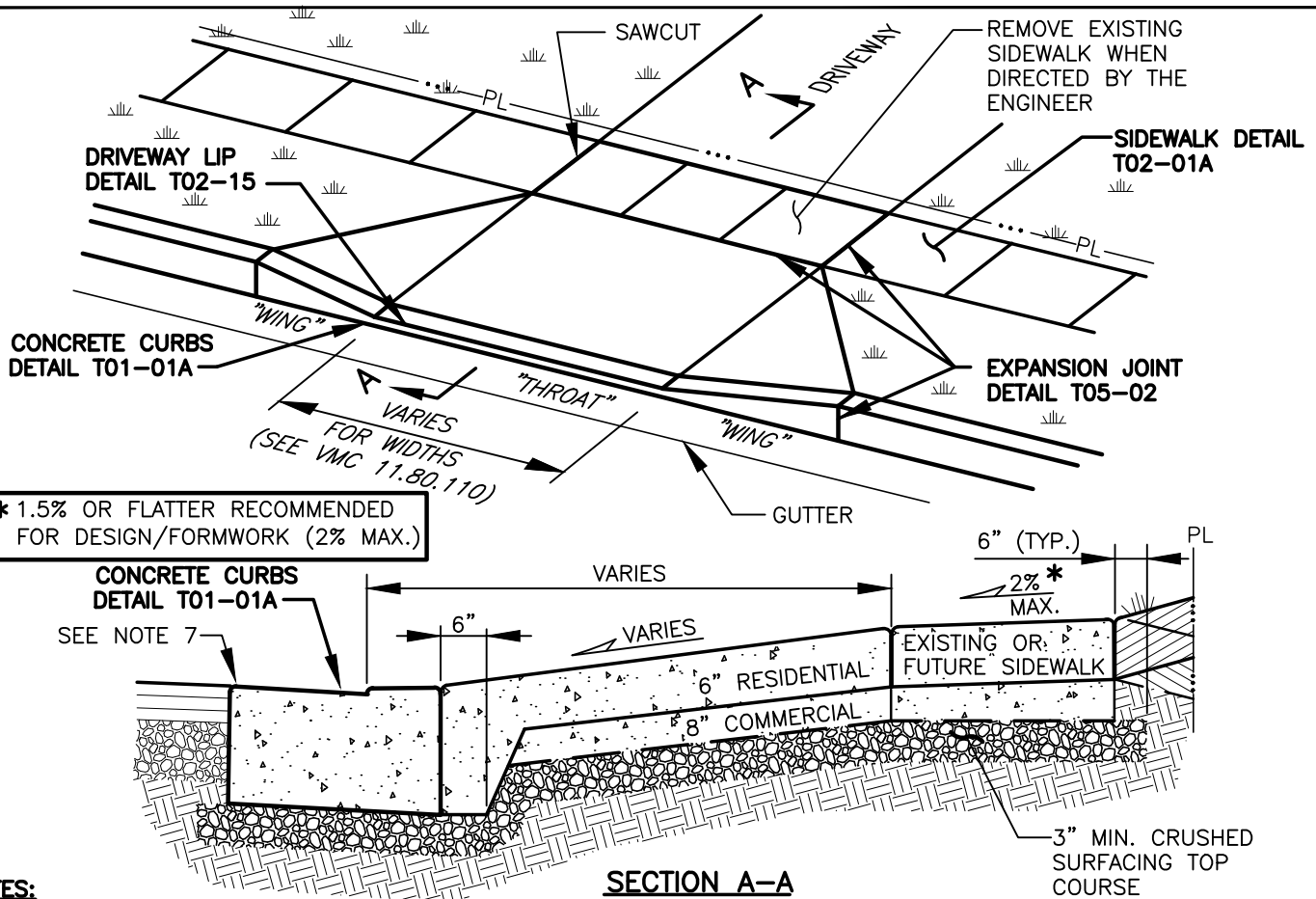
CITY OF VANCOUVER  
DEPARTMENT OF PUBLIC WORKS  
TRANSPORTATION DIVISION

DRAWN BY	APPROVED BY	APPROVAL DATE
CDC	<i>MAH</i>	8/04
REVISION	APPROVED BY	APPROVAL DATE
7	<i>MAH</i>	3/24

STD. PLAN NO.

**T01-02**





#### NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMMERCIAL DRIVEWAYS REQUIRE 8" CONCRETE WITH REINFORCING STEEL (6x6 - W2.9xW2.9 WWF, MIN.), 1 1/2" COVER FROM BOTTOM OF SLAB. RESIDENTIAL DRIVEWAYS REQUIRE 6" CONCRETE.
4. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
5. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
6. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
7. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
8. ALL EXISTING EDGES SHALL BE SAWCUT.
9. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
10. 3' WING MIN. FOR NON-ARTERIAL STREETS.
11. 45° ANGLE FOR WINGS ON ARTERIAL STREETS.
12. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
13. FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.



