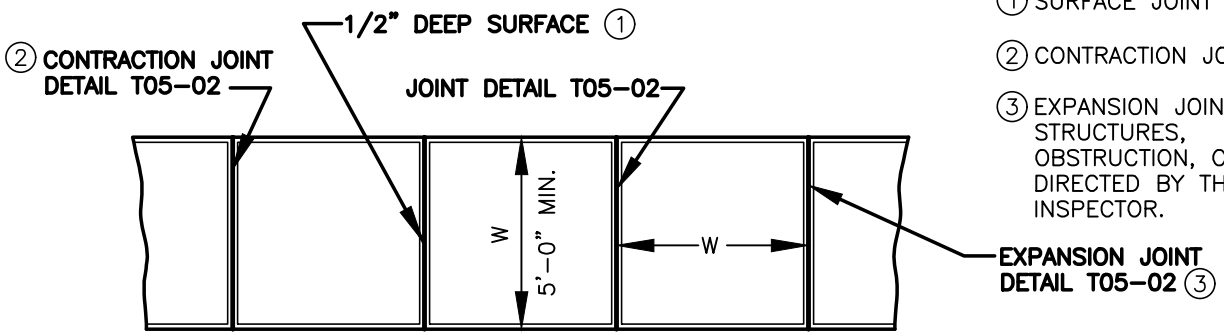
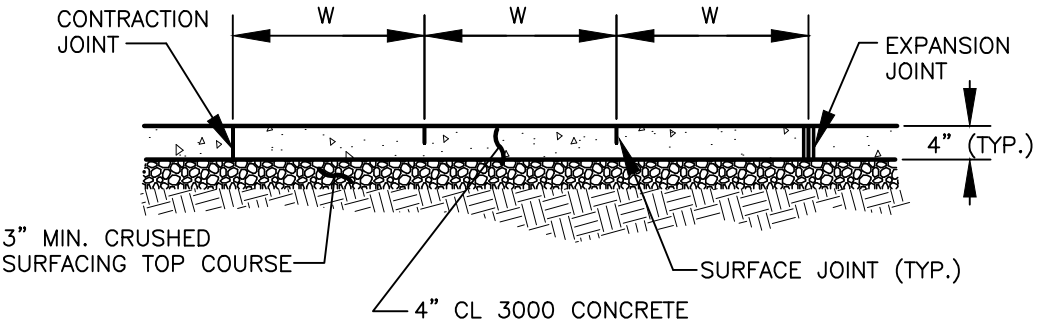
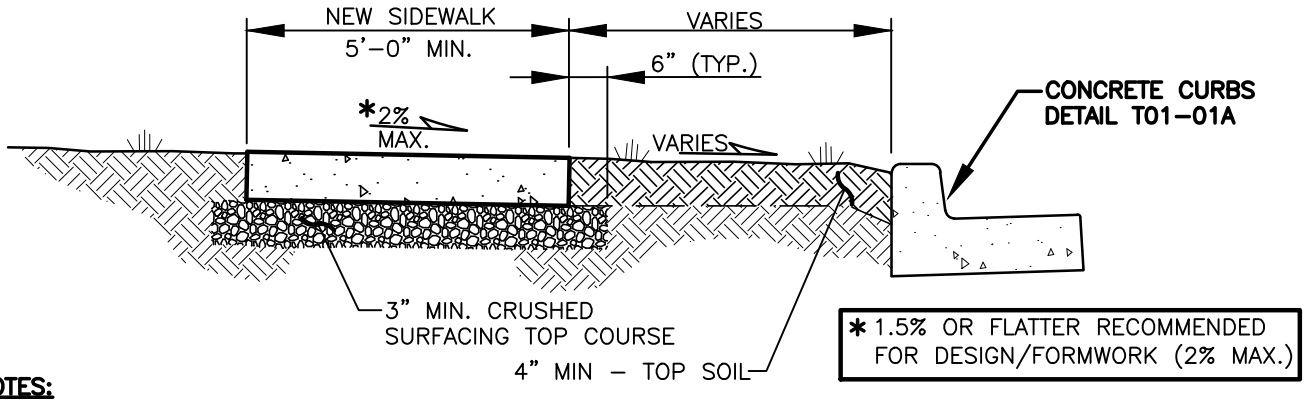
NOMINAL JOINT SPACING:

1. SURFACE JOINT AT 5'.
2. CONTRACTION JOINT AT 15'.
3. EXPANSION JOINT AT STRUCTURES, OBSTRUCTION, OR AS DIRECTED BY THE INSPECTOR.







NOTES:

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3 1/2" SLUMP (MAX.).
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. FINISH SHALL BE MEDIUM BROOM PERPENDICULAR TO PEDESTRIAN TRAFFIC UNLESS OTHERWISE DIRECTED.
4. 2" SMOOTH FINISH BORDER AROUND EACH SIDEWALK PANEL OR MATCH EXISTING BORDER.
5. SEE **CONCRETE JOINTS DETAIL T05-02** FOR SURFACE, CONTRACTION, AND EXPANSION JOINTS.
6. ALL EXISTING EDGES SHALL BE SAWCUT.
7. CROSS SLOPE OF PLANTER STRIP SHALL BE 2% (TYP.) AND 4:1 (MAX.).
8. ALL SIDEWALK REMOVAL AND REPLACEMENT SHALL BE JOINT TO JOINT.
9. SIDEWALK REPLACEMENT AREAS SHALL BE ROCKED LEVEL AND COMPACTED UNTIL CONCRETE IS PLACED.
10. FIRE HYDRANT OR WATER METER BOX SHALL NOT BE WITHIN THE SIDEWALK.
11. WHERE ROLLED CURB IS USED WITH ATTACHED SIDEWALKS, SIDEWALK SHALL BE A MIN. OF 6" IN DEPTH.

CITY OF Vancouver WASHINGTON

SIDEWALK DETAIL

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-01A**

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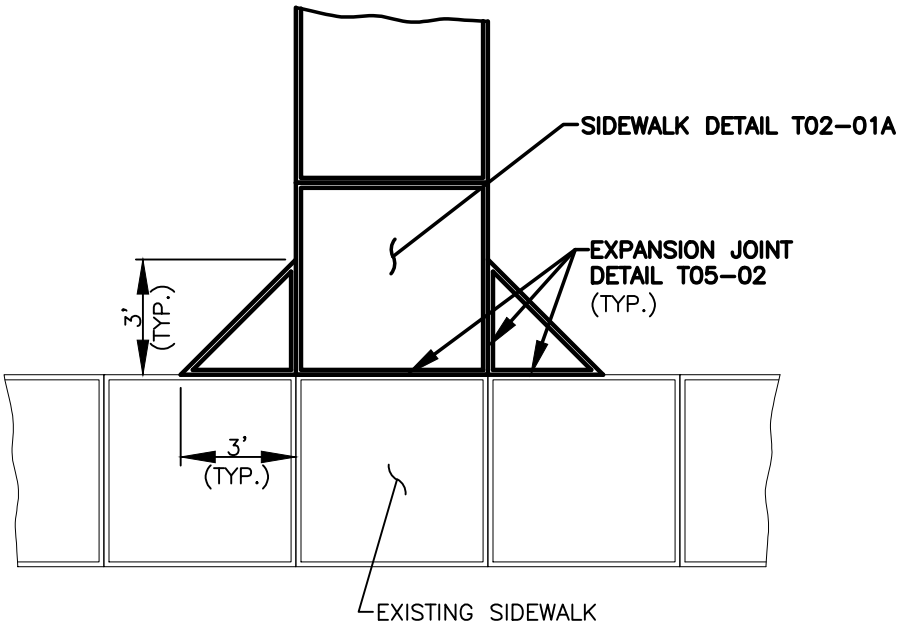
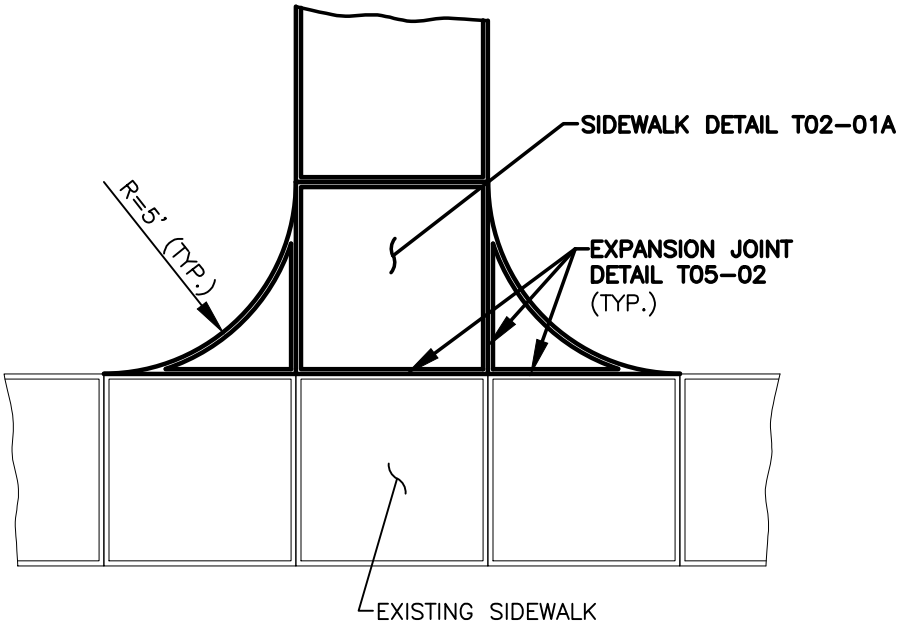


OPTION A



OPTION B

NOTES:

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3 1/2" SLUMP (MAX.).
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. FINISH SHALL BE MEDIUM BROOM PERPENDICULAR TO PEDESTRIAN TRAFFIC UNLESS OTHERWISE DIRECTED.
4. 2" SMOOTH FINISH BORDER AROUND EACH SIDEWALK PANEL OR MATCH EXISTING BORDER.
5. SEE **CONCRETE JOINTS DETAIL T05–02** FOR SURFACE, CONTRACTION, AND EXPANSION JOINTS.

CITY OF Vancouver WASHINGTON

INTERSECTINGSIDEWALK DETAIL

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-01B**

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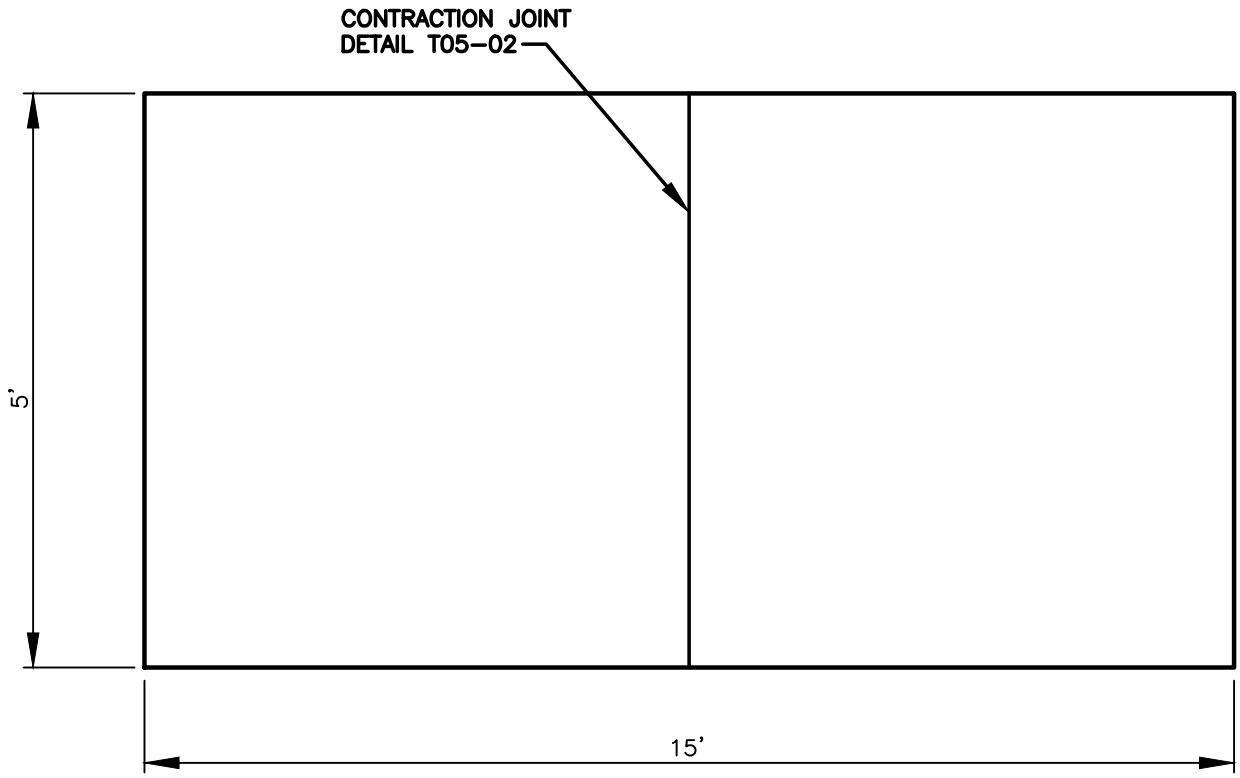

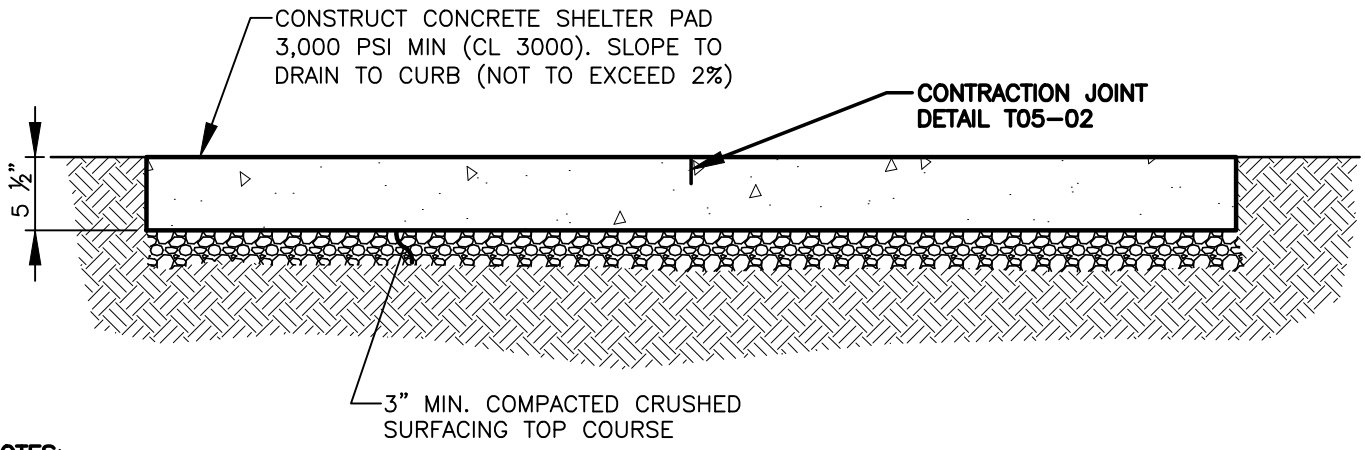

NOTES:

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3 1/2" SLUMP (MAX.).
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. FINISH SHALL BE MEDIUM BROOM PERPENDICULAR TO PEDESTRIAN TRAFFIC UNLESS OTHERWISE DIRECTED.
4. 2" SMOOTH FINISH BORDER AROUND EACH SIDEWALK PANEL OR MATCH EXISTING BORDER.
5. SEE **CONCRETE JOINTS DETAIL T05–02** FOR CONTRACTION EXPANSION JOINTS.
6. ALL EXISTING EDGES SHALL BE SAWCUT.
7. SIDEWALK CLEARANCE ZONE IS 4' MIN.
8. CHECK WITH C–TRAN FOR ADDITIONAL INFORMATION IN REGARDS TO SHELTER STYLE AND INSTALLATION.
9. BUS SHELTER CONCRETE PAD SHALL BE EITHER BEHIND SIDEWALK OR 2' FROM FACE OF CURB.

CITY OF Vancouver WASHINGTON

BUS STOP SHELTER CONCRETE PAD

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO.
T02-01C

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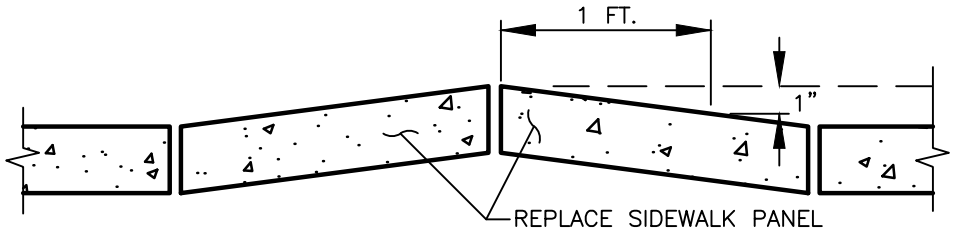
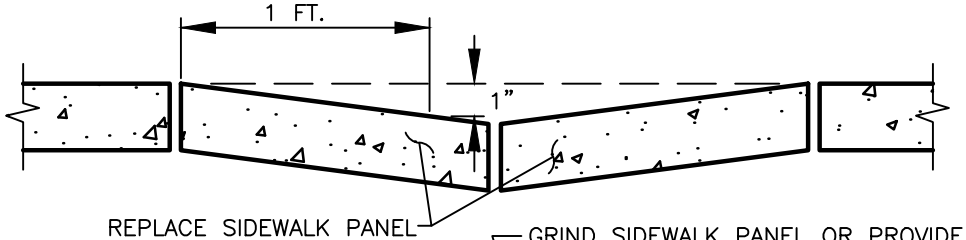
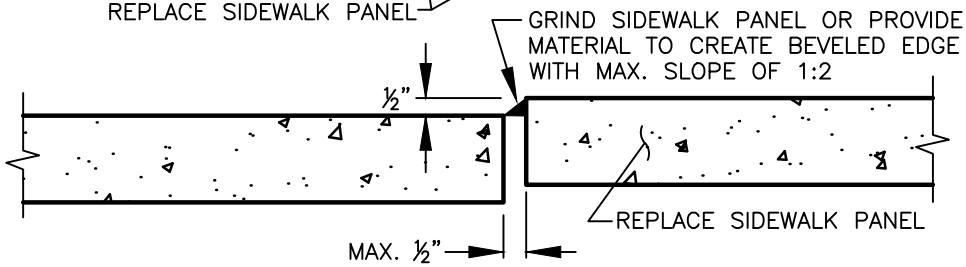
1) RAISED SIDEWALK
(REPLACE PANELS)



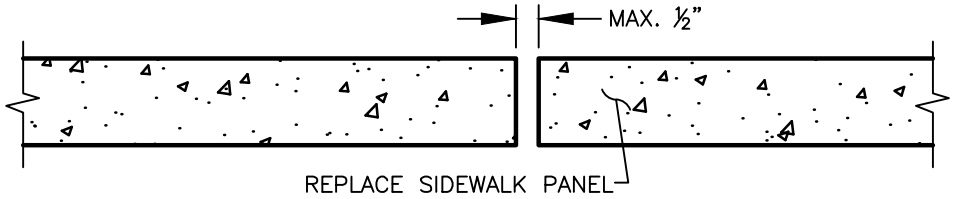
2) SUNKEN SIDEWALK
(REPLACE PANELS)



