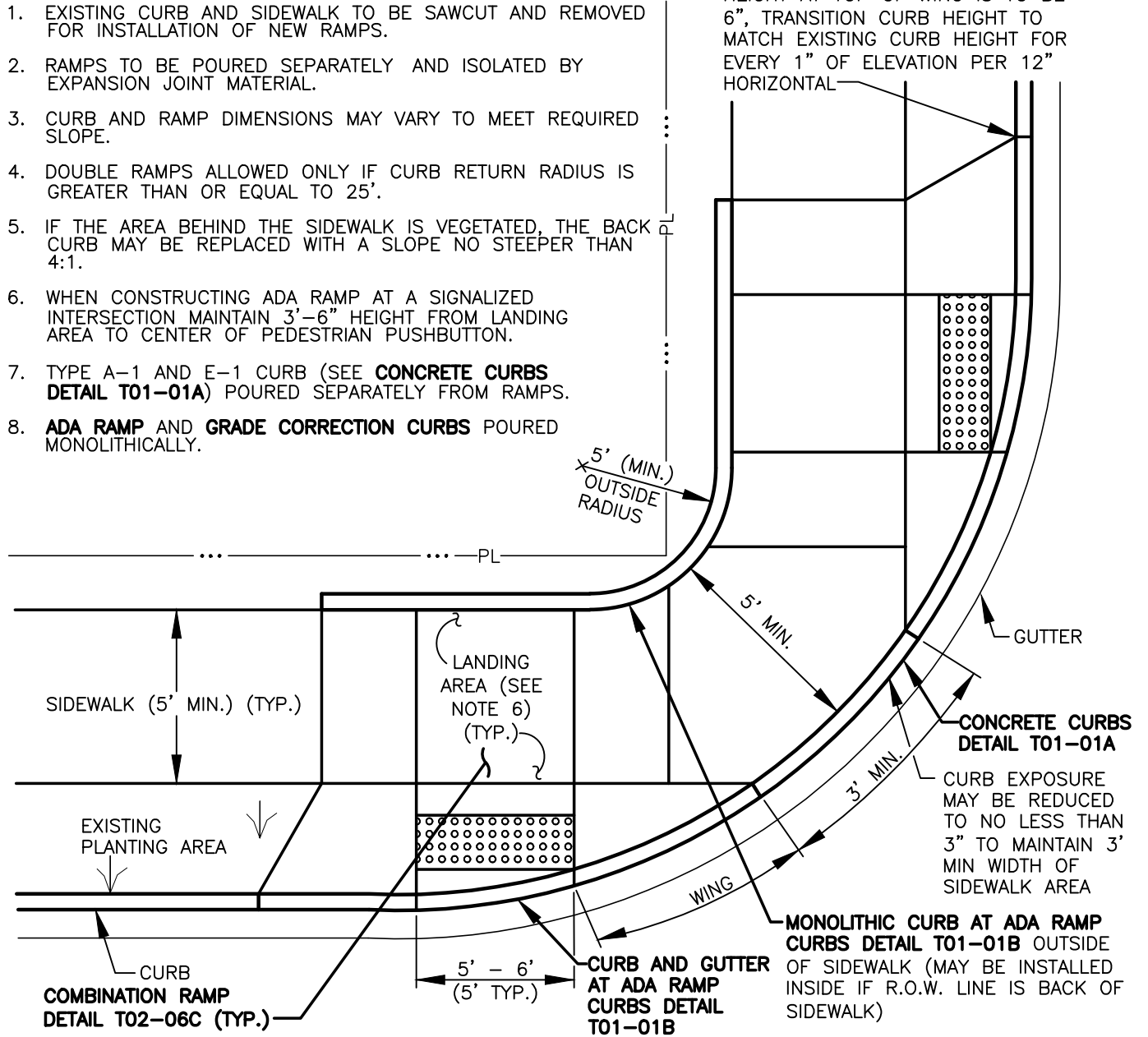


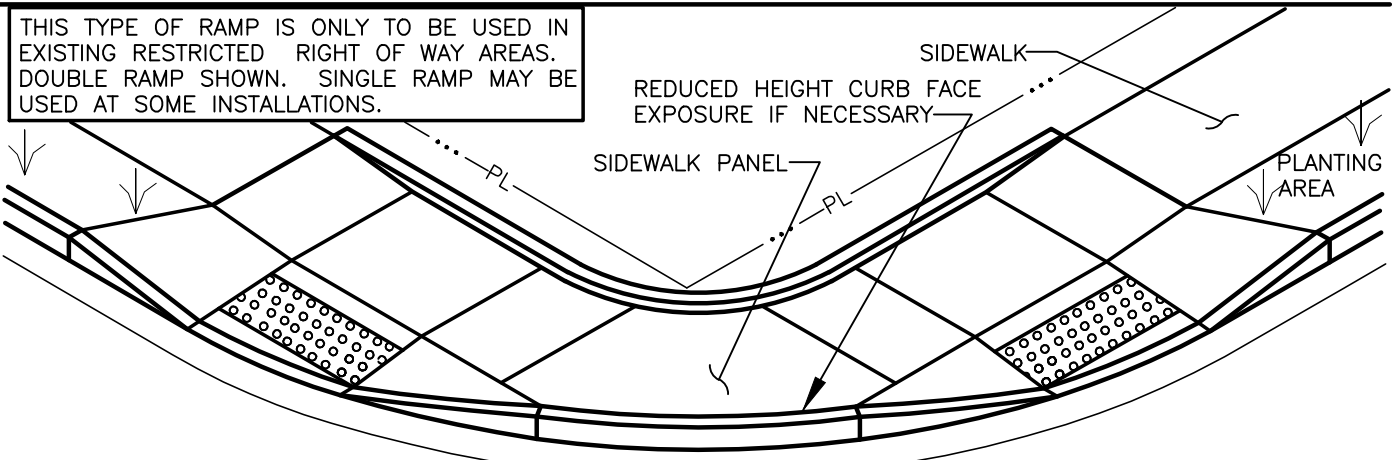
NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMPS.
2. RAMPS TO BE POURED SEPARATELY AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. CURB AND RAMP DIMENSIONS MAY VARY TO MEET REQUIRED SLOPE.
4. DOUBLE RAMPS ALLOWED ONLY IF CURB RETURN RADIUS IS GREATER THAN OR EQUAL TO 25'.
5. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
6. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'-6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
7. TYPE A-1 AND E-1 CURB (SEE **CONCRETE CURBS DETAIL T01-01A**) POURED SEPARATELY FROM RAMPS.
8. **ADA RAMP AND GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

ON RETROFIT CONSTRUCTION CURB HEIGHT AT TOP OF WING IS TO BE 6", TRANSITION CURB HEIGHT TO MATCH EXISTING CURB HEIGHT FOR EVERY 1" OF ELEVATION PER 12" HORIZONTAL



THIS TYPE OF RAMP IS ONLY TO BE USED IN EXISTING RESTRICTED RIGHT OF WAY AREAS. DOUBLE RAMP SHOWN. SINGLE RAMP MAY BE USED AT SOME INSTALLATIONS.



DOUBLE COMBINATION RAMP PLACEMENT - A



CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

DRAWN BY	APPROVED BY	APPROVAL DATE
CDC	<i>[Signature]</i>	8/04
REVISION	APPROVED BY	APPROVAL DATE
7	<i>[Signature]</i>	3/24

STD. PLAN NO.
T02-06A