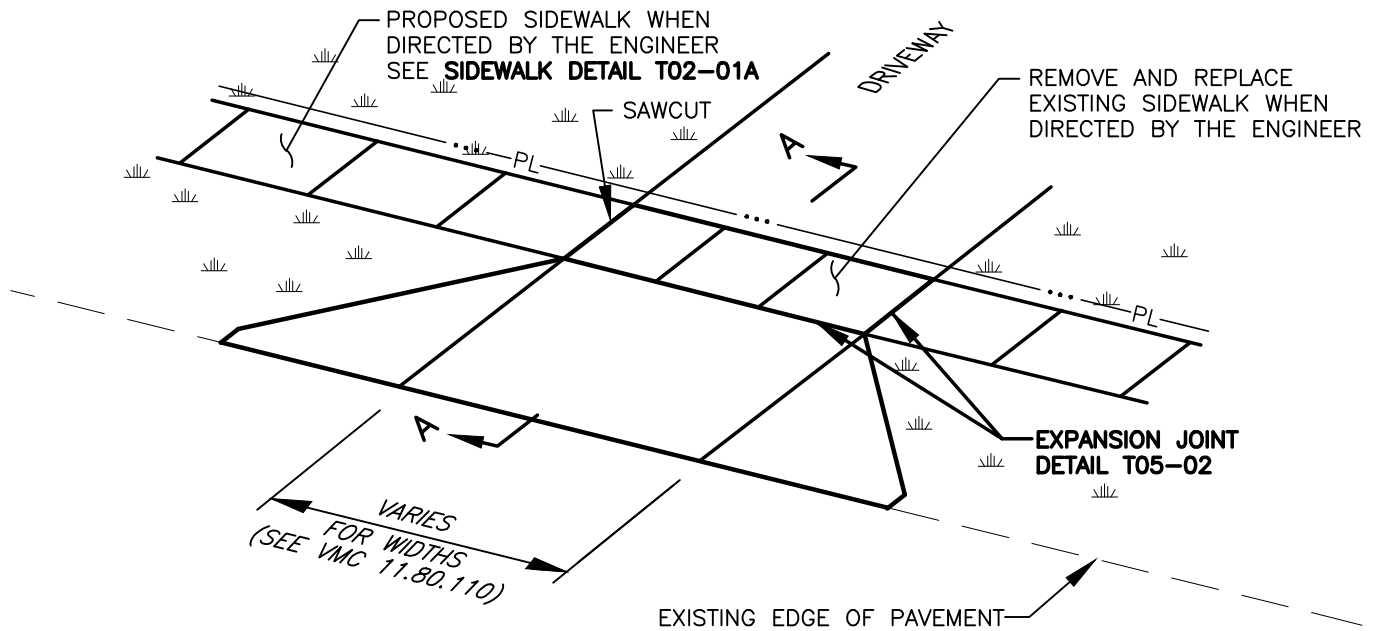
#### DRIVEWAY WITH DETACHED SIDEWALK

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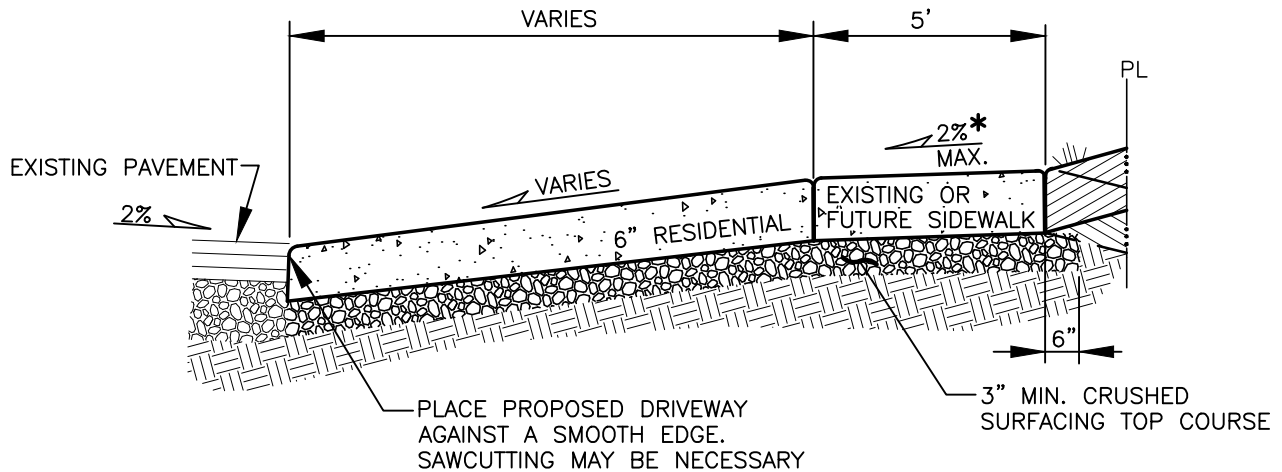
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CDC	MAHE	2/07
REVISION	APPROVED BY	APPROVAL DATE
7	MAHE	3/24

STD. PLAN NO.