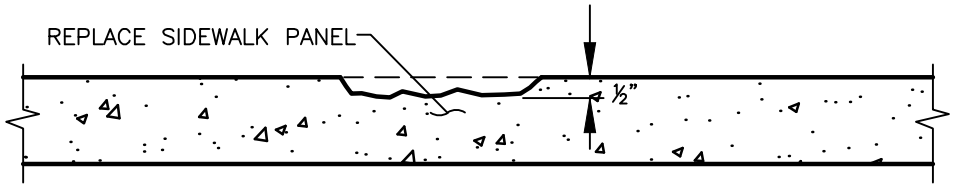
3) STEP SEPARATIONS
(REPLACE PANEL OR PROVIDE BEVELED EDGE AT 1:2 SLOPE)



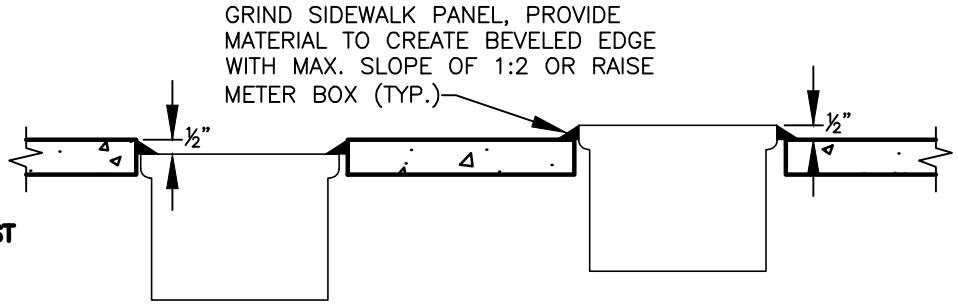
4) OPENING IN SIDEWALK
(REPLACE PANEL)



5) SPALLING OF SIDEWALK
(REPLACE DAMAGED PANEL)



6) METER BOXES
(REPLACE PANEL, PROVIDE BEVELED EDGE AT 1:2 SLOPE OR ADJUST METER BOXES TO MEET SIDEWALK EDGE)



7) BROKEN CURB

8) OTHER HAZARD

NOTE:

1. PAVERS SHALL FOLLOW THE SAME CRITERIA AS CONCRETE SIDEWALKS.
2. REPAIR OPTIONS ARE IN PARENTHESIS **(XXX)**.

CITY OF Vancouver WASHINGTON

SIDEWALK REPLACEMENT MINIMUM CRITERIA TO ENSURE ADA COMPLIANCE AND REPAIR OPTIONS

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-01D**

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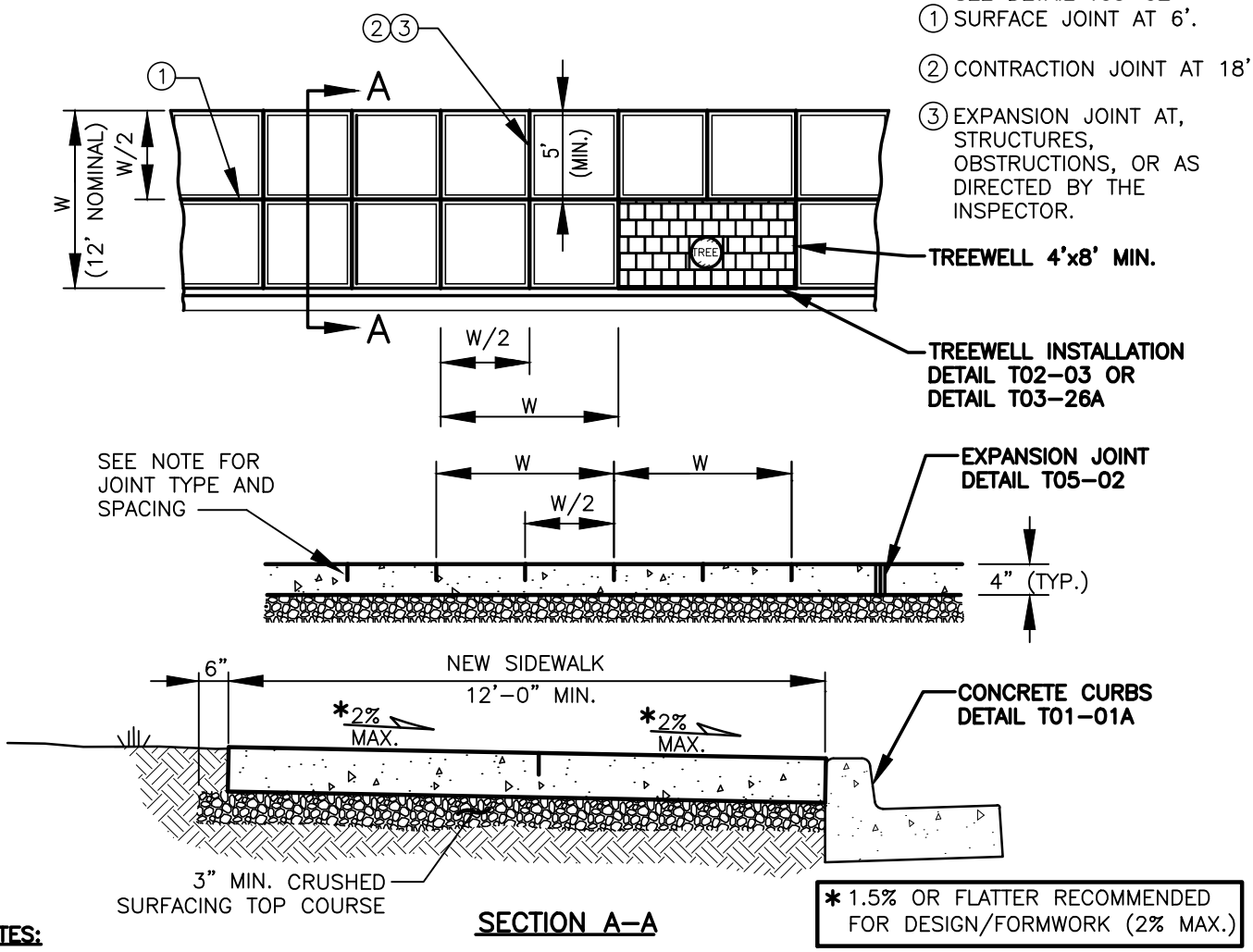

SECTION A–A

* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

NOTES:

1. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3 1/2" SLUMP (MAX.).
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. FINISH SHALL BE MEDIUM BROOM PERPENDICULAR TO PEDESTRIAN TRAFFIC UNLESS OTHERWISE DIRECTED.
4. SIDEWALK PANEL TO MATCH EXISTING BORDER AND SCORING OR 2" SMOOTH FINISH BORDER AROUND EACH SIDE.
5. SEE **CONCRETE JOINTS DETAIL T05–02**.
6. BRICK BANDING SET IN MORTAR ON TOP OF A CONCRETE FOUNDATION TO MATCH EXISTING SIDEWALK OR APPROVED BY CITY PLANNER.
7. UTILITY RISERS SHALL BE KEPT ADJACENT TO THE CURB TO CREATE A STRAIGHT PATH FREE OF OBSTRUCTIONS FOR PEDESTRIAN TRAVEL.
8. SIDEWALK PATTERN MAY BE USED OUTSIDE OF CITY CENTER AREA WITH ENGINEER APPROVAL.
9. SEE **ROOT BARRIER DETAILS T03–22A AND T03–22B** IF A ROOT BARRIER IS NEEDED TO PROTECT THE SIDEWALK.
10. FIRE HYDRANT OR WATER METER BOX SHALL NOT BE WITHIN THE SIDEWALK.
11. WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED BRICK RAMPS PER **BRICK UNIT PAVER PATTERN DETAIL T02–16** SHALL ALSO BE CONSTRUCTED WITH WHITE TRUNCATED DOMES PER **RAMP LIP, DRIVEWAY LIP AND DETECTABLE WARNING PATTERN DETAIL T02–15** SHALL BE PLACED IN ALL BRICK RAMPS.

CITY OF Vancouver WASHINGTON

HERITAGE SIDEWALK

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-02**

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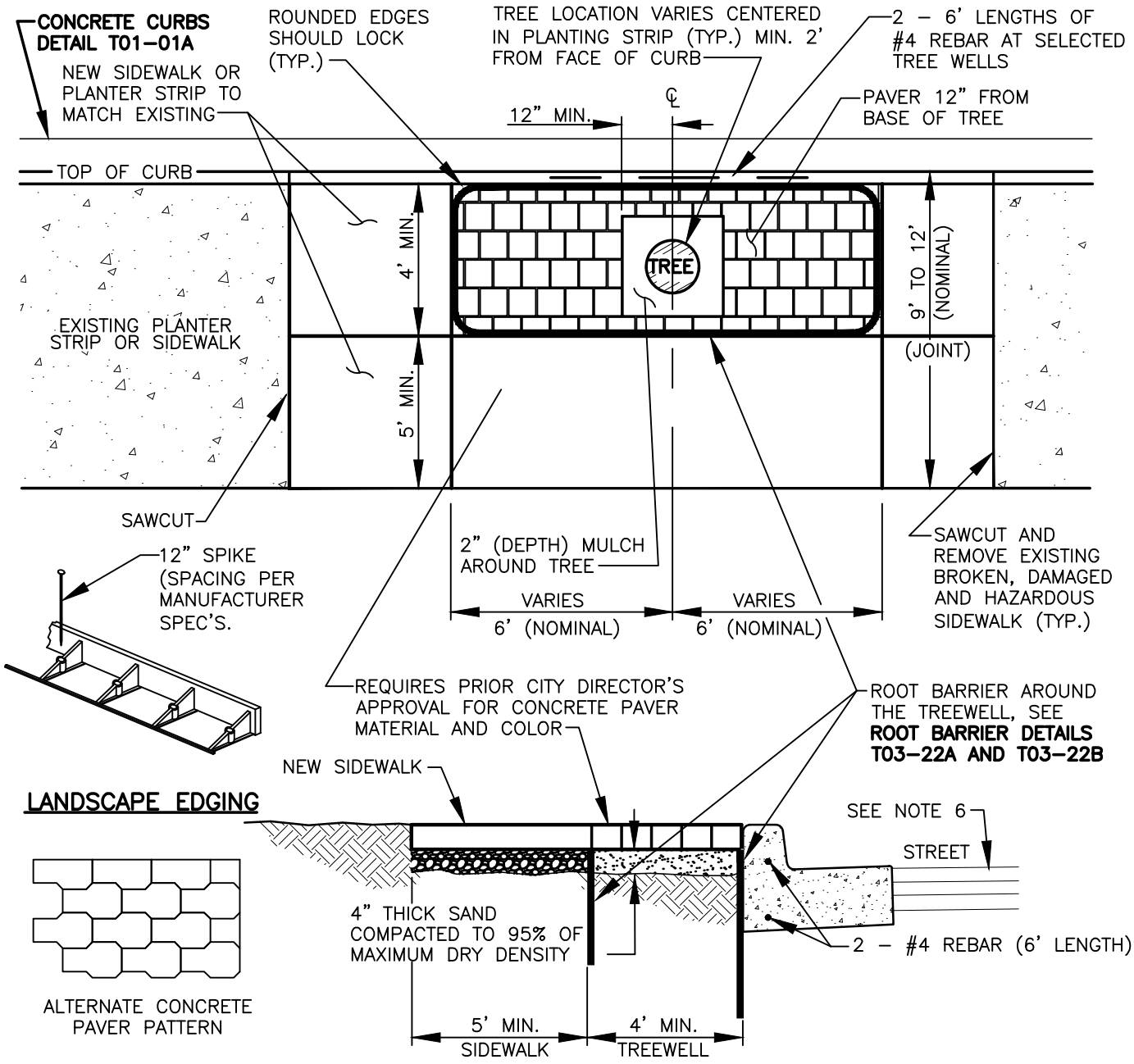

PAVER EDGE DETAIL

NOTES:

1. SAMPLE OF BRICK COLOR SHALL BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION. BRICK MANUFACTURED BY MUTUAL MATERIALS, "BURGUNDY-MICA TILE" OR ENDICOTT "MEDIUM IRONSPOT #46" OR APPROVED EQUAL.
2. CONCRETE SHALL BE 3000 PSI MIN. (CL 3000), 3 1/2" SLUMP (MAX.), SMOOTH FINISH, AND EDGES FINISHED WITH 1/4" EDGE UNLESS OTHERWISE NOTED.
3. THE CENTER OF THE PAVER PATTERN SHALL BE THE STREET TREE.
4. EACH TREE SHALL HAVE APPROXIMATELY 144 S.F. OF PAVER AREA (72 S.F. TREEWELL AND 72 S.F. WALKWAY/SIDEWALK).
5. CONCRETE BAND MAY BE DELETED IF PAVERS ABUT A BUILDING.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-02** WHEN CUTTING EXISTING CURB.
7. SEE **ROOT BARRIER DETAILS T03-22A AND T03-22B** IF A ROOT BARRIER IS REQUIRED BY ENGINEER TO PROTECT THE SIDEWALK.

CITY OF Vancouver WASHINGTON

TREE WELL AND PAVER EDGE INSTALLATION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-03**

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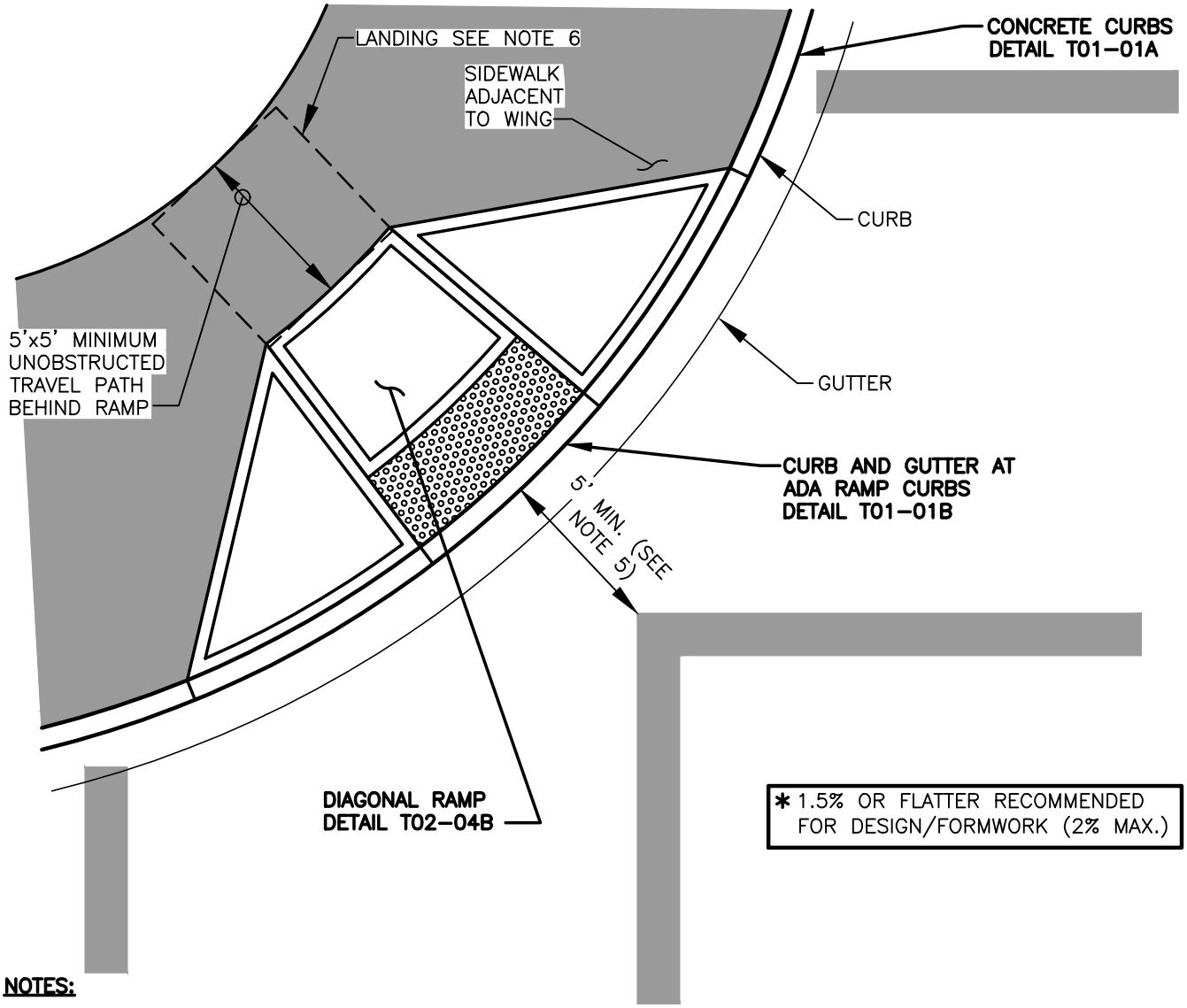

NOTES:

1. RAMPS TO BE CENTERED IN CROSSWALKS.
2. RAMPS TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. CROSSWALKS TO BE CENTERED ON SIGNAL POLE. WHERE NO SIGNAL POLE EXISTS, CROSSWALK LOCATION SHALL BE PER APPROVED SITE PLAN.
4. SURROUNDING SIDEWALK CROSS SLOPE TO BE 2% MAX. RADIALLY AROUND CORNER SECTION.
5. IF A SINGLE DIAGONAL CURB RAMP IS PERMITTED, 5' MIN. CLEAR SPACE SHALL BE PROVIDED FOR MANEUVERING ROOM IN CROSSWALK.
6. AT THE TOP EACH RAMP A 5'x5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
7. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
8. SEE **DOUBLE DIAGONAL RAMP PLACEMENT FOR TRAFFIC SIGNAL STANDARDS DETAIL T20–06A** IF ADA RAMPS ARE BEING INSTALLED AT A SIGNALIZED INTERSECTION.
9. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
10. ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED ARE REQUIRED TO BE CONSTRUCTED WITH BRICK UNIT PAVERS, SEE **BRICK UNIT PAVER PATTERN T02–16** FOR BRICK LAYOUTS.