**T01-04A**



\* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)



#### NOTES:

#### SECTION A-A

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
4. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
5. 3' WING MIN.
6. NO WATER METERS IN DRIVEWAY APPROACH OR WING.
7. FOR SINGLE FAMILY RESIDENCE DEVELOPMENTS ALONG ROADWAYS WITH OUT CURBS.
8. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.



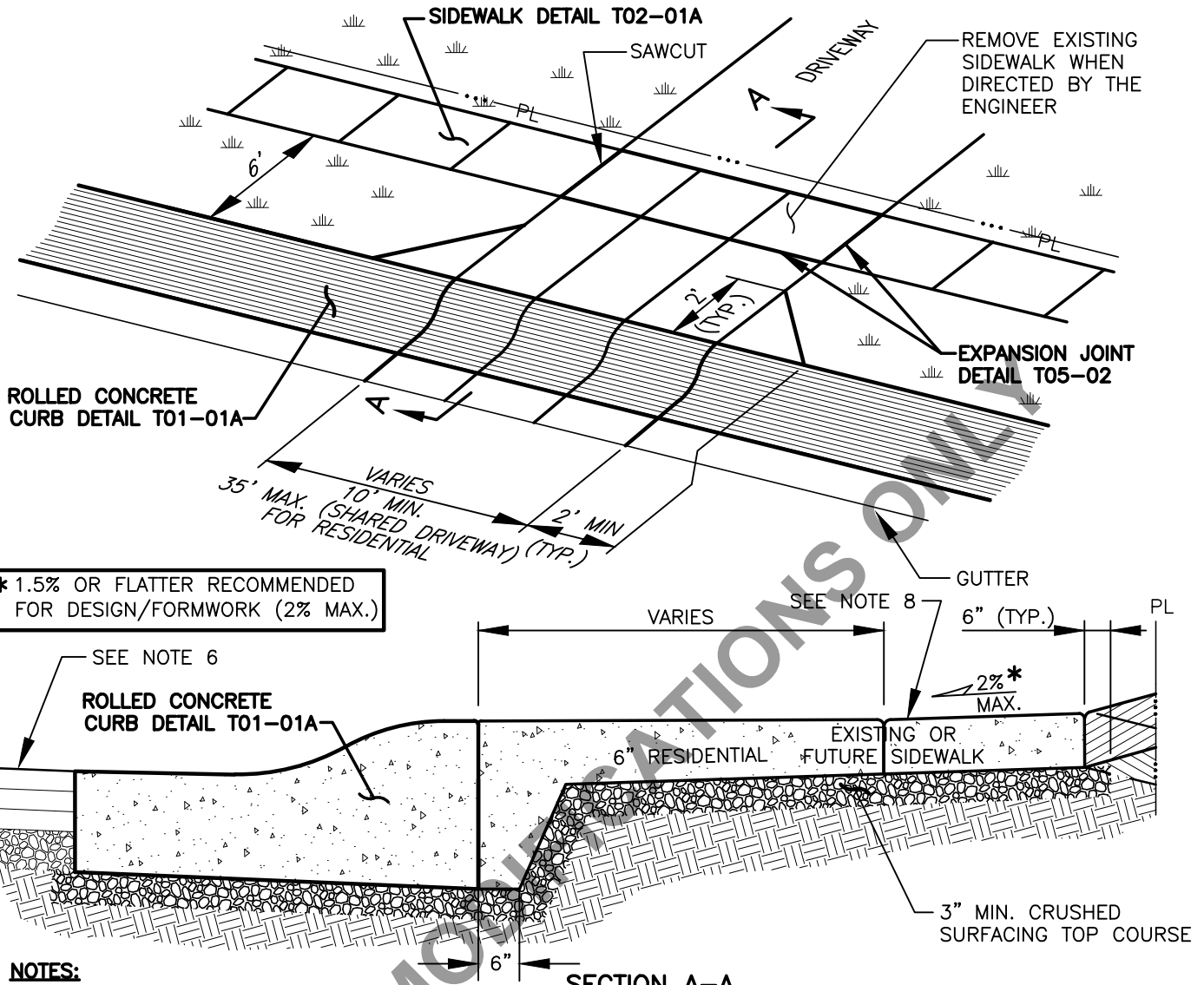
#### SINGLE FAMILY RESIDENCE DRIVEWAY WITH WINGS AND NO CURB

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CDC	MAHE	2/07
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7	MAHE	3/24

STD. PLAN NO.

**T01-04B**



#### NOTES:

1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
4. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
5. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01** WHEN CUTTING EXISTING CURB.
7. ALL EXISTING EDGES SHALL BE SAWCUT.
8. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
9. NO WATER METERS IN DRIVEWAY APPROACH OR WINGS.
10. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.

#### SECTION A-A



**CITY OF**  
**Vancouver**  
WASHINGTON

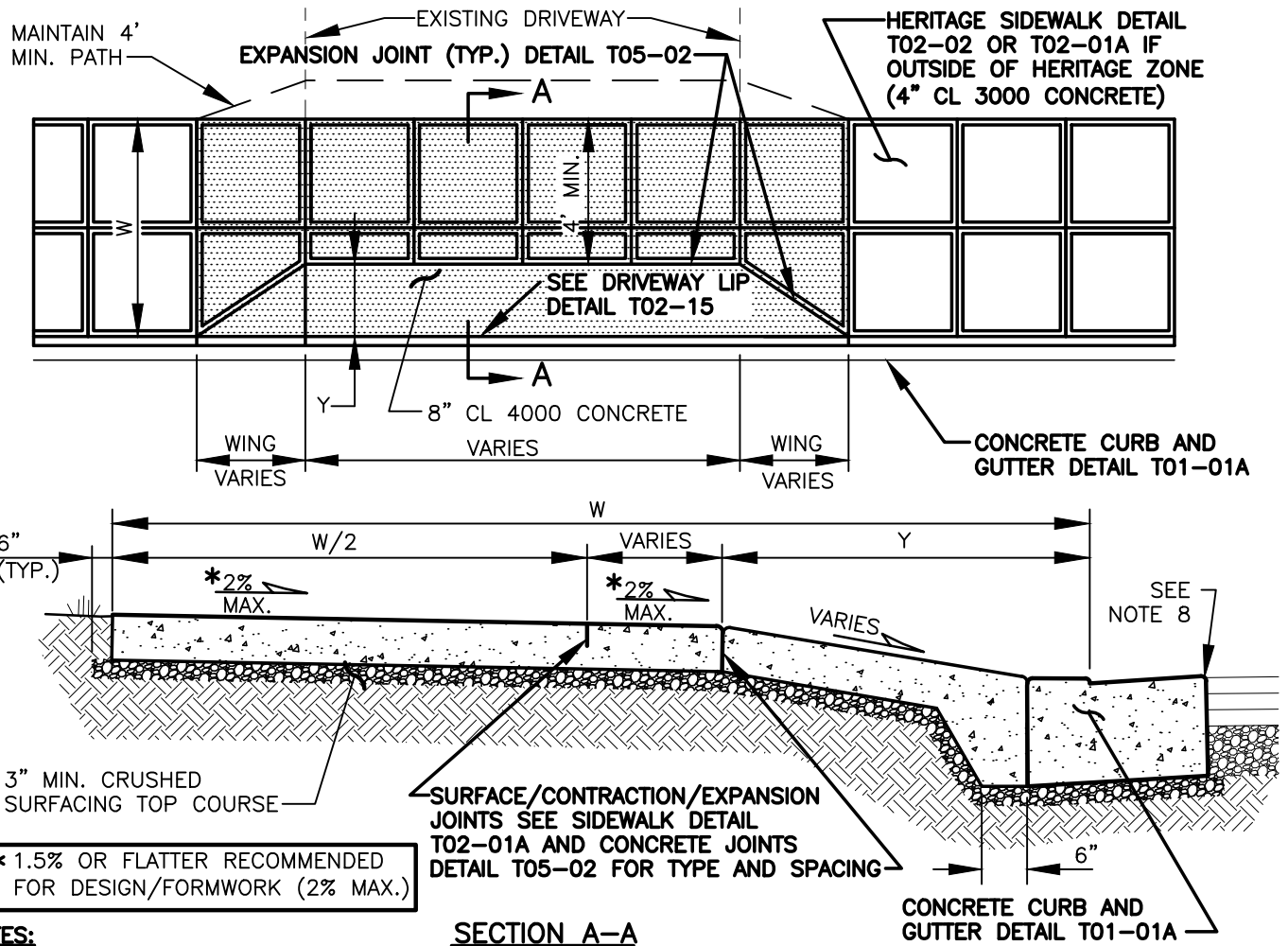
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TRANSPORTATION DIVISION

#### ROLLED CURB DRIVEWAY WITH DETACHED SIDEWALK

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REVISION	APPROVED BY	APPROVAL DATE
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STD. PLAN NO.

**T01-04C**



**NOTES:**

- IF W IS LESS THAN 8' IN WIDTH, THEN Y=2'. (IF W=<6', THEN PUSH OUT SIDEWALK BEHIND DRIVEWAY TO MAINTAIN 4' MIN. PATH). IF W IS MORE THAN 8' AND LESS THAN 12' IN WIDTH, THEN Y=W/2  
IF W IS GREATER THAN OR EQUAL TO 12' IN WIDTH, THEN Y=4'
- CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3½" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
- REINFORCING STEEL REQUIRED (6x6 - W2.9xW2.9 WWF, MIN.), MIN. 1½" COVER FROM BOTTOM OF SLAB.
- COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
- DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED BY THE CITY INSPECTOR. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
- PARALLEL JOINTS SHALL BE SEPARATED BY A MINIMUM OF 2'.
- SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
- ALL EXISTING EDGES SHALL BE SAWCUT.
- EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
- IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
- FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.