CITY OF Vancouver WASHINGTON

SINGLE DIAGONAL RAMP REPLACEMENT

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-04A**

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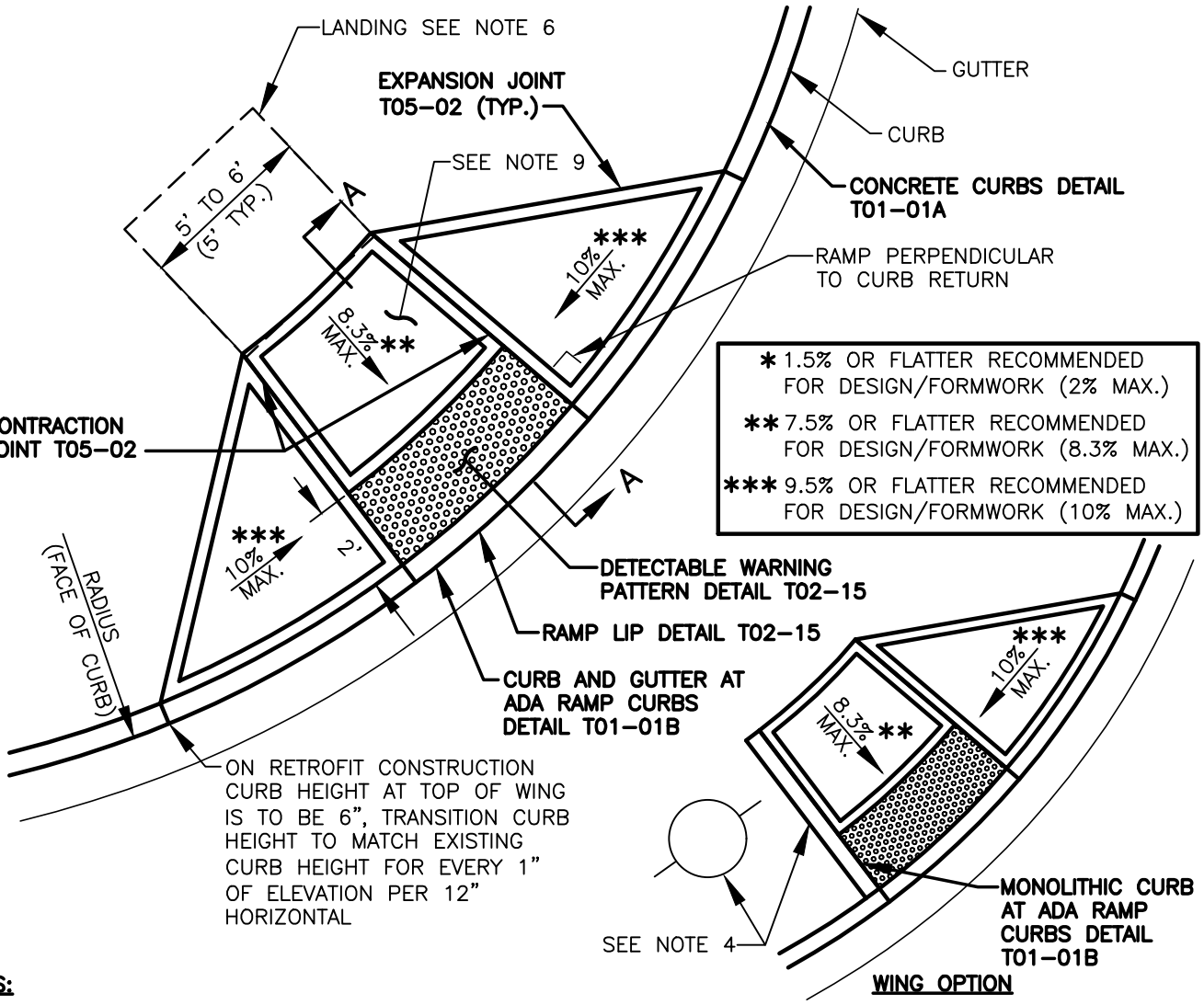

NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMP.
2. RAMP MAY BE USED MID–BLOCK OR ON INTERSECTION RADII.
3. RAMP TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
4. RAMP WINGS MAY BE REPLACED WITH A MONOLITHIC CURB **ADA CURB RAMPS DETAIL T01–01B** IF OBSTRUCTION OR PLANTER PREVENTS PEDESTRIAN TRAFFIC IN WING AREA.
5. SEE SECTION A–A ON **STANDARD LANDING CROSS SECTIONS – A–A AND B–B DETAIL T02–11**.
6. AT THE TOP OF EACH RAMP A 5'x5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2% * EACH DIRECTION.
7. WING DIMENSIONS MAY VARY TO MEET REQUIRED SLOPE.
8. IF THE MAXIMUM SLOPE OF 8.3% ** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
9. RAMP CROSS SLOPE SHALL BE 2% * MAXIMUM.
10. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
11. ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED ARE REQUIRED TO BE CONSTRUCTED WITH BRICK UNIT PAVERS, SEE **BRICK UNIT PAVER PATTERN T02–16** FOR BRICK LAYOUTS.

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DIAGONAL RAMP CONSTRUCTION

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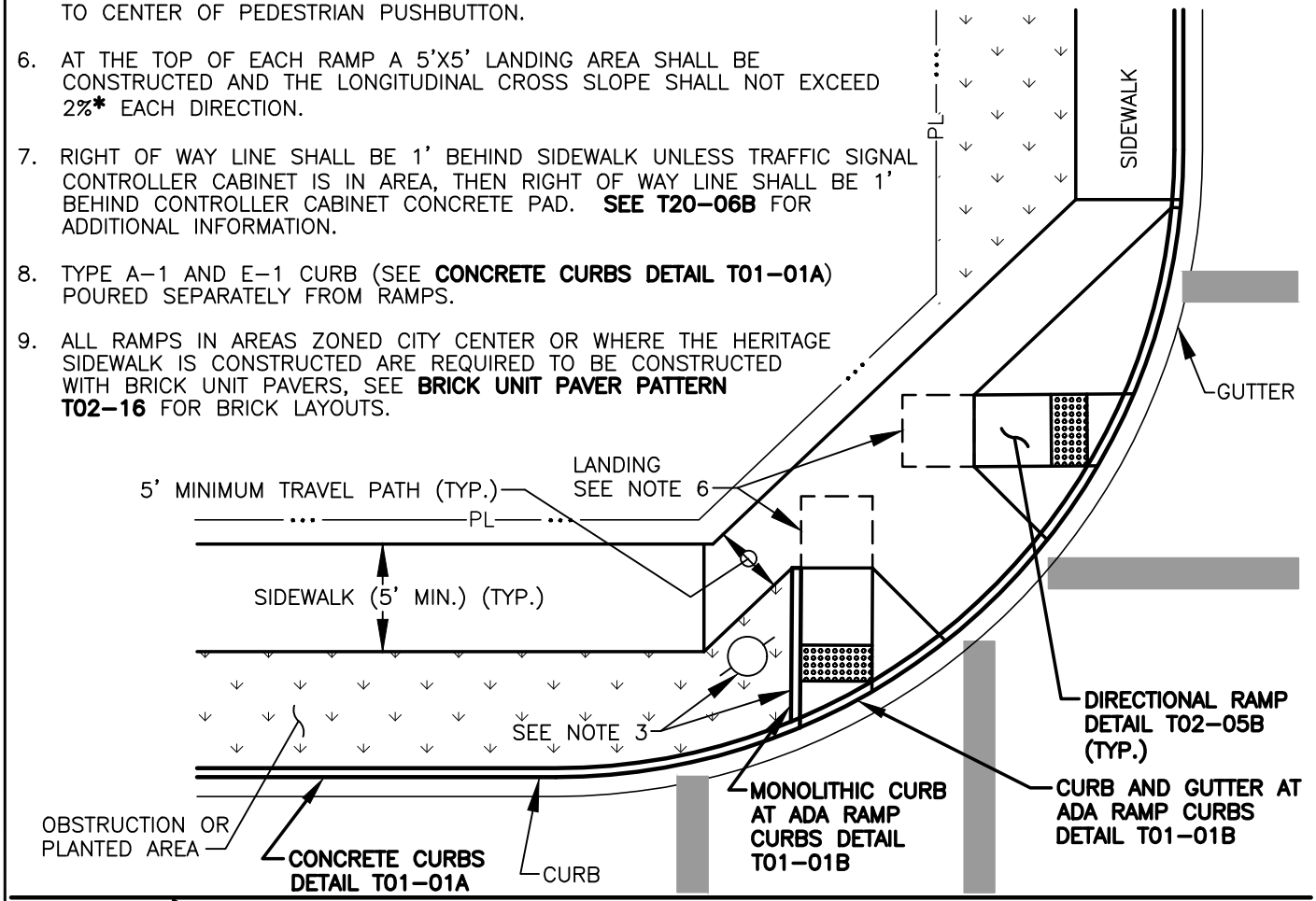
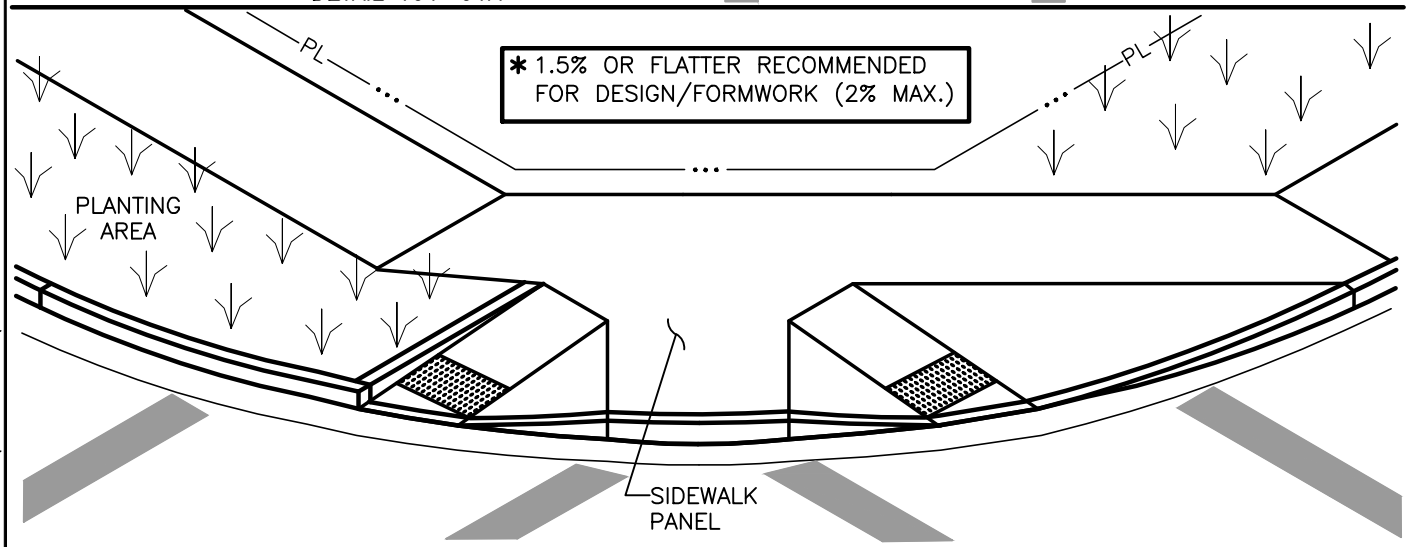
STD. PLAN NO. **T02-04B**

NOTES:

1. RAMPS TO BE CENTERED IN CROSSWALKS.
2. RAMPS TO BE CONSTRUCTED SEPARATELY AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. RAMP WING MAY BE REPLACED WITH MONOLITHIC CURB SEE **ADA RAMP CURB DETAIL T01–01B** IF OBSTRUCTION OR PLANTER PREVENTS PEDESTRIAN TRAFFIC IN WING AREA.
4. SURROUNDING SIDEWALK CROSS SLOPE TO BE 2%* MAX. RADIALLY AROUND CORNER SECTION.
5. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
6. AT THE TOP OF EACH RAMP A 5'X5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
7. RIGHT OF WAY LINE SHALL BE 1' BEHIND SIDEWALK UNLESS TRAFFIC SIGNAL CONTROLLER CABINET IS IN AREA, THEN RIGHT OF WAY LINE SHALL BE 1' BEHIND CONTROLLER CABINET CONCRETE PAD. **SEE T20–06B** FOR ADDITIONAL INFORMATION.
8. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
9. ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED ARE REQUIRED TO BE CONSTRUCTED WITH BRICK UNIT PAVERS, SEE **BRICK UNIT PAVER PATTERN T02–16** FOR BRICK LAYOUTS.





CITY OF Vancouver WASHINGTON

DOUBLE DIRECTIONAL RAMP PLACEMENT

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-05A**

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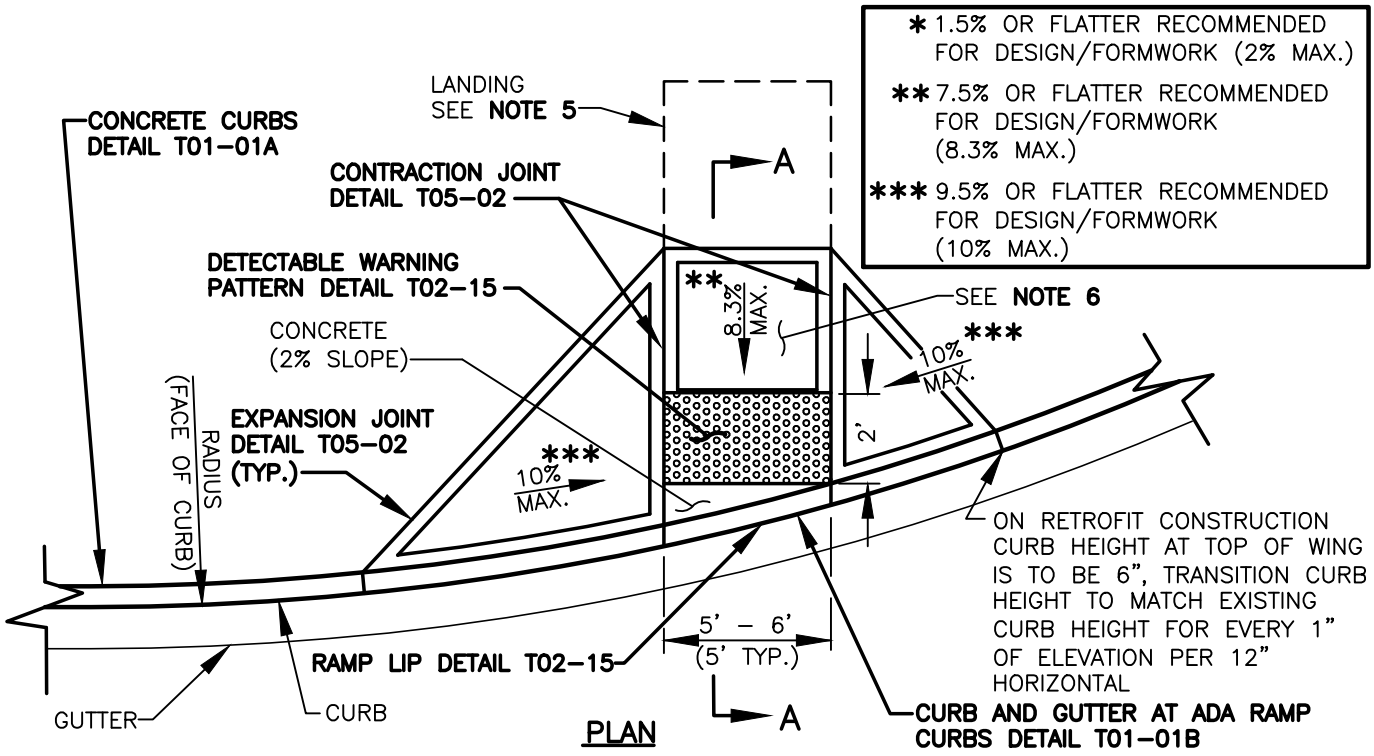

NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMP.
2. RAMP TO BE CENTERED IN CROSSWALK.
3. RAMP TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
4. RAMP WING MAY BE REPLACED WITH MONOLITHIC CURB SEE **ADA RAMP CURB DETAIL T01-01B** IF OBSTRUCTION OR PLANTER PREVENTS PEDESTRIAN TRAFFIC IN WING AREA.
5. AT THE TOP OF EACH RAMP A 5'X5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
6. IF THE MAXIMUM SLOPE OF 8.3%** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
7. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05-01A** WHEN CUTTING EXISTING CURB.
8. RAMP CROSS SLOPE SHALL BE 2%* MAXIMUM.
9. TYPE A-1 AND E-1 CURB (SEE **CONCRETE CURBS DETAIL T01-01A**) POURED SEPARATELY FROM RAMPS.
10. ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED ARE REQUIRED TO BE CONSTRUCTED WITH BRICK UNIT PAVERS, SEE **BRICK UNIT PAVER PATTERN T02-16** FOR BRICK LAYOUTS.

CITY OF Vancouver WASHINGTON

DIRECTIONAL RAMP CONSTRUCTION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-05B**

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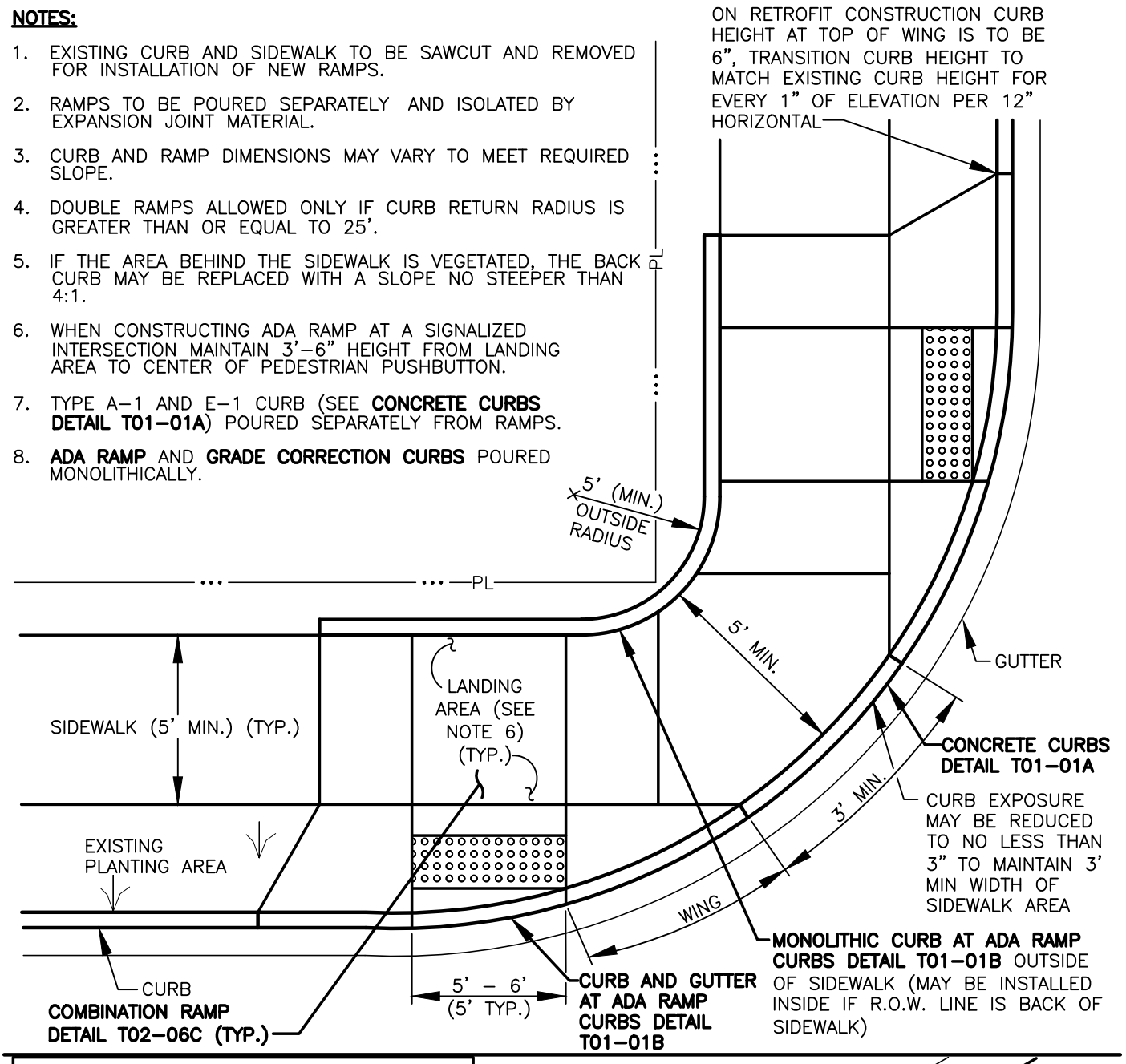
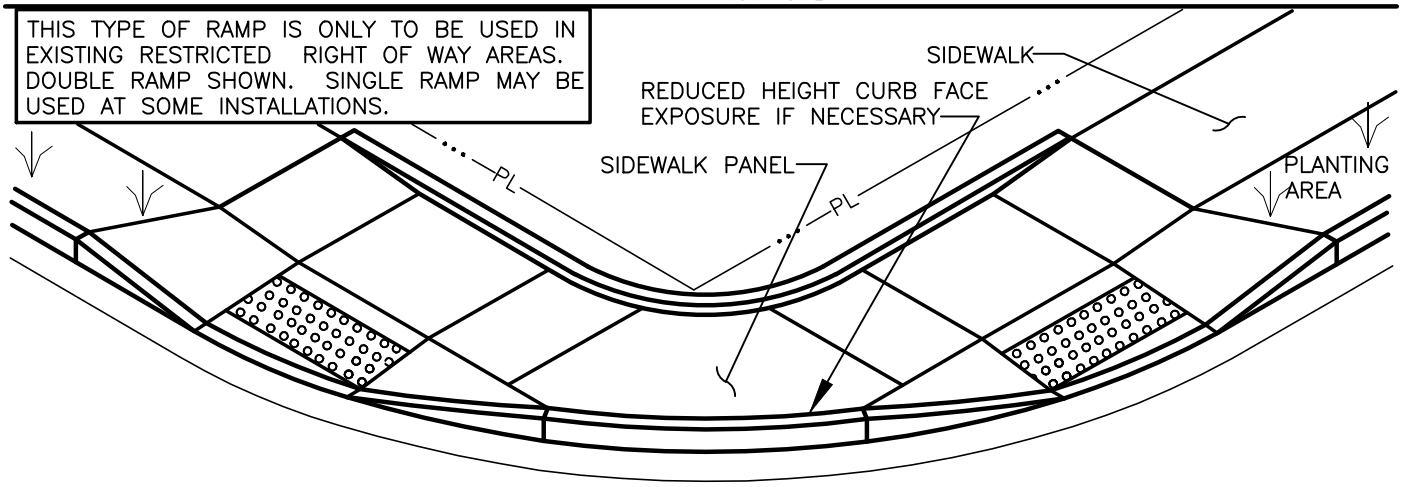
NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMPS.
2. RAMPS TO BE POURED SEPARATELY AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. CURB AND RAMP DIMENSIONS MAY VARY TO MEET REQUIRED SLOPE.
4. DOUBLE RAMPS ALLOWED ONLY IF CURB RETURN RADIUS IS GREATER THAN OR EQUAL TO 25'.
5. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
6. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
7. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
8. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.





CITY OF Vancouver WASHINGTON

DOUBLE COMBINATION RAMP PLACEMENT - A

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

DRAWN BY	APPROVED BY	APPROVAL DATE
CDC	MAHE	8/04
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STD. PLAN NO. **T02-06A**

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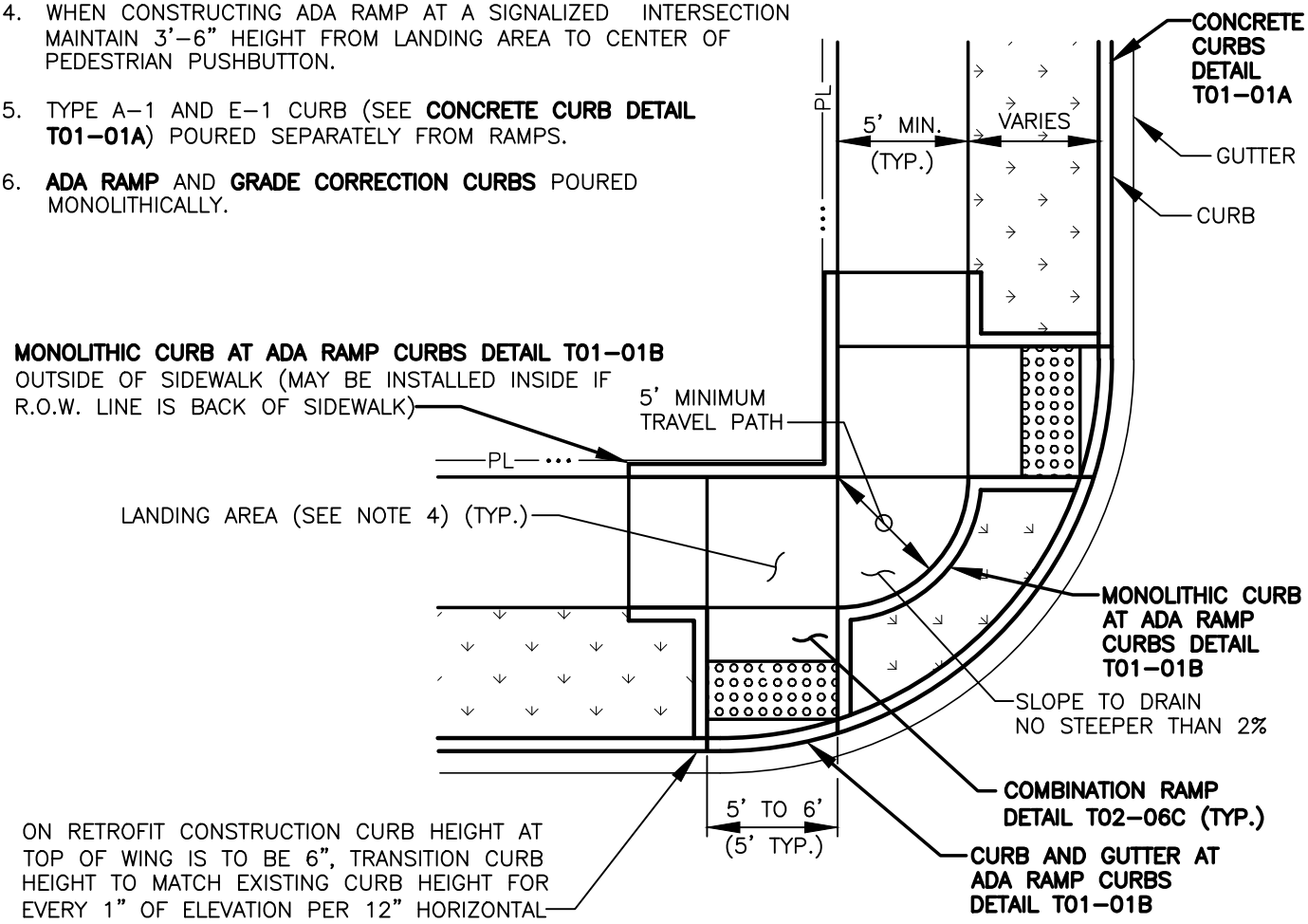
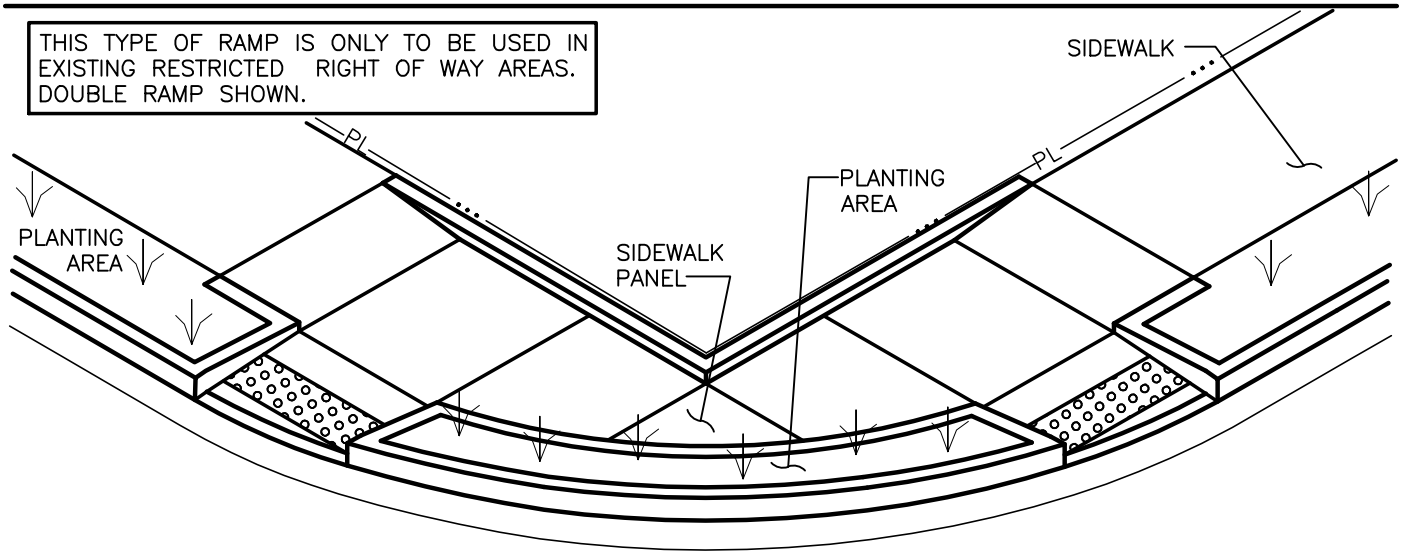
NOTES:

1. RAMPS TO BE CENTERED IN CROSSWALKS.
2. RAMPS TO BE CONSTRUCTED SEPARATELY AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
4. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
5. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURB DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
6. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.



THIS TYPE OF RAMP IS ONLY TO BE USED IN EXISTING RESTRICTED RIGHT OF WAY AREAS. DOUBLE RAMP SHOWN.



CITY OF Vancouver WASHINGTON

DOUBLE COMBINATION RAMP PLACEMENT - B

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-06B**

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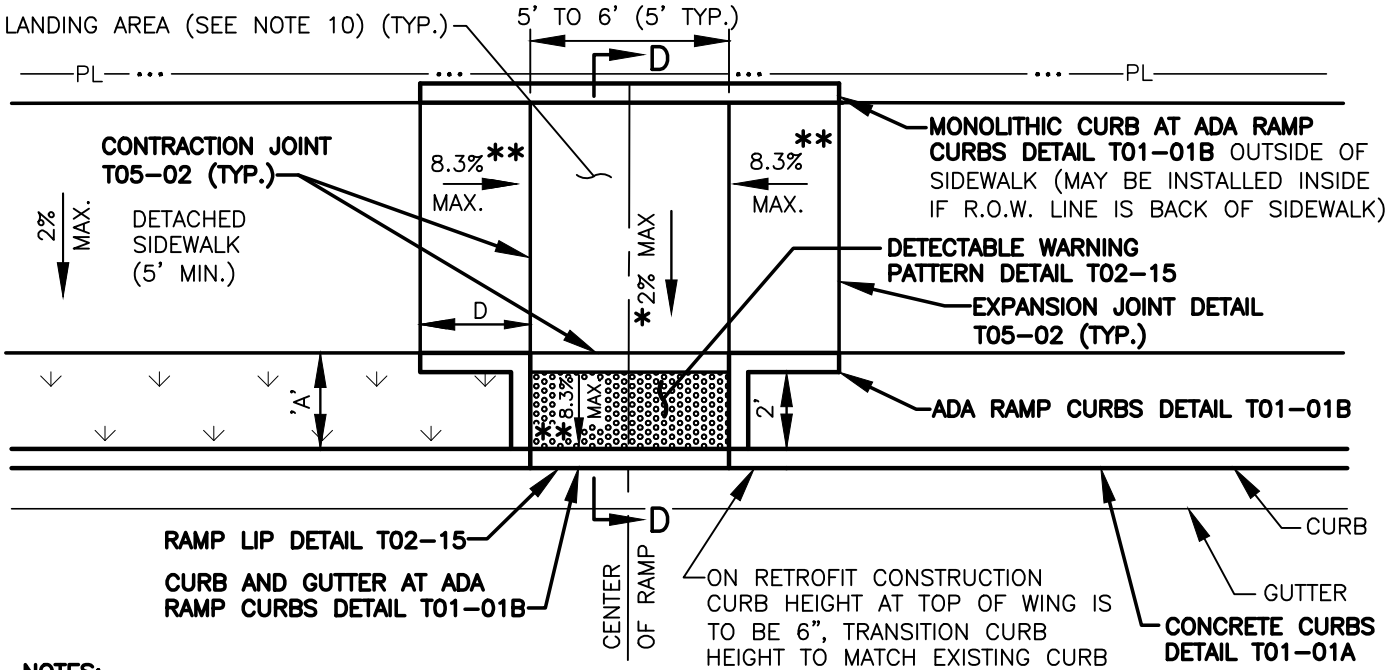


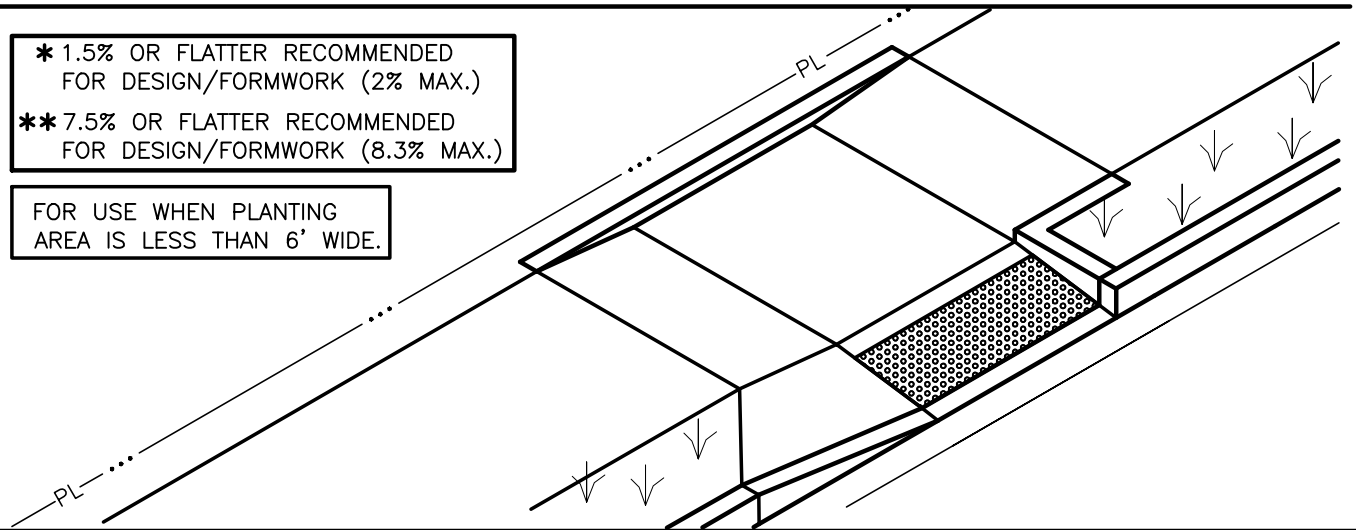
NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMP.
2. RAMP MAY BE USED MID BLOCK OR ON INTERSECTION RADIUS.
3. RAMP TO BE CENTERED IN CROSSWALK.
4. RAMPS TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
5. WING DIMENSIONS MAY VARY TO MEET REQUIRED SLOPE.
6. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED BY A SLOPE NO STEEPER THAN 4:1.
7. DIMENSION 'A' VARIES DEPENDING UPON THE WIDTH OF THE PLANTER STRIP D=6.00–C, 2' MIN.
8. SEE SECTION D–D ON **STANDARD LANDING CROSS SECTIONS – C–C AND D–D DETAIL T02–13.**
9. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
10. ON RETROFIT CONSTRUCTION PROJECTS TRANSITION FROM RAMP 2%* CROSS SLOPE TO MATCH SLOPE OF EXISTING SIDEWALK OVER A 5' AREA.
11. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
12. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)

FOR USE WHEN PLANTING AREA IS LESS THAN 6' WIDE.



CITY OF Vancouver WASHINGTON

COMBINATION RAMP CONSTRUCTION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-06C**

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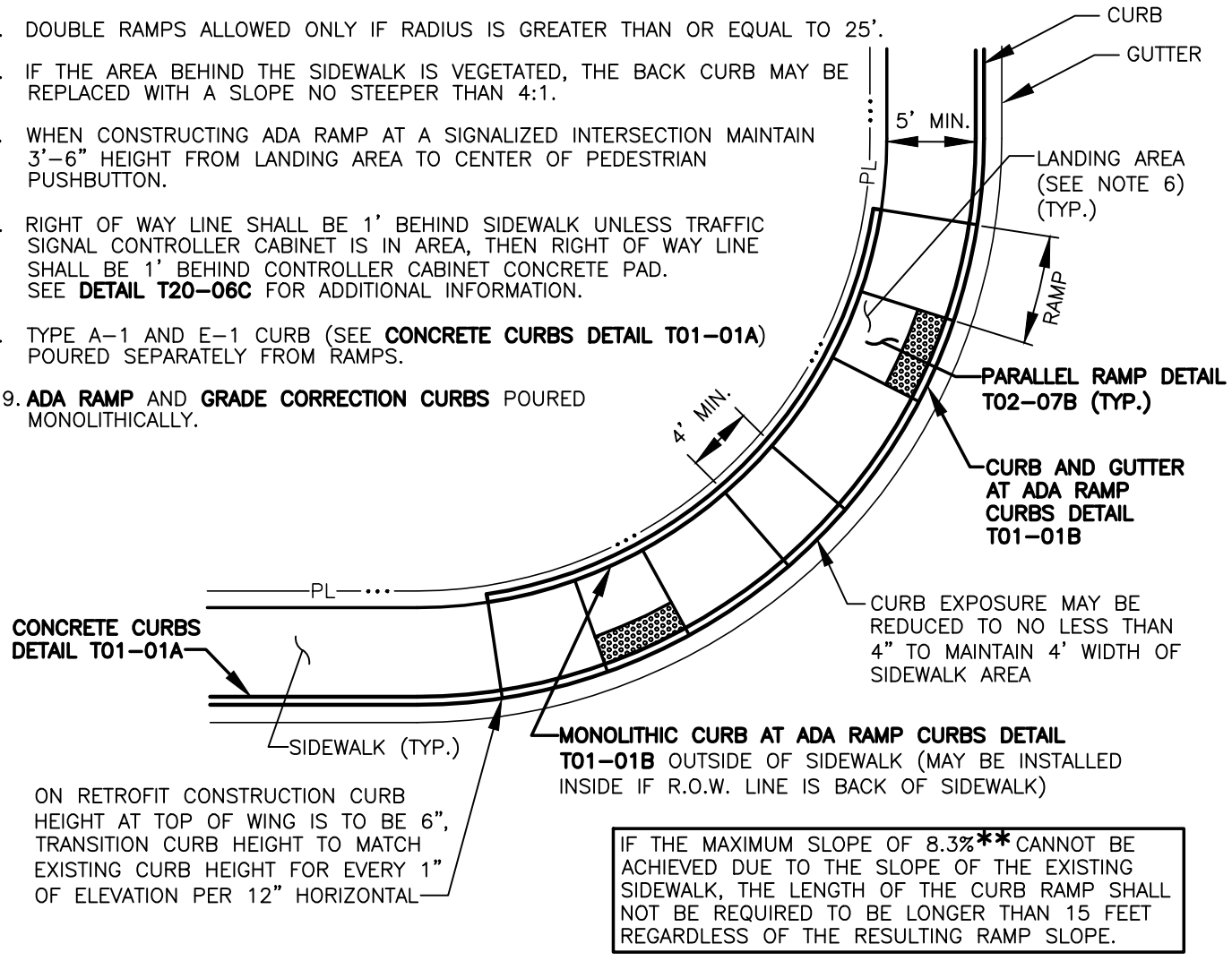
NOTES:

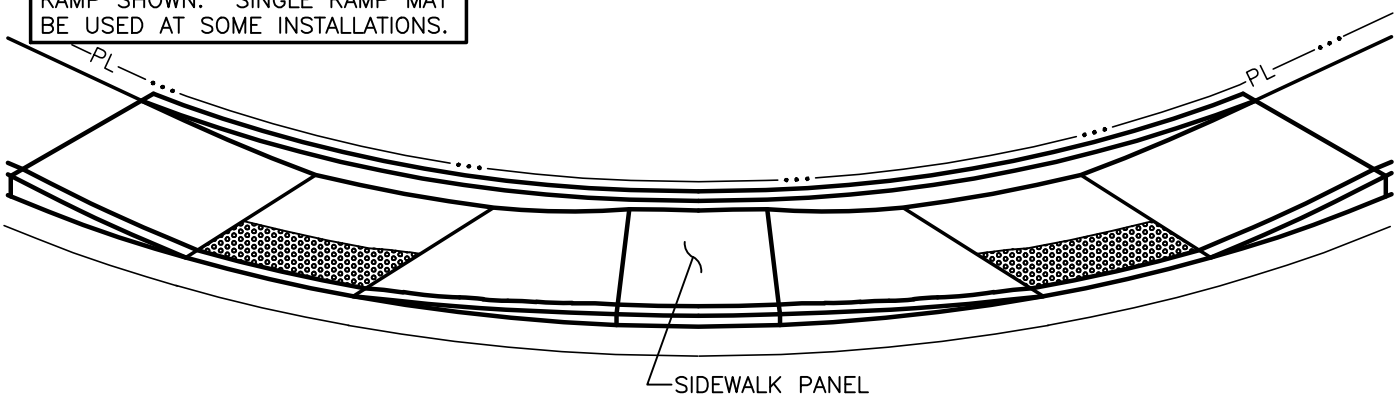
1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMPS.
2. RAMPS TO BE POURED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
3. WING DIMENSIONS MAY VARY TO MEET REQUIRED SLOPE.
4. DOUBLE RAMPS ALLOWED ONLY IF RADIUS IS GREATER THAN OR EQUAL TO 25'.
5. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
6. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'–6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
7. RIGHT OF WAY LINE SHALL BE 1' BEHIND SIDEWALK UNLESS TRAFFIC SIGNAL CONTROLLER CABINET IS IN AREA, THEN RIGHT OF WAY LINE SHALL BE 1' BEHIND CONTROLLER CABINET CONCRETE PAD. SEE **DETAIL T20–06C** FOR ADDITIONAL INFORMATION.
8. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
9. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.