**DRIVEWAY WITH ATTACHED SIDEWALK OPTION A**

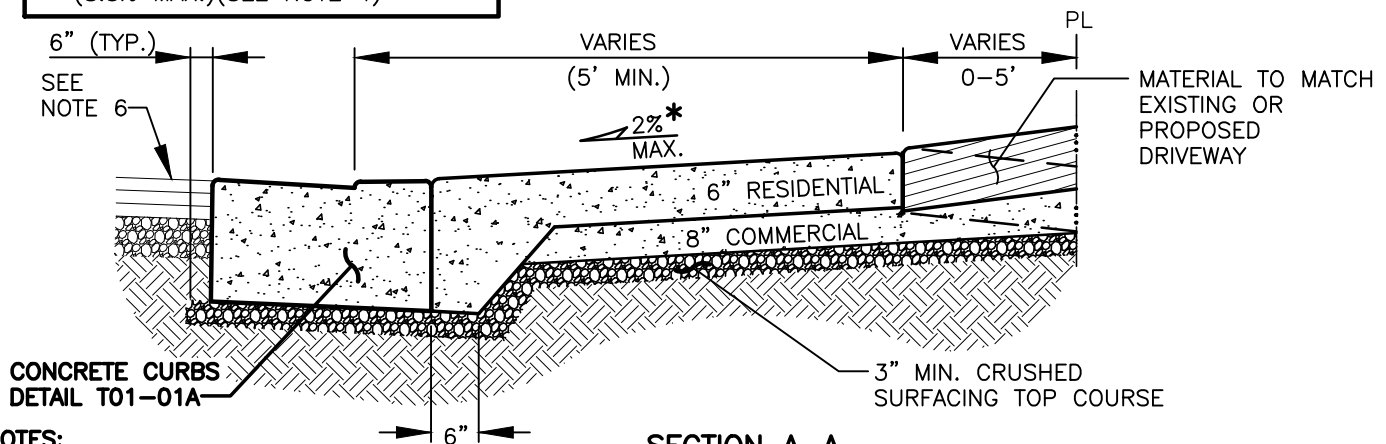
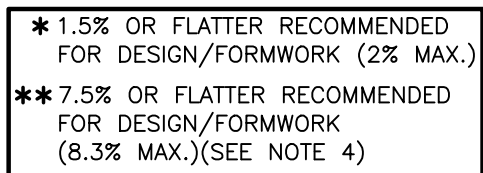
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DEPARTMENT OF PUBLIC WORKS  
TRANSPORTATION DIVISION

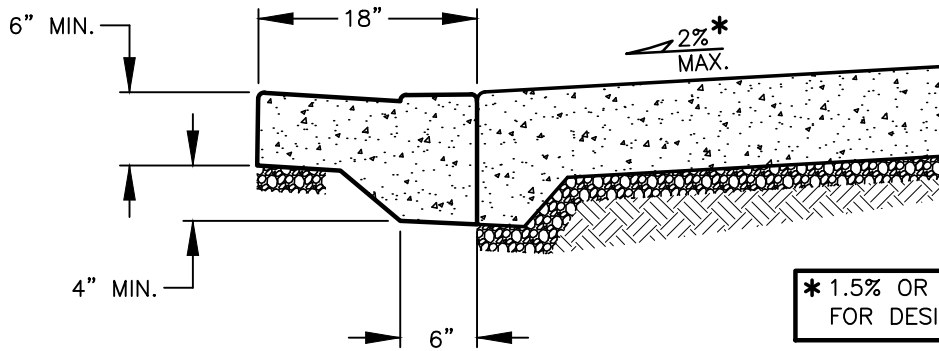
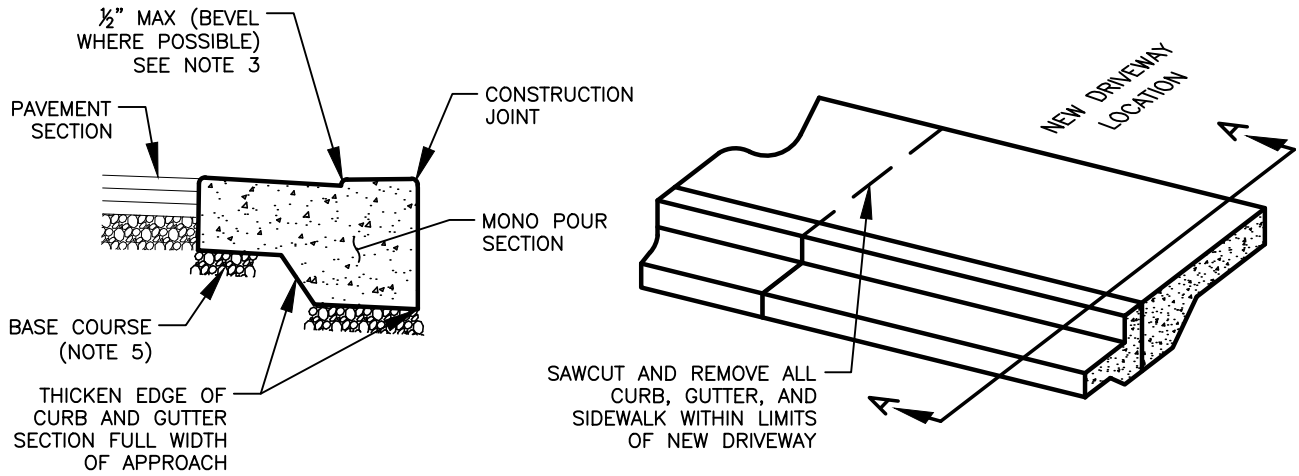
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7	MAHE	3/24