IF THE MAXIMUM SLOPE OF 8.3%** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.

THIS TYPE OF RAMP IS ONLY TO BE USED IN EXISTING RESTRICTED RIGHT OF WAY AREAS, DOUBLE RAMP SHOWN. SINGLE RAMP MAY BE USED AT SOME INSTALLATIONS.

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)



CITY OF Vancouver WASHINGTON

DOUBLE PARALLEL RAMP PLACEMENT

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-07A**

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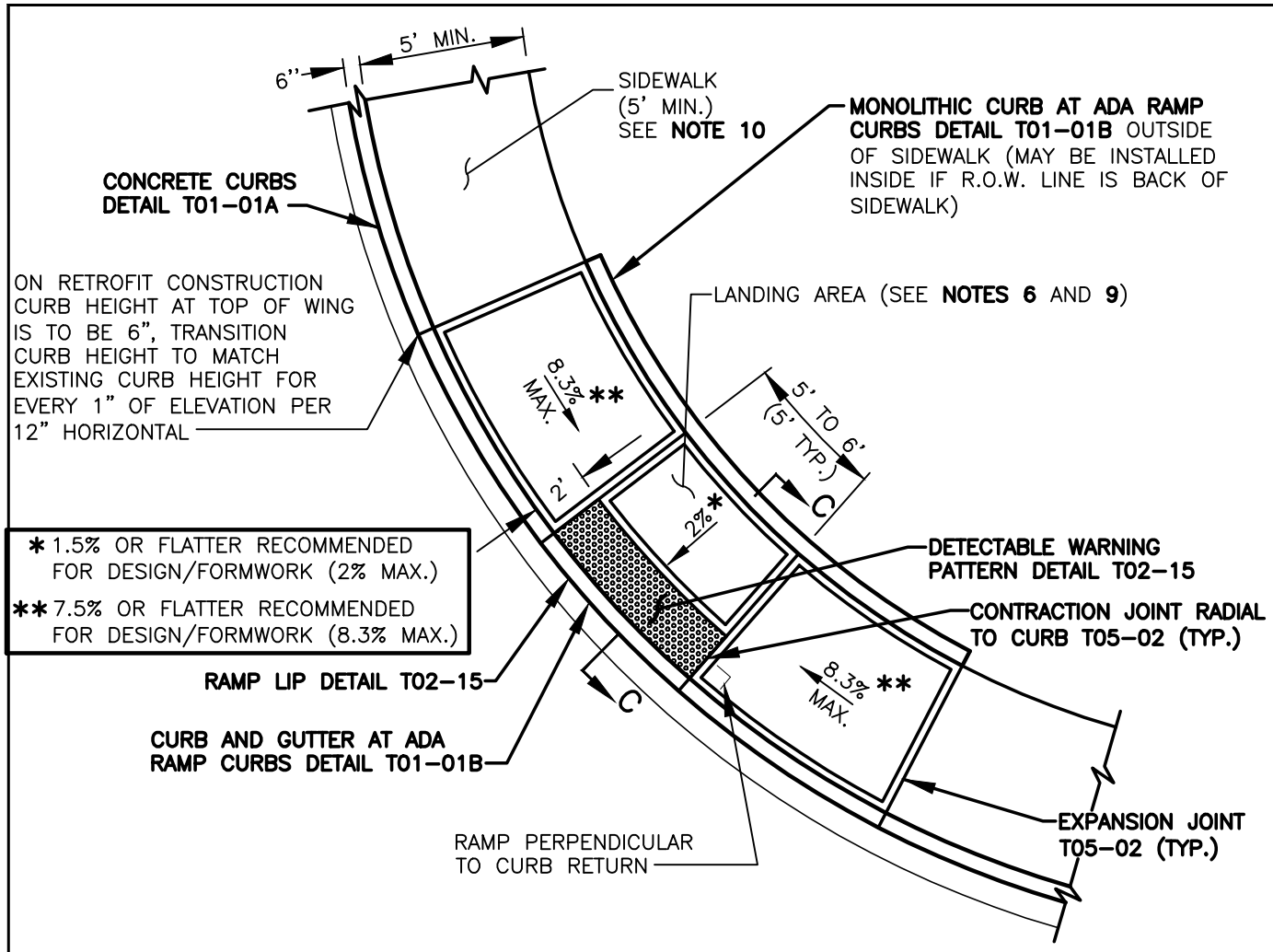

NOTES:

1. RAMPS SHALL HAVE A MAXIMUM 1:12 SLOPE.
2. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMP.
3. RAMP MAY BE USED MID-BLOCK OR ON INTERSECTION RADIUS.
4. RAMP TO BE CENTERED IN CROSSWALK.
5. RAMPS TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK AND ISOLATED BY EXPANSION JOINT MATERIAL.
6. SEE SECTION C-C ON **STANDARD LANDING CROSS SECTIONS – C-C AND D-D DETAIL T02-13**.
7. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
8. IF THE MAXIMUM SLOPE OF 8.3%** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
9. WHEN CONSTRUCTING ADA RAMP AT A SIGNALIZED INTERSECTION MAINTAIN 3'-6" HEIGHT FROM LANDING AREA TO CENTER OF PEDESTRIAN PUSHBUTTON.
10. ON RETROFIT CONSTRUCTION PROJECTS TRANSITION FROM RAMP 2%* CROSS SLOPE TO MATCH SLOPE OF EXISTING SIDEWALK OVER A 5' AREA.
11. TYPE A-1 AND E-1 CURB (SEE **CONCRETE CURBS DETAIL T01-01A**) POURED SEPARATELY FROM RAMPS.
12. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

CITY OF Vancouver WASHINGTON

PARALLEL RAMP CONSTRUCTION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-07B**

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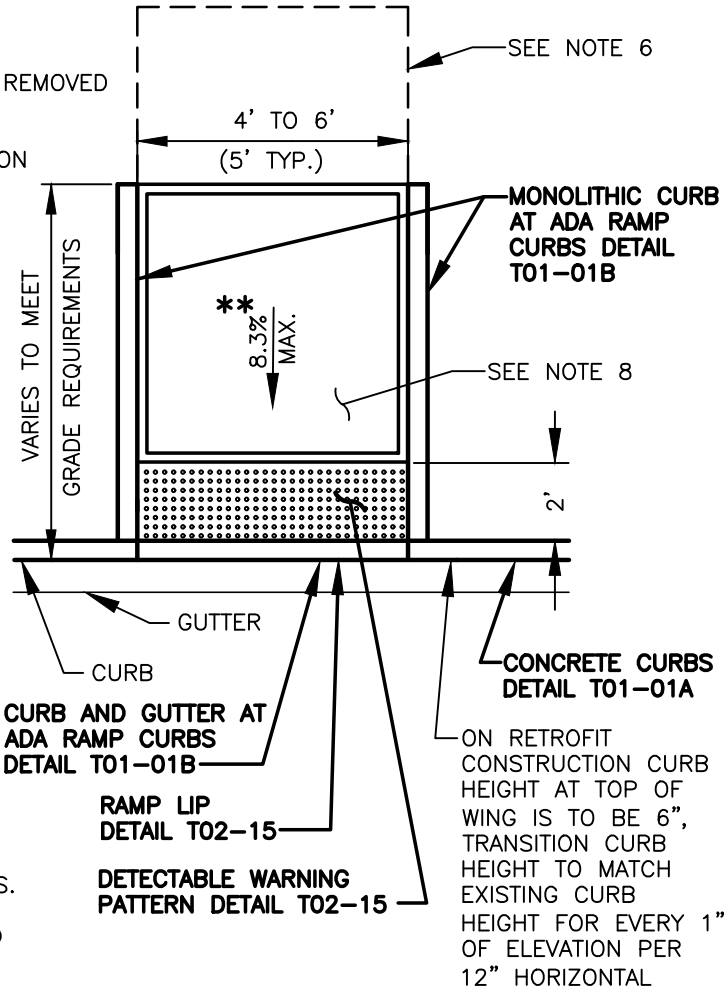
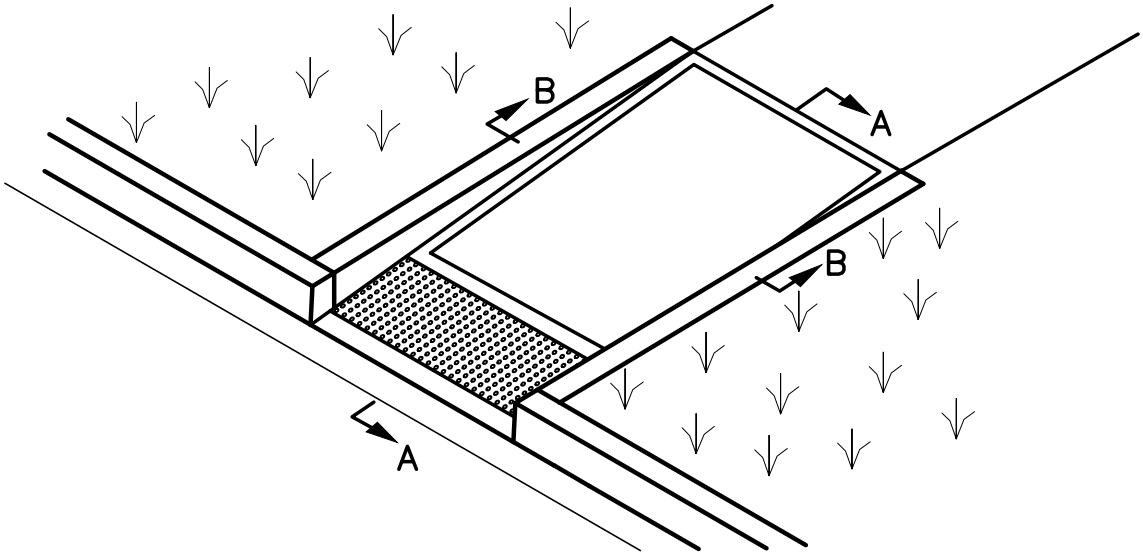
NOTES:

1. EXISTING CURB AND SIDEWALK TO BE SAWCUT AND REMOVED FOR INSTALLATION OF NEW RAMP.
2. RAMP MAY BE USED MID BLOCK OR ON INTERSECTION RADIUS.
3. RAMP TO BE CENTERED IN CROSSWALK.
4. RAMPS TO BE CONSTRUCTED SEPARATELY FROM SIDEWALK, AND ISOLATED BY EXPANSION JOINT MATERIAL.
5. FOR SECTIONS A–A AND B–B, SEE **STANDARD LANDING CROSS SECTIONS – A–A AND B–B DETAIL T02–11**.
6. AT THE TOP OF EACH RAMP A 5'X5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2% *EACH DIRECTION.
7. IF THE MAXIMUM SLOPE OF 8.3% **CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
8. RAMP CROSS SLOPE SHALL BE 2% *MAXIMUM.
9. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–0A1**) POURED SEPARATELY FROM RAMPS.
10. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.
11. ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED ARE REQUIRED TO BE CONSTRUCTED WITH BRICK UNIT PAVERS, SEE **BRICK UNIT PAVER PATTERN T02–16** FOR BRICK LAYOUTS.



* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)



CITY OF Vancouver WASHINGTON

PERPINDICULAR RAMP CONSTRUCTION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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CDC	MAH	8/04
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STD. PLAN NO. **T02-08**

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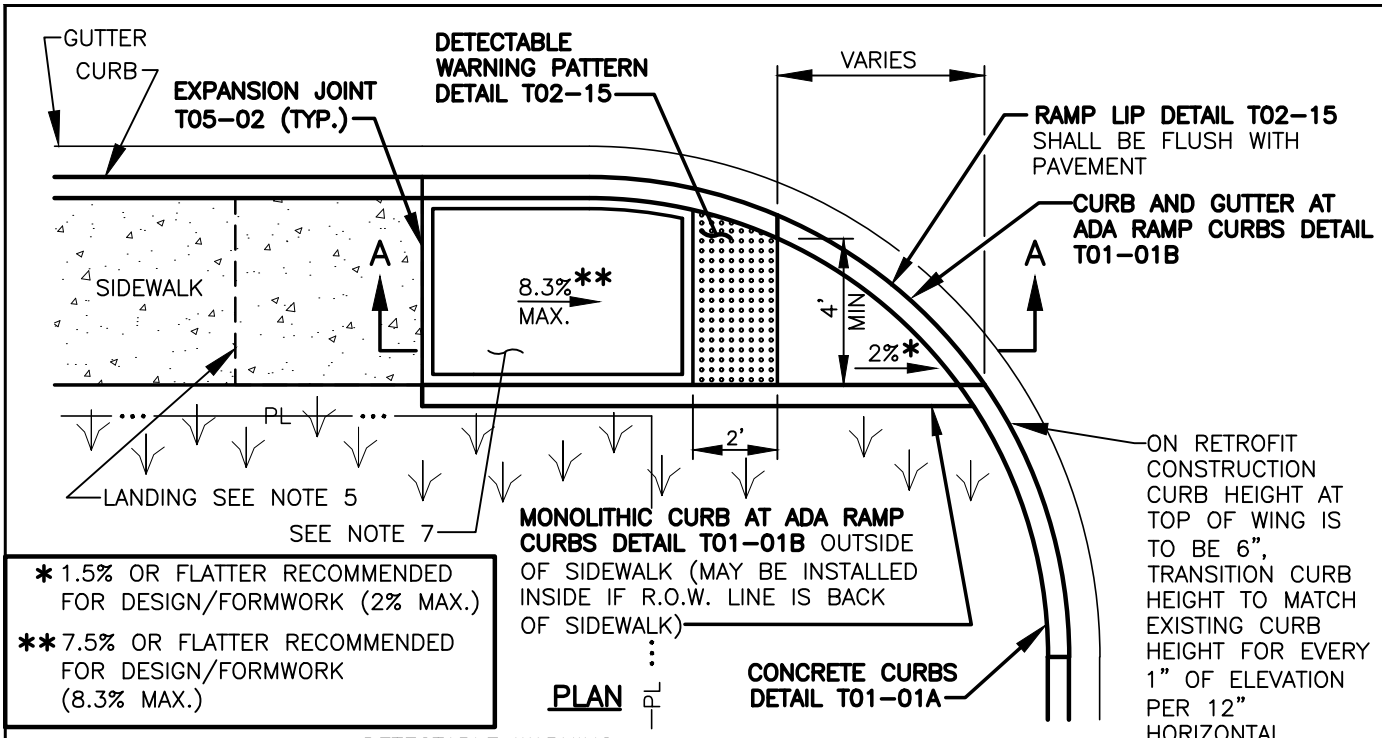

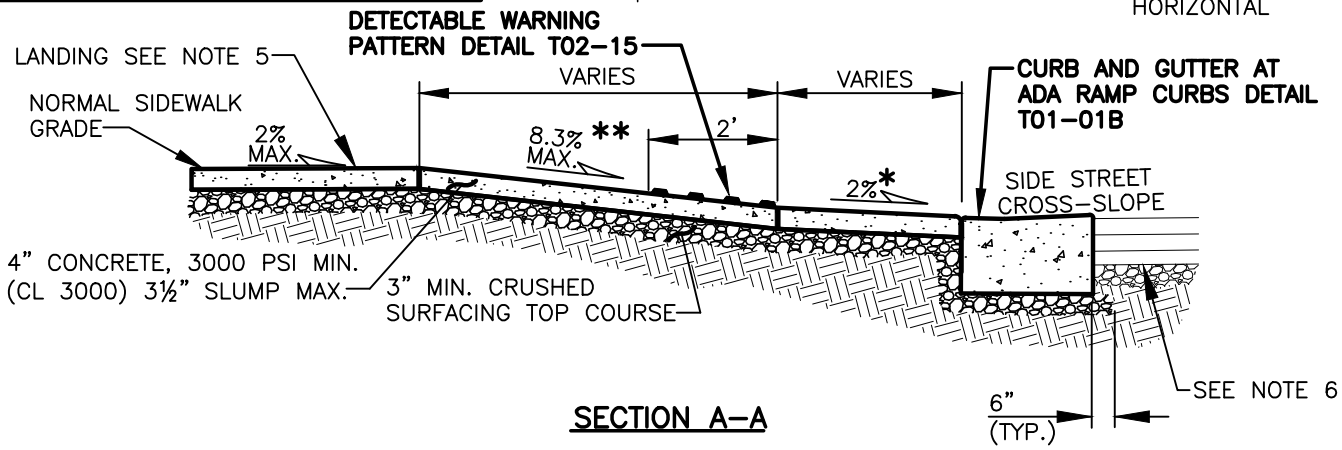

NOTES:

1. AN UNOBSTRUCTED PATH OF TRAVEL WITH A MINIMUM WIDTH OF 4' SHALL BE MAINTAINED.
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
4. IF THE MAXIMUM SLOPE OF 8.3%** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
6. AT THE TOP OF EACH RAMP A 5'X5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
7. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05–01A** WHEN CUTTING EXISTING CURB.
8. RAMP CROSS SLOPE SHALL BE 2%* MAXIMUM.
9. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURB DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
10. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

CITY OF Vancouver WASHINGTON

SINGLE DIRECTIONAL CURB RAMP ATTACHED SIDEWALK

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-09A**

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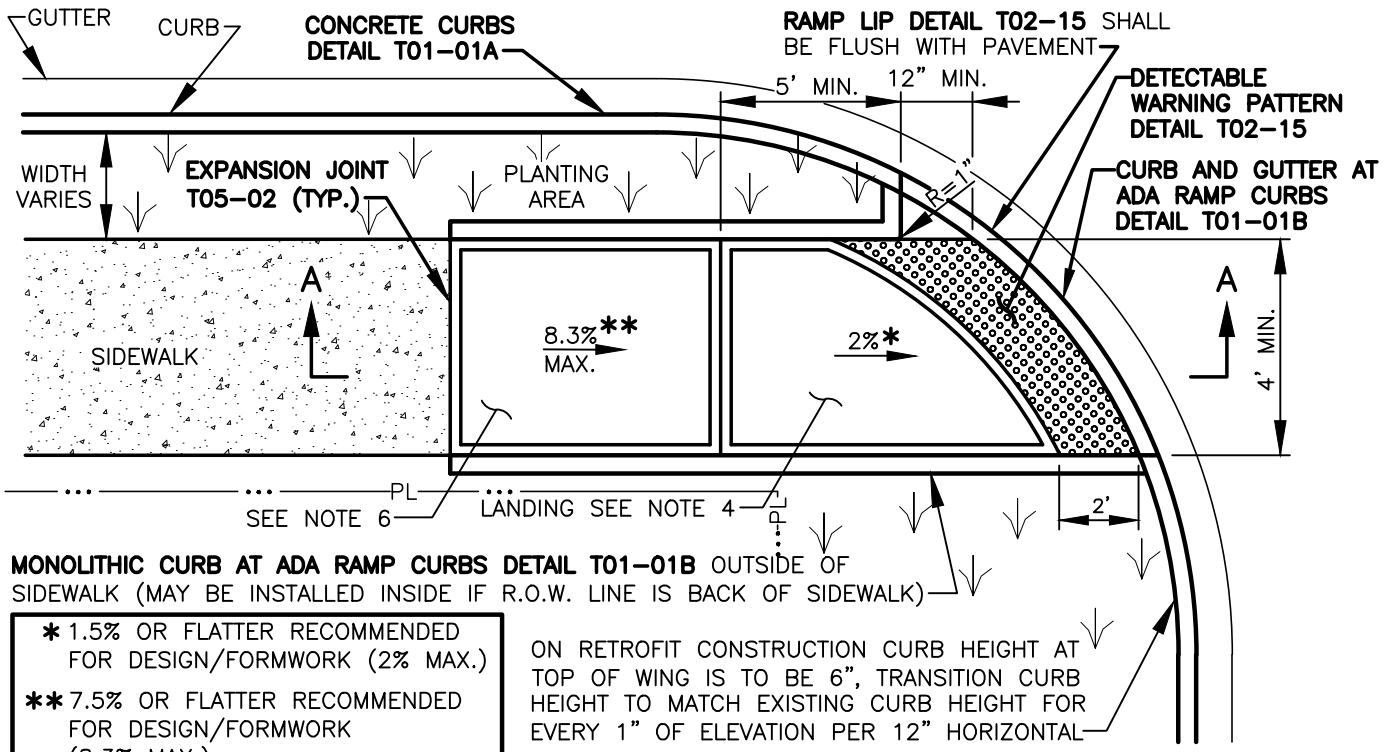
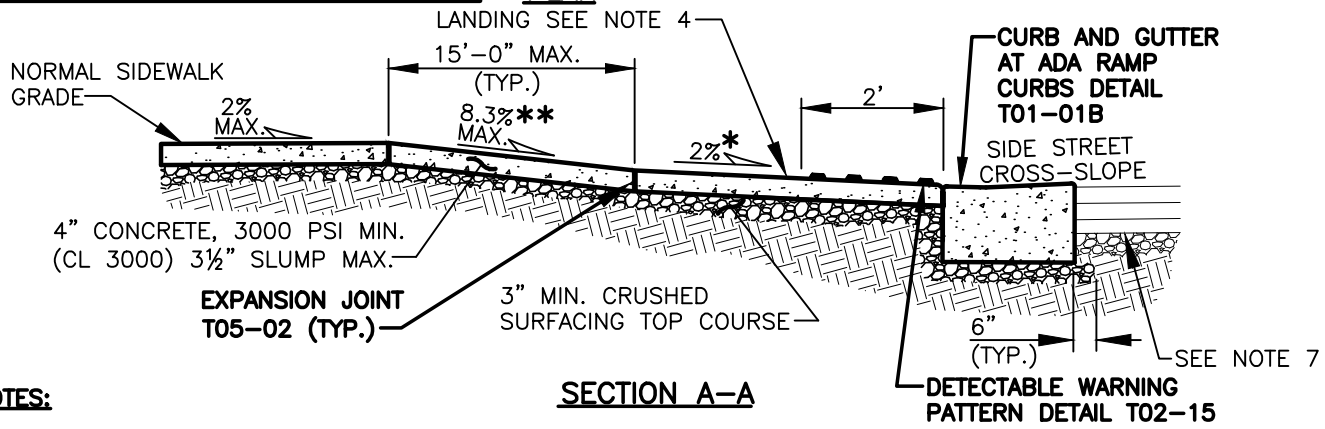


PLAN



SECTION A–A

NOTES:

1. AN UNOBSTRUCTED PATH OF TRAVEL WITH A MINIMUM WIDTH OF 4' SHALL BE MAINTAINED.
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
4. IF THE MAXIMUM SLOPE OF 8.3%** CANNOT BE ACHIEVED DUE TO THE SLOPE OF THE EXISTING SIDEWALK, THE LENGTH OF THE CURB RAMP SHALL NOT BE REQUIRED TO BE LONGER THAN 15 FEET REGARDLESS OF THE RESULTING RAMP SLOPE.
5. AT THE TOP OF EACH RAMP A 5'X5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
6. SEE **PAVEMENT RESTORATION/WIDENING AT CURB DETAIL T05–01A** WHEN CUTTING EXISTING CURB.
7. RAMP CROSS SLOPE SHALL BE 2%* MAXIMUM.
8. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURB DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
9. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

CITY OF Vancouver WASHINGTON

SINGLE DIRECTIONAL CURB RAMP DETACHED SIDEWALK

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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CDC	MAHE	3/17
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STD. PLAN NO. **T02-09B**

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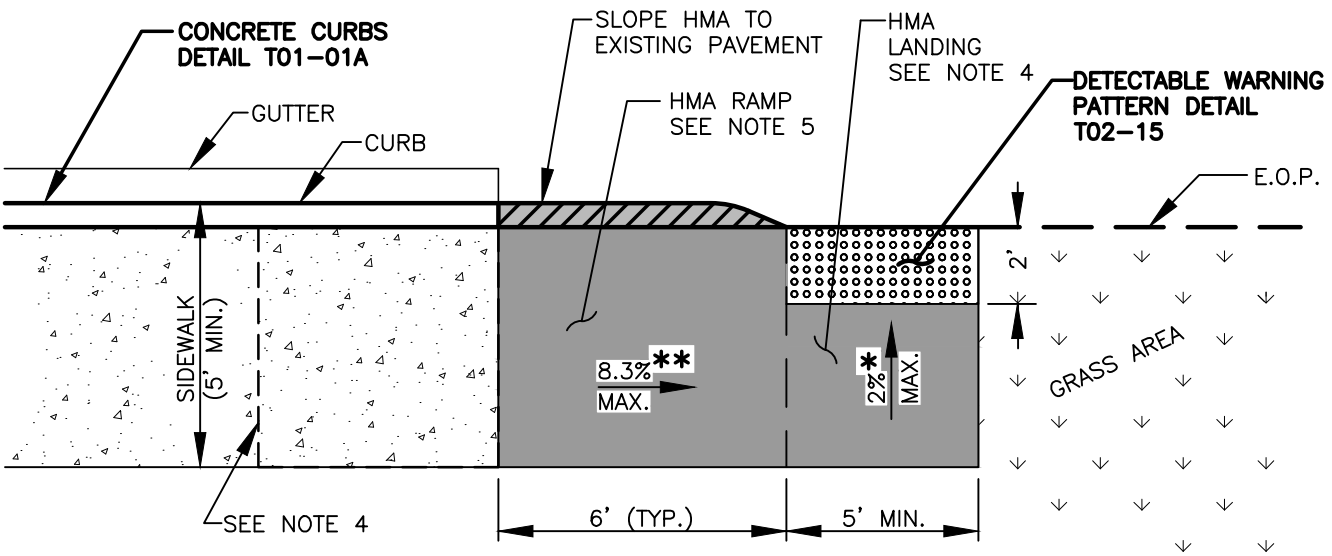
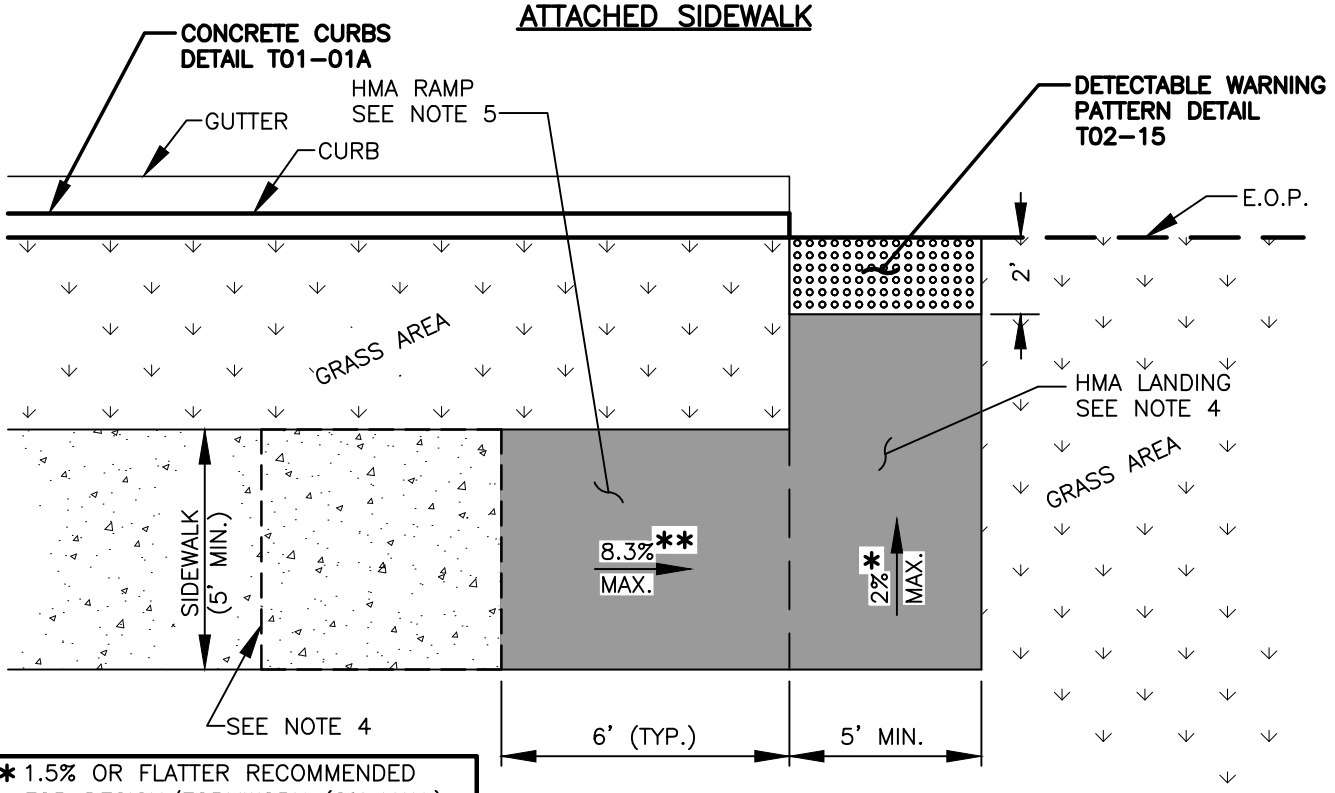


ATTACHED SIDEWALK



DETACHED SIDEWALK

* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)

NOTES:

1. CLASS "G" HOT MIX ASPHALT (HMA) SHALL BE A MINIMUM OF 0.20' DEEP, OVER 3" MIN. DEPTH OF CRUSHED SURFACING TOP COURSE.
2. HMA LANDINGS SHALL BE AT A SLOPE NO GREATER THAN 2%*.
3. BEGIN HMA RAMP AT NEAREST SIDEWALK JOINT.
4. 5'x5' LANDING AREA SHALL BE CONSTRUCTED AND THE LONGITUDINAL AND THE CROSS SLOPE SHALL NOT EXCEED 2%* EACH DIRECTION.
5. RAMP CROSS SLOPE SHALL BE 2%* MAXIMUM.

CITY OF Vancouver WASHINGTON

TEMPORARY HMA RAMP

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
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STD. PLAN NO. **T02-10**

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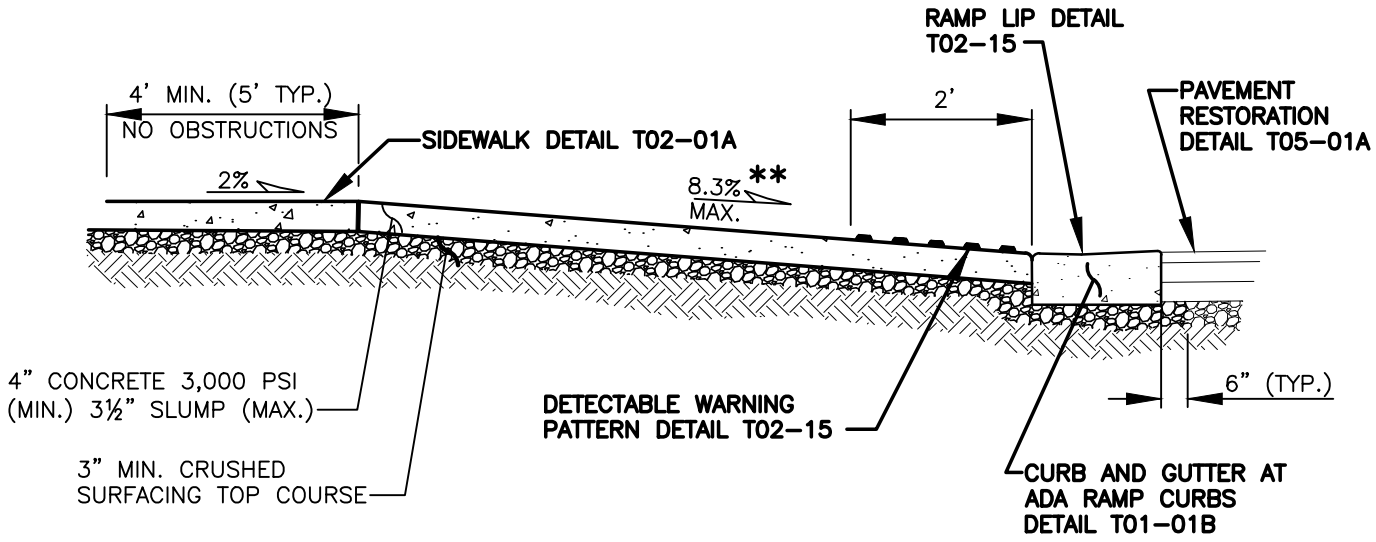
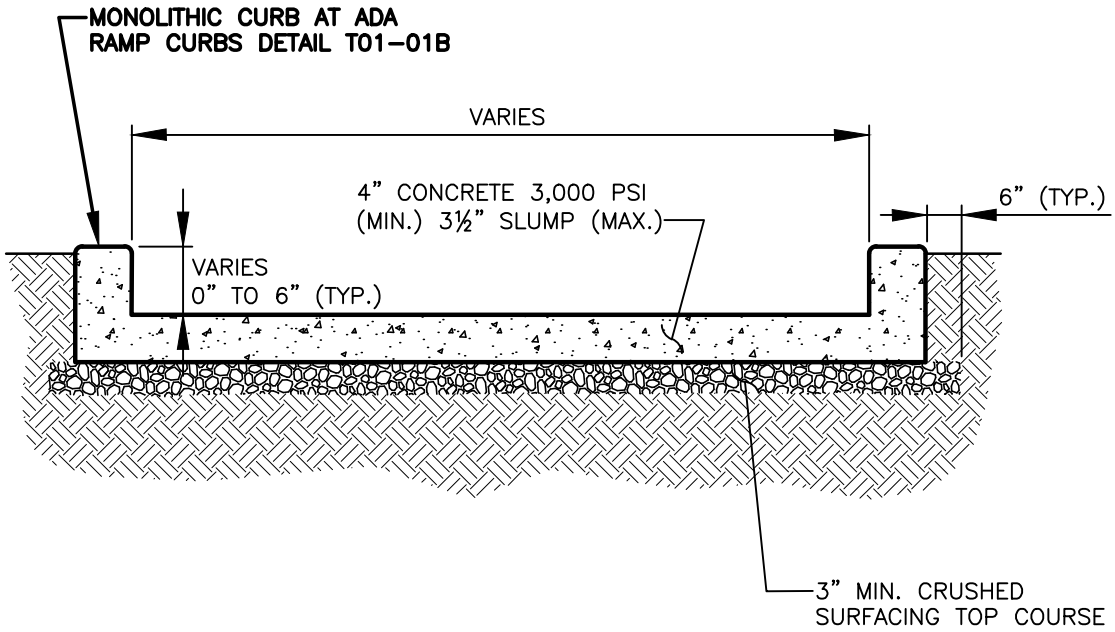


SECTION A–A
FOR DETAILS T02–04B AND T02–08



SECTION B–B
FOR DETAIL T02–08

NOTES:

1. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.

   ADA RAMP AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

2. ~~2.~~

3. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)

CITY OF Vancouver WASHINGTON

STANDARD LANDING CROSS SECTIONS A-A AND B-B

CITY OF VANCOUVER
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STD. PLAN NO.
T02-11