STD. PLAN NO.

**T01-05A**

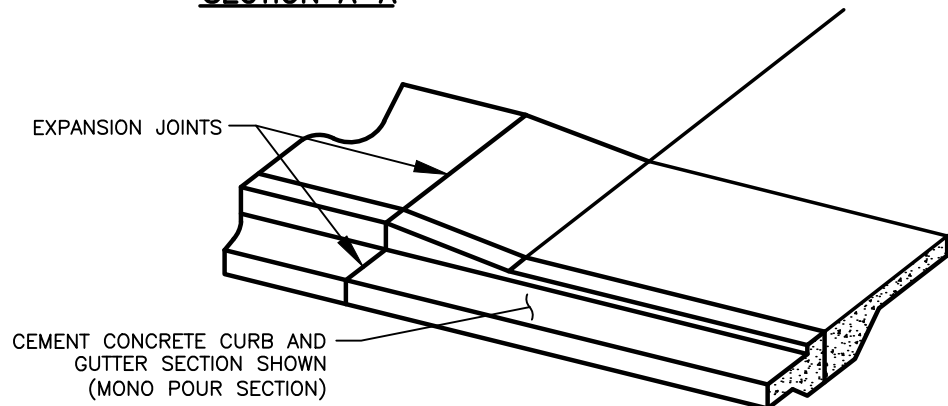






\* 1.5% OR FLATTER RECOMMENDED  
FOR DESIGN/FORMWORK (2% MAX.)

**SECTION A-A**



**HALF ISOMETRIC**

**NOTES:**

1. CONCRETE SHALL BE CLASS 4000 MIN.
2. CURB TO BE BRUSHED FINISHED. ALL EXISTING EDGES SHALL BE SAWCUT.
3. 1/2" VERTICAL LIP ACROSS APPROACH.
4. SUBGRADE SHALL BE COMPACTED TO 95% MAX DRY DENSITY.
5. MIN. 3" DEPTH CSBC, COMPACTED TO 95% OF MAX DRY DENSITY.
6. SEE **STANDARD DETAIL T05-02** FOR CONCRETE JOINTS.