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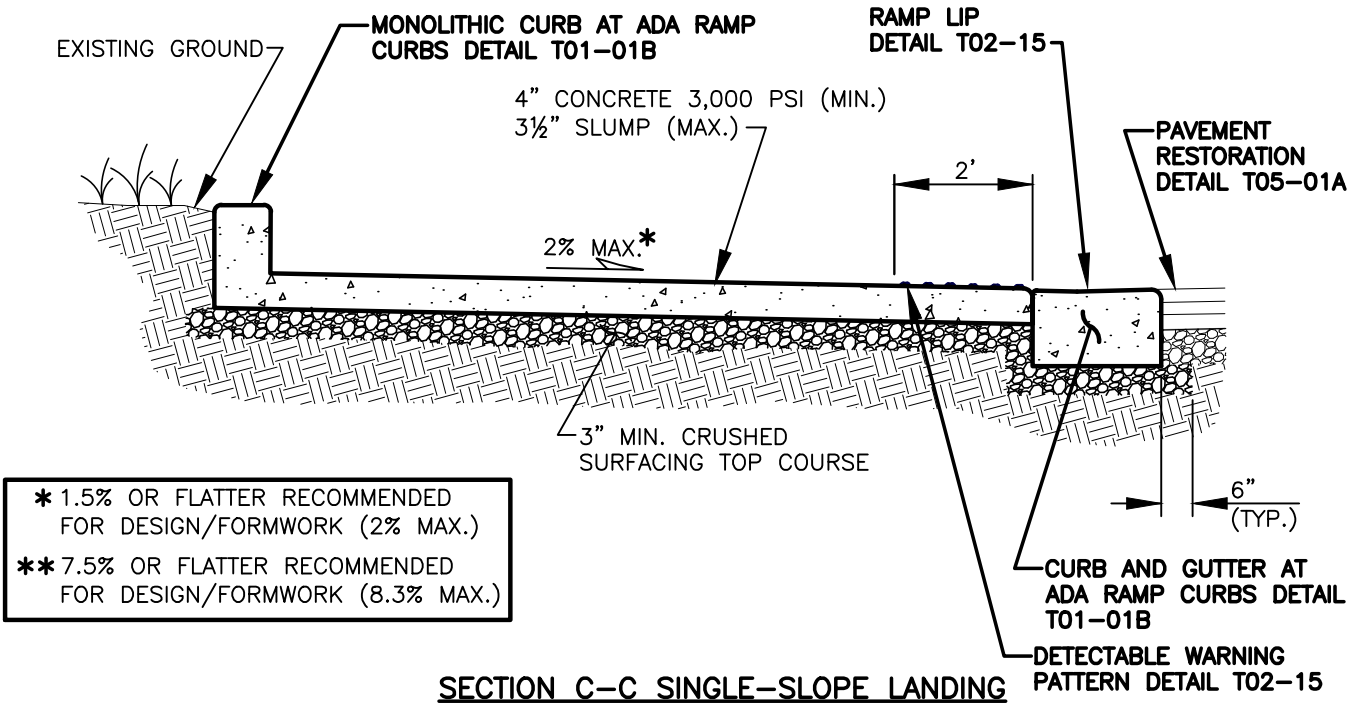
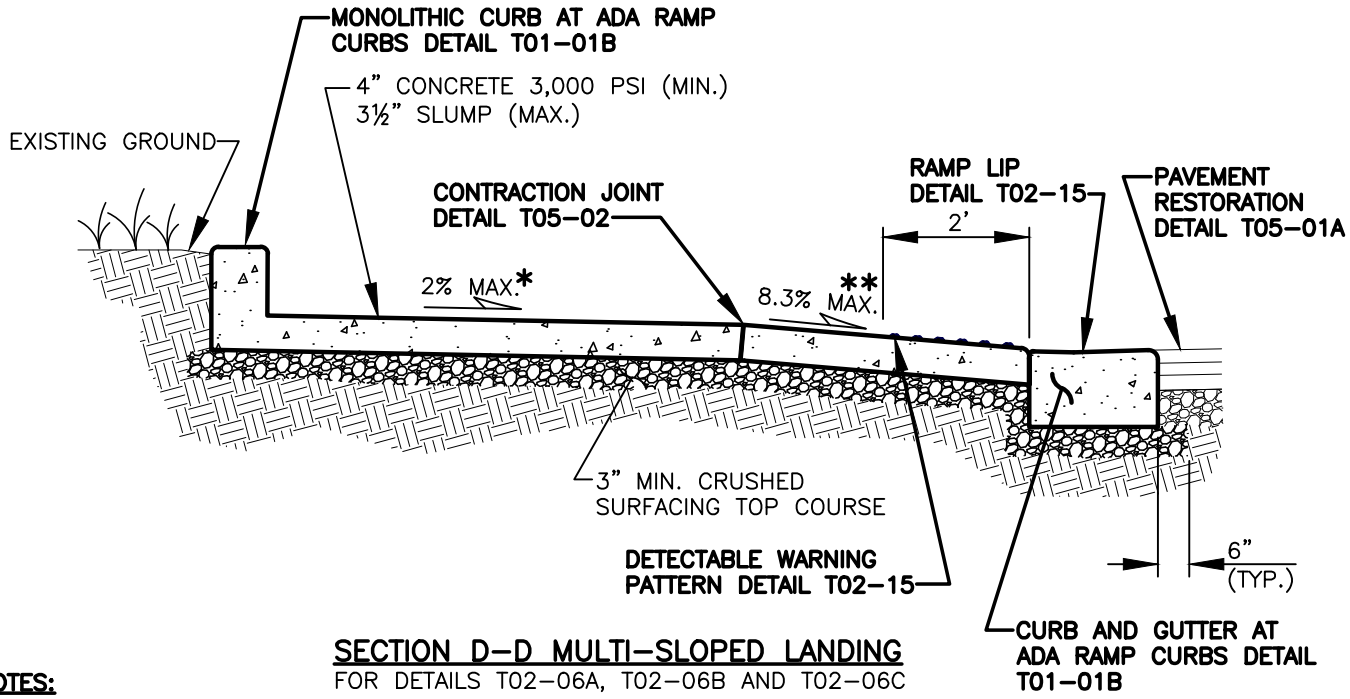


* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)

** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)

SECTION C–C SINGLE–SLOPE LANDING

FOR DETAIL T02–07B



SECTION D–D MULTI–SLOPED LANDING

FOR DETAILS T02–06A, T02–06B AND T02–06C

NOTES:

1. IF THE AREA BEHIND THE SIDEWALK IS VEGETATED, THE BACK CURB MAY BE REPLACED WITH A SLOPE NO STEEPER THAN 4:1.
2. COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN. DEPTH).
3. A WALL MAY BE USED IN LIEU OF CURB TO MAINTAIN 2%* MAX. SLOPE.
4. CURB OR WALL MAY BE INSTALLED INSIDE OF SIDEWALK IF R.O.W. LINE IS AT BACK OF SIDEWALK.
5. TYPE A–1 AND E–1 CURB (SEE **CONCRETE CURBS DETAIL T01–01A**) POURED SEPARATELY FROM RAMPS.
6. **ADA RAMP** AND **GRADE CORRECTION CURBS** POURED MONOLITHICALLY.

CITY OF Vancouver WASHINGTON

STANDARD LANDING CROSS SECTIONS C-C AND D-D

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
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STD. PLAN NO. **T02-13**

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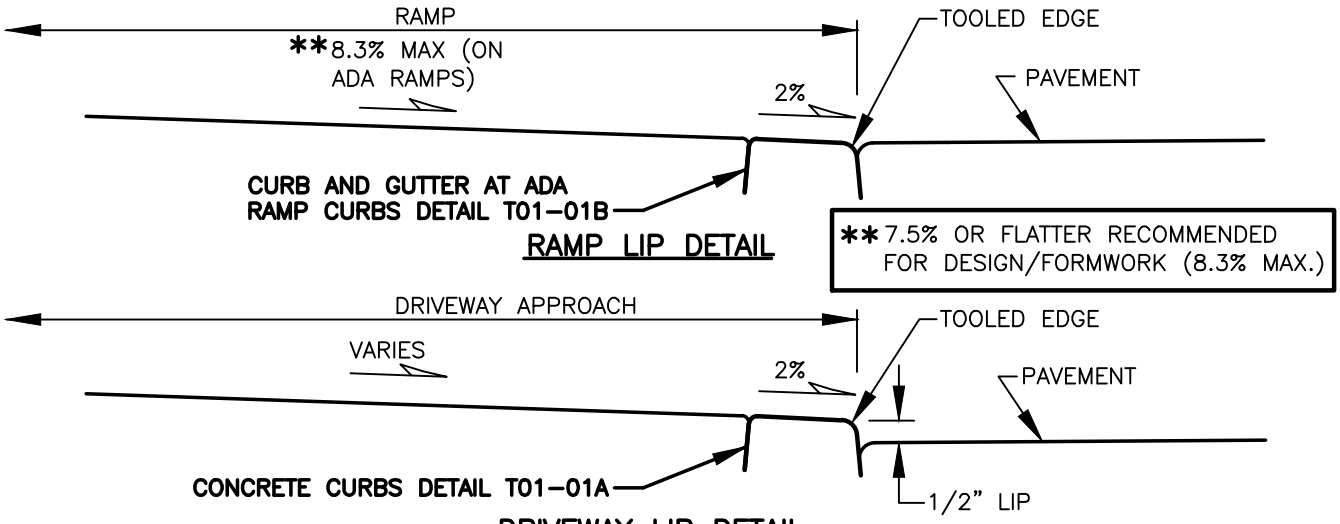


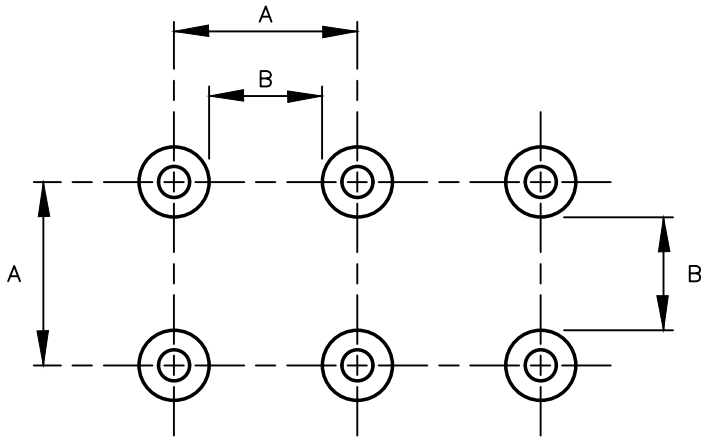
RAMP LIP DETAIL

DRIVEWAY LIP DETAIL

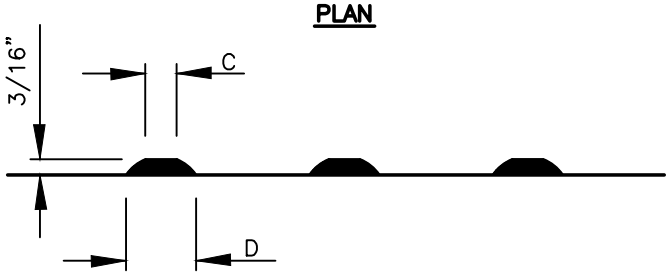
TRUNCATED DOMES COLOR SCHEME

LOCATION	COLORS
ALL BRICK RAMPS	WHITE
STREETS WITH A MAJORITY OF RESIDENTIAL FRONTAGE	BRICK RED
NON-RESIDENTIAL ARTERIAL STREETS	SAFETY YELLOW

	MIN.	MAX.
A	1 5/8"	2 3/8"
B	2/3"	1 1/2"
C	7/16"	3/4"
D	7/8"	1 7/16"



PLAN



ELEVATION

NOTES:

1. MANUFACTURERS SHALL MEET THE REQUIREMENTS LISTED UNDER THE CONTRACT SPECIAL PROVISIONS AND MUST BE ON THE CITY OF VANCOUVER'S APPROVED PRODUCT LIST. THE PRODUCT LIST IS TAKEN FROM WSDOT PRODUCT LIST, THAT IS UPDATED PERIODICALLY.
2. DETECTABLE WARNINGS SHALL BE MANUFACTURED USING THE MATERIALS SPECIFIED ON THE PLAN SHEETS WITH THE DOME DIMENSIONS AND SPACING SHOWN AND INSTALLED PER THE MANUFACTURER'S RECOMMENDED PROCEDURES.
3. DETECTABLE WARNINGS SHALL BE INSET INTO NEW CONCRETE. GLUED ON OR NAILED DOWN PRODUCTS ARE NOT ACCEPTABLE FOR NEW CONSTRUCTION.
4. ANY VARIATION FROM THE CITY DETECTABLE WARNING POLICY REQUIRES APPROVAL FROM THE DIRECTOR.
5. SAFETY YELLOW TRUNCATED DOMES ARE RECOMMENDED IN SCHOOL ZONES AND ALONG SCHOOL ROUTES.

DETECTABLE WARNING PATTERN DETAIL

CITY OF Vancouver WASHINGTON

RAMP LIP, DRIVEWAY LIP, AND DETECTABLE WARNING PATTERN

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-15**

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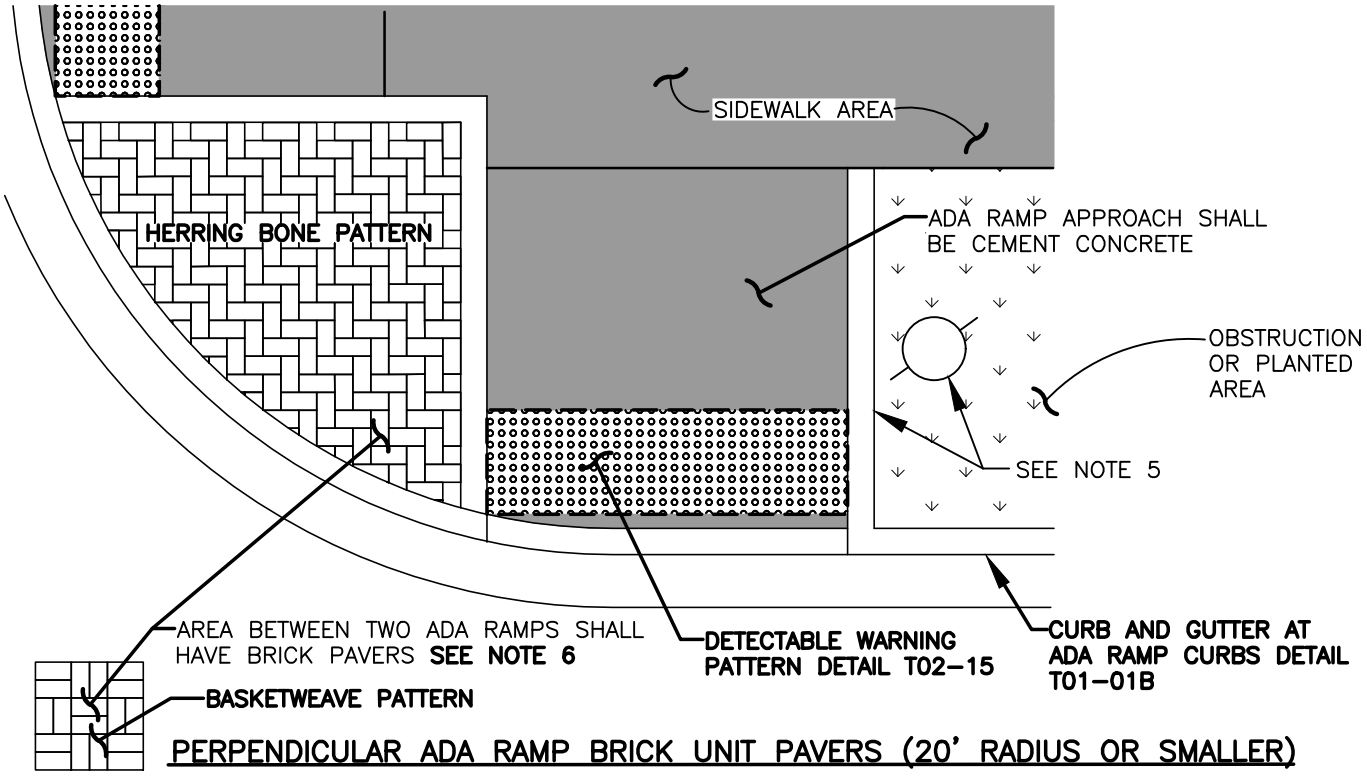
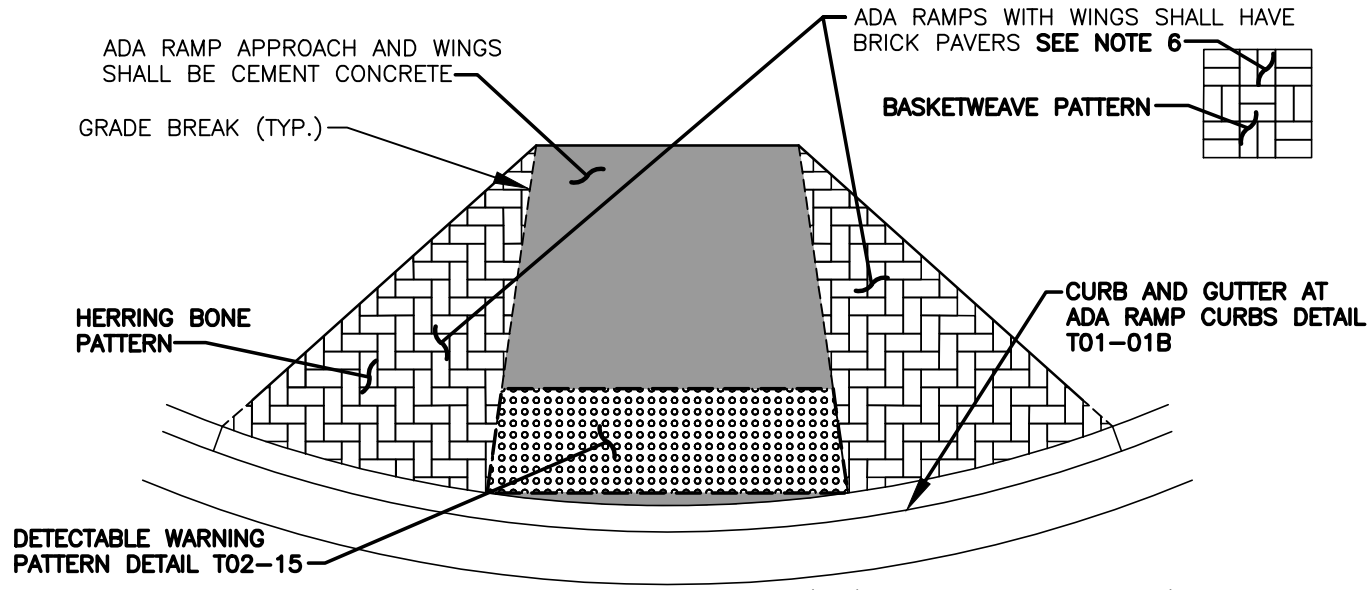


PERPENDICULAR ADA RAMP BRICK UNIT PAVERS (20' RADIUS OR SMALLER)



WINGED ADA RAMP BRICK UNIT PAVERS (25' RADIUS OR LARGER)

NOTES:

1. SAMPLE OF BRICK AND MORTAR COLOR SHALL BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION. BRICK MANUFACTURED BY MUTUAL MATERIALS, "BURGUNDY–MICA TILE" OR ENDICOTT "MEDIUM IRONSPOT #46", PREMIXED MORTAR ASTM C270, TYPE S, 1800 PSI 28 DAY STRENGTH OR APPROVED EQUAL.
2. THE BRICK PATTERN IS REQUIRED FOR ALL RAMPS IN AREAS ZONED CITY CENTER OR WHERE THE HERITAGE SIDEWALK IS CONSTRUCTED.
3. SURFACE JOINT NOT REQUIRED WHEN USING BRICK PAVERS. TRANSITION TO FIT BETWEEN GRADES.
4. THE DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE WHITE ON THE BRICK UNIT PAVER PATTERN.
5. RAMP WINGS MAY BE REPLACED WITH A MONOLITHIC CURB **ADA CURB RAMPS DETAIL T01–01B** IF OBSTRUCTION OR PLANTER PREVENTS PEDESTRIAN TRAFFIC IN WING AREA.
6. RAMP WING BRICK PATTERN OPTIONS ARE EITHER HERRING BONE OR BASKET WEAVE PATTERNS.