**CONCRETE DRIVEWAY CUT DETAIL**

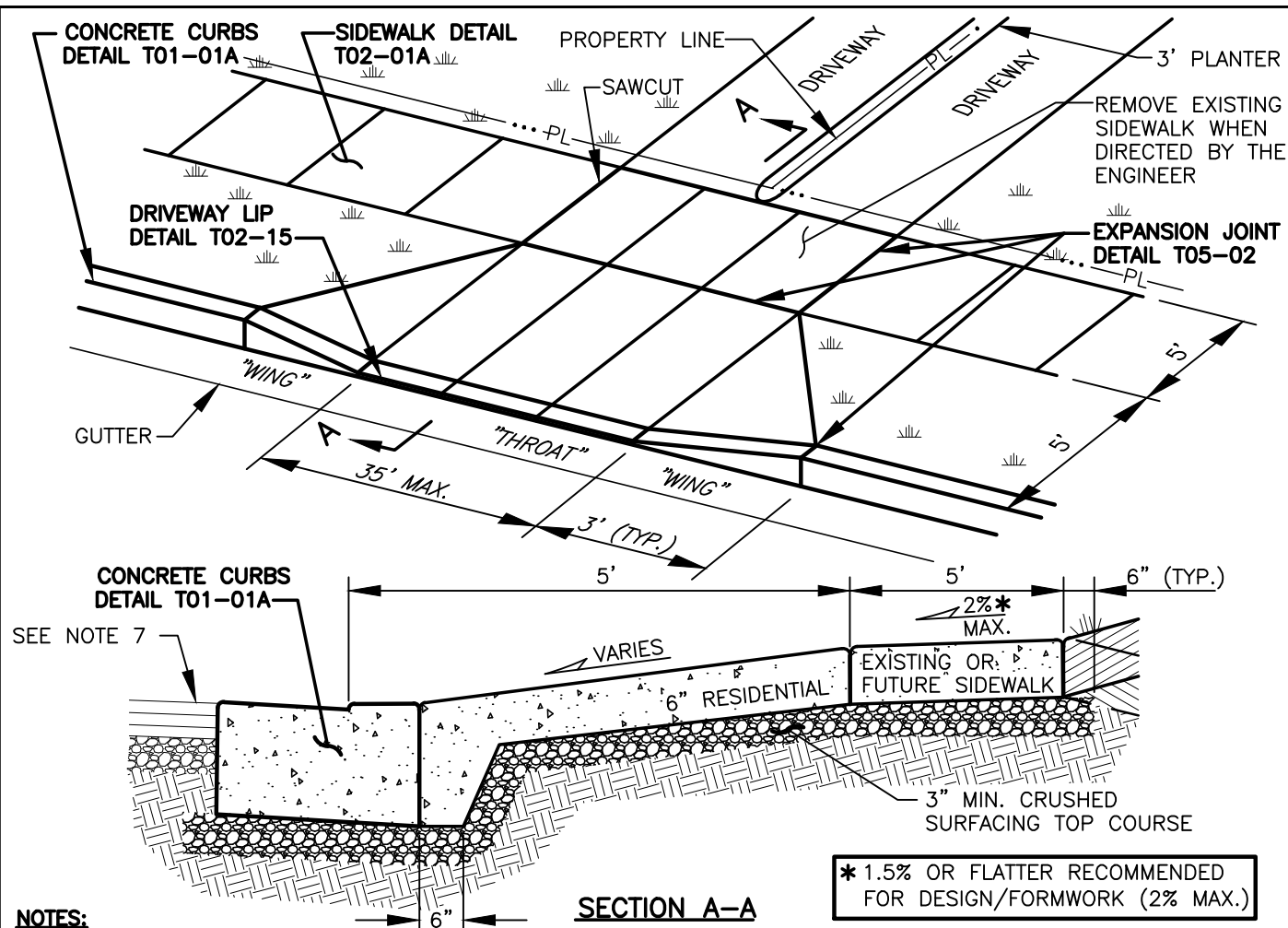
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CDC	MAHE	2/21
REVISION	APPROVED BY	APPROVAL DATE
3	MAHE	3/24

STD. PLAN NO.

**T01-06**





1. CONCRETE SHALL BE 4000 PSI MIN. (CL 4000), 3-1/2" SLUMP (MAX.), MEDIUM BROOM FINISH PARALLEL TO DRIVEWAY CENTERLINE.
2. TO BE USED WHERE CURB AND SIDEWALK ARE SEPARATED BY A PLANTER STRIP.
3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
4. DRIVEWAYS EXCEEDING 15' IN TOTAL WIDTH SHALL HAVE ADDITIONAL LONGITUDINAL JOINTS AS DIRECTED. JOINT SPACING SHALL NOT EXCEED 15'. SEE **CONCRETE JOINTS DETAIL T05-02**.
5. EXISTING CURB SHALL BE REMOVED TO EXISTING JOINT OR SAWCUT SUCH THAT 3' MIN. OF NEW CURB IS CONSTRUCTED ADJACENT TO NEW DRIVEWAY.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
7. ALL EXISTING EDGES SHALL BE SAWCUT.
8. STRUCTURAL SECTION OF DRIVEWAY TO BE EXTENDED THROUGH SIDEWALK AREA.
9. WHERE DRIVEWAY IS OFF OF ROADWAY, SIDEWALK IS NEEDED, IF DRIVEWAY IS OFF OF ALLEY, NO SIDEWALK IS REQUIRED.
10. SEE **NARROW LOT STREET PLAN AND SECTION DETAILS T10-24A AND T10-24B** AND **NARROW LOT ALLEY PLAN AND SECTION DETAILS T10-25A AND T10-25B** FOR ADDITIONAL INFORMATION.
11. OPTIONAL DRIVEWAY WINGS ARE AVAILABLE THROUGH THE "ADMINISTRATIVE ROAD MODIFICATION" PROCESS.
12. IF A FIRE HYDRANT OR WATER METER BOX IS SITE NEARBY, THERE SHALL BE A MIN. OF 5' BETWEEN THE EDGE OF CONCRETE (WING) AND THE FIRE HYDRANT ASSEMBLY OR METER BOX.
13. FOR RESIDENTIAL SUBDIVISIONS ONLY, THE CONTRACTOR MUST INSTALL ALL CURBS THROUGHOUT THE PROJECT AND DEFER THE CONSTRUCTION OF DRIVEWAY DROPS. DRIVEWAY DROPS WOULD THEN BE HORIZONTALLY SAWCUT INTO THE CURB FACE AT TIME OF HOUSE CONSTRUCTION.



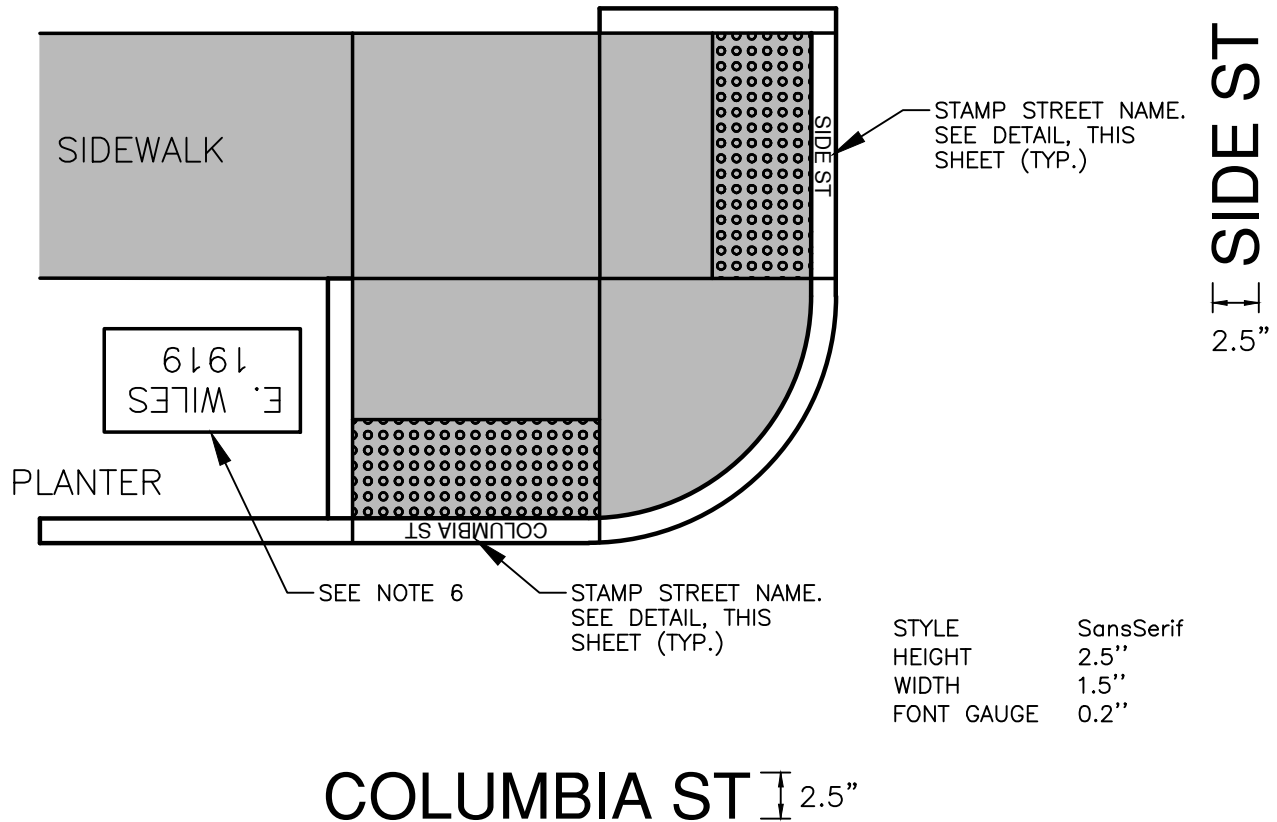
**SHARED DRIVEWAY WITH DETACHED SIDEWALK**

CITY OF VANCOUVER  
DEPARTMENT OF PUBLIC WORKS  
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7	MAHE	3/24

STD. PLAN NO.

**T01-08**



#### NOTES:

1. STREET NAME SHALL BE STAMPED ON PROPOSED CURB AND BE CENTERED WITHIN THE 5' WIDE PEDESTRIAN RAMP
2. TEXT STYLE SHALL BE SansSerif OR APPROVED EQUAL.
3. CONTRACTOR SHALL SUBMIT STAMP SAMPLE FOR CITY APPROVAL PRIOR TO STAMPING.
4. FAILURE TO STAMP STREET NAMES WILL REQUIRE CURB REPLACEMENT AT THE CONTRACTORS EXPENSE.
5. STAMP SHALL BE PLACED WITHIN 30 MINUTES OF CONCRETE POUR AND SHALL BE CLEARLY VISIBLE AFTER CURING.
6. CONTRACTOR SHALL PLACE EXISTING CONTRACTOR AND DATE STAMPED CONCRETE IN PLANTER STRIP AT GRADE. ONE PER NAME PER INTERSECTION



#### CURB STAMP DETAIL

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CDC	MAHE	7/14
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3	MAHE	3/24

STD. PLAN NO.

**T01-09**