CITY OF Vancouver WASHINGTON

BRICK UNIT PAVER PATTERN

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
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STD. PLAN NO. **T02-16**

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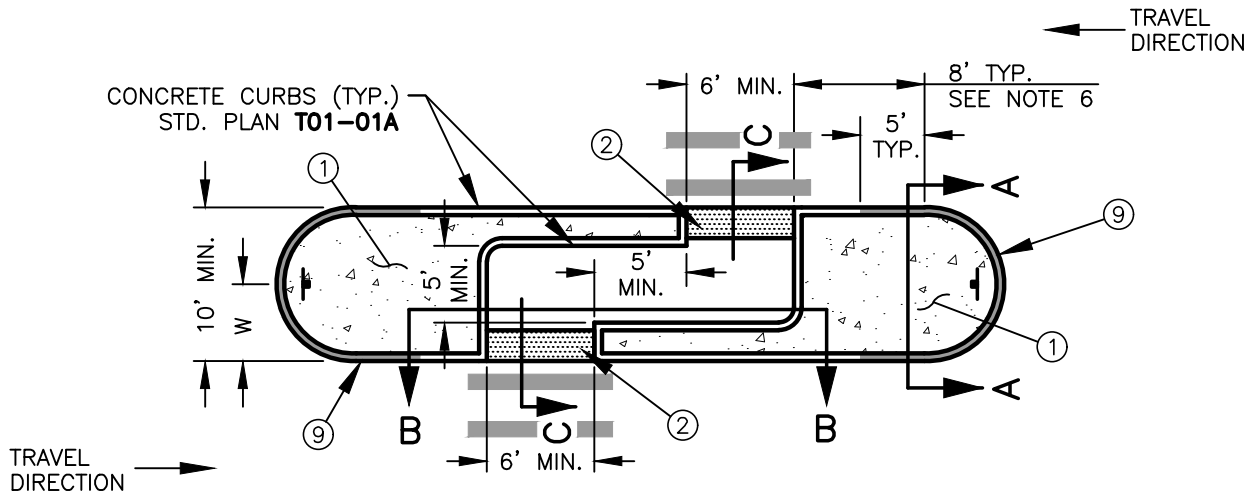
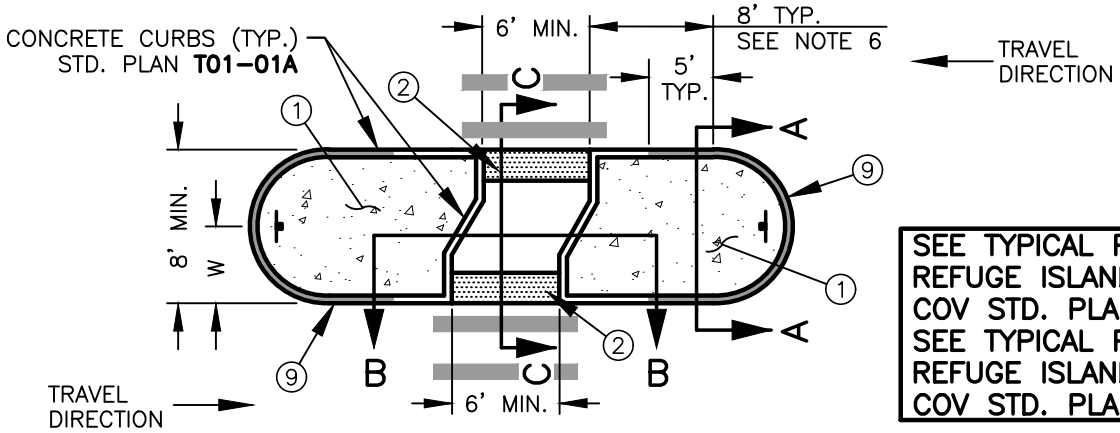
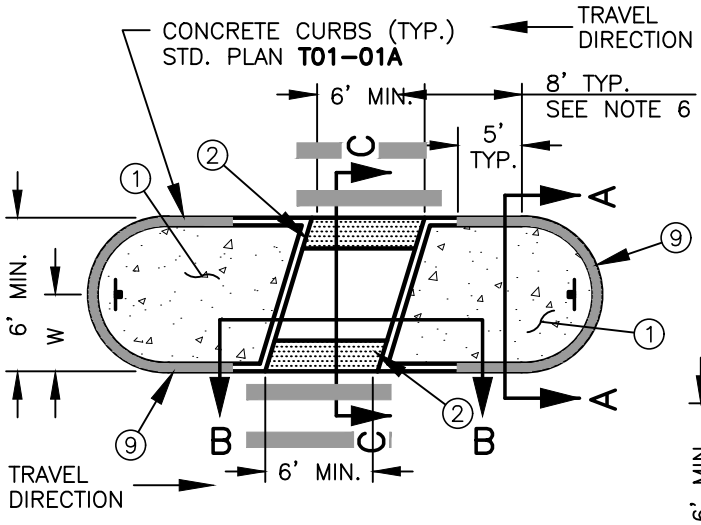


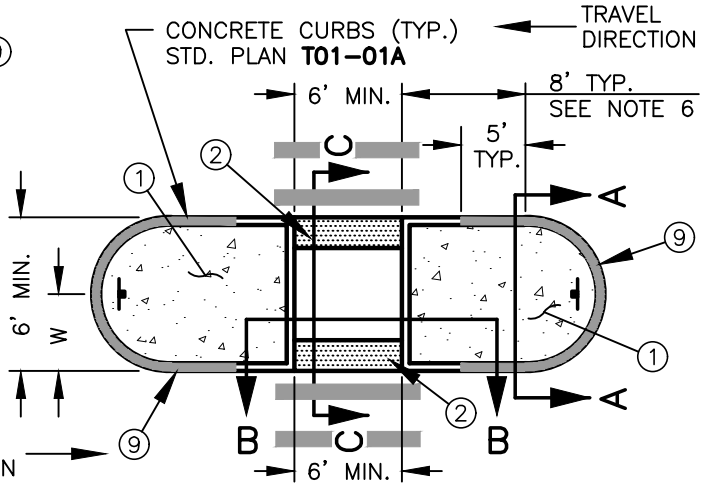
PREFERRED PEDESTRIAN REFUGE ISLAND (10' MIN)



SEE TYPICAL PEDESTRIAN REFUGE ISLAND SECTIONS ON COV STD. PLAN T02–17C. SEE TYPICAL PEDESTRIAN REFUGE ISLAND NOTES ON COV STD. PLAN T02–17D.

ALTERNATE PEDESTRIAN REFUGE ISLAND – OPTION A (8' MIN)





ALTERNATE PEDESTRIAN REFUGE ISLAND – OPTION B (6' MIN)

CITY OF Vancouver WASHINGTON

PEDESTRIAN REFUGE ISLAND MID-BLOCK

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-17A**

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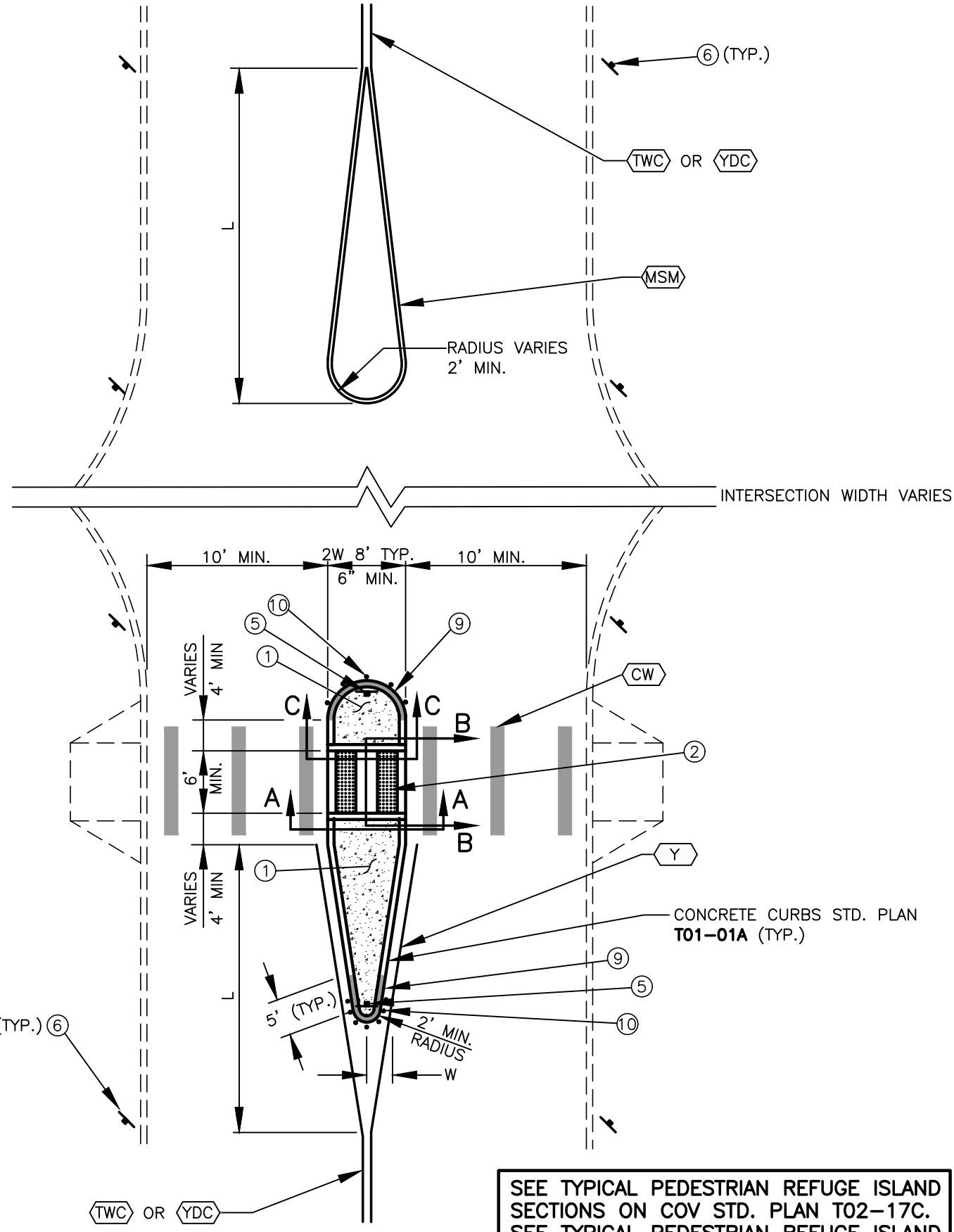

SEE TYPICAL PEDESTRIAN REFUGE ISLAND SECTIONS ON COV STD. PLAN T02-17C.
SEE TYPICAL PEDESTRIAN REFUGE ISLAND NOTES ON COV STD. PLAN T02-17D.

CITY OF Vancouver WASHINGTON

TYPE 2 INTERSECTION PEDESTRIAN REFUGE ISLAND WITH NOSE

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-17B**

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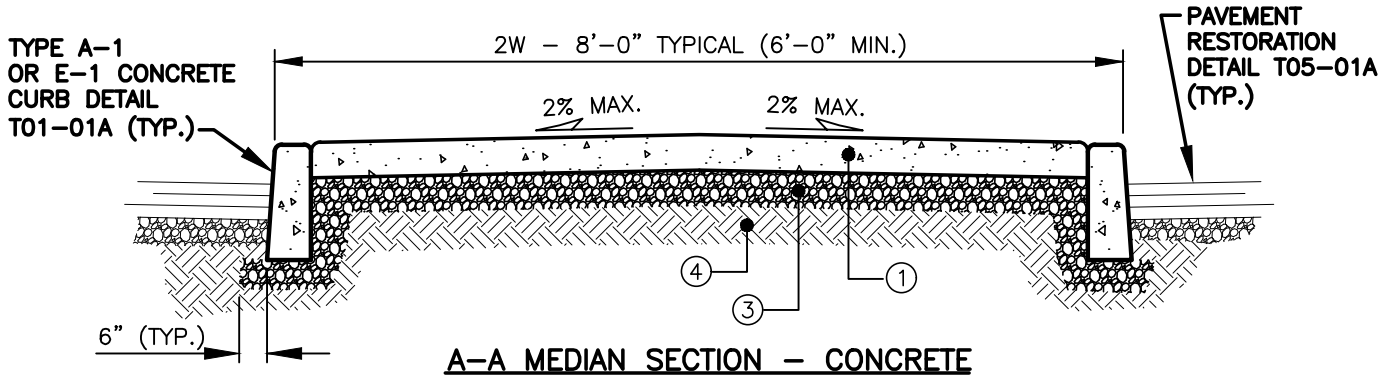
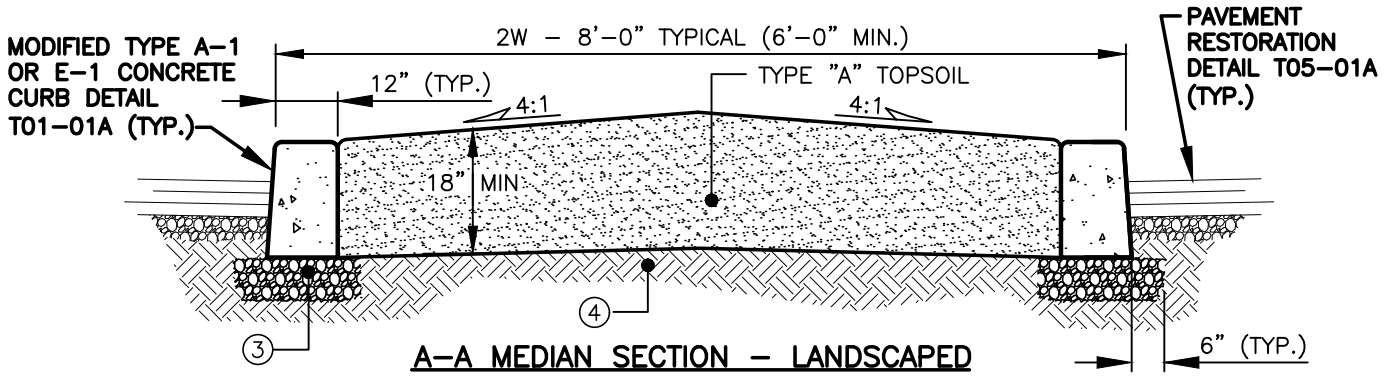
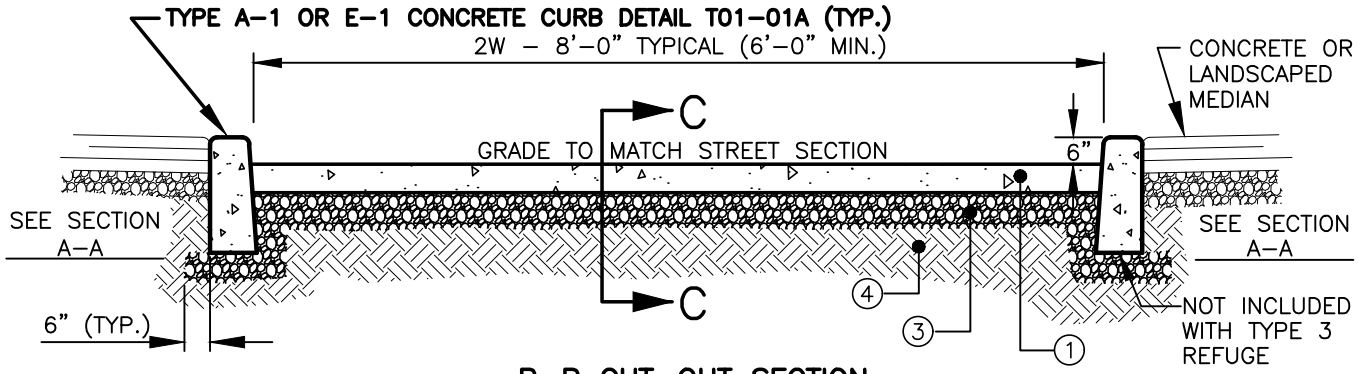


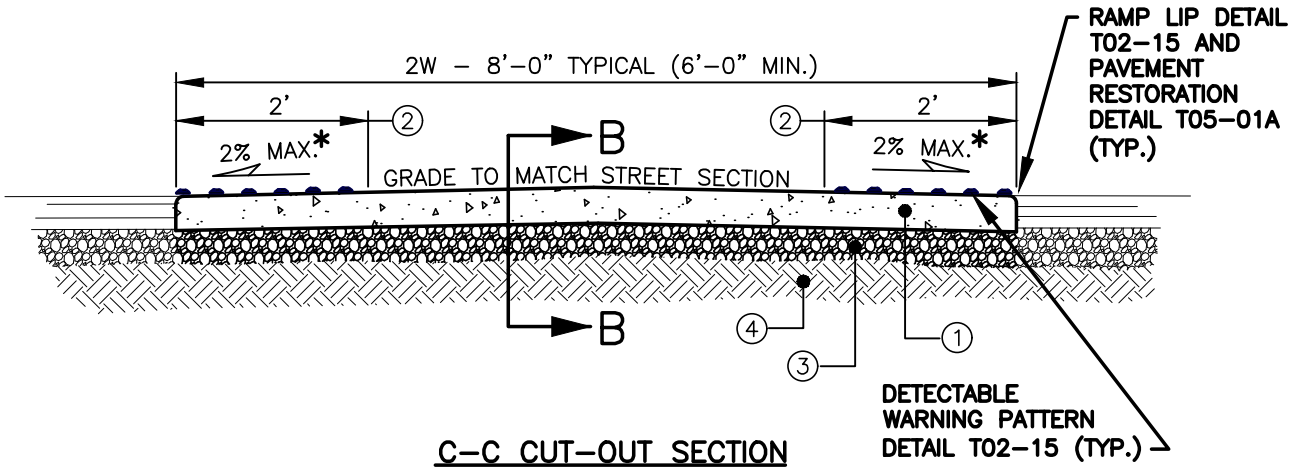
A–A MEDIAN SECTION – CONCRETE



A–A MEDIAN SECTION – LANDSCAPED



B–B CUT–OUT SECTION



C–C CUT–OUT SECTION

SEE TYPICAL PEDESTRIAN REFUGE ISLAND NOTES ON COV STD. PLAN T02–17D.

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CITY OF Vancouver WASHINGTON

TYPICAL PEDESTRIAN REFUGE ISLAND SECTIONS

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-17C**

GENERAL NOTES:

1. **FOR TAPER LENGTH L=WS**
   L = TAPER LENGTH
   W = SHIFT DISTANCE
   S = SPEED (POSTED OR 85TH PERCENTILE, WHICHEVER IS GREATER)

2. SEE CITY OF VANCOUVER SIGN MOUNTING STD. PLAN **T29–02** AND SIGN POST/ANCHOR INSTALLATION STD. PLAN **T29–03** FOR ALL SIGN INSTALLATIONS.

3. CROSSWALK SHALL BE ILLUMINATED.

4. SHY DISTANCE MAY BE REDUCED TO 1 FOOT FOR POSTED SPEEDS OF 35 MPH OR LESS.

5. WIDTH SHALL BE SAME WIDTH AS MULTI–USE PATHS IF ONE GOES THROUGH A RAISED MEDIAN.

6. CITY TRAFFIC ENGINEER APPROVAL REQUIRED TO USE A DIMENSION OF LESS THAN 8 FEET.

7. STRIPING SHOWN WITH PREFERRED PEDESTRIAN REFUGE ISLAND ON STANDARD PLAN **T02–17E**, TO BE USED AS TYPICAL WITH ALTERNATE PEDESTRIAN REFUGE ISLAND OPTIONS.

CONSTRUCTION NOTES:

① MEDIAN SHALL BE CONCRETE (CL 3000) PAVEMENT. OTHER OPTIONS WITHIN THE MEDIAN ARE CONCRETE PAVEMENT WITH COLORING OR TEXTURE, PAVERS, OR LANDSCAPING WITH APPROVAL THE CITY ENGINEER. CONCRETE SCORING SHALL MATCH TEXTURE PATTERN. NO SIGHT OBSTRUCTIONS TALLER THAN 2' WITHOUT APPROVAL FROM THE CITY TRAFFIC ENGINEER.

② DETECTABLE WARNING AREA SHALL BE 2' IN WIDTH FROM THE FACE OF MEDIAN ISLAND CURB (TYP.). SEE DETECTABLE WARNING PATTERN STANDARD PLAN **T02–15**.

③ 3" MIN. DEPTH OF CRUSHED SURFACING TOP COURSE.

④ COMPACT SUBGRADE AND CRUSHED SURFACING TOP COURSE TO 95% OF MAXIMUM DRY DENSITY (3" MIN.).

⑤ "STOP FOR PEDESTRIANS" (R1–6A) SHALL BE PLACED PER MUTCD STANDARDS.

⑥ POST "NO PARKING ANYTIME" (R7–1a) SIGN FOR TRAVEL LANES LESS THAN 18' (TYP.). SIGNS SHALL BE PLACED TO MEET SIGHT DISTANCE REQUIREMENTS.

⑦ 30" "PEDESTRIAN CROSSING" (W11–2) SIGN AND "DIAGONAL DOWNWARD ARROW" (W16–7P) SIGN SHALL BE PLACED PER MUTCD STANDARDS. SEE STANDARD PLANS **T29–03 AND T29–04** FOR PLACEMENT.

⑧ STANDARD STREET ILLUMINATION. SEE STANDARD PLANS **T21–01A THROUGH T21–01D**.

⑨ PAINT CURB YELLOW AROUND LEADING RADIUS.

⑩ PLACE RAISED PAVEMENT MARKERS (TYPE 2YY) ON 1'–0" CENTERS AROUND LEADING RADIUS. SEE STANDARD PLAN **T29–20**.

STRIPING NOTES:

〈CW〉 INSTALL THERMOPLASTIC (PLASTIC) STANDARD CROSSWALK, SEE STANDARD PLAN **T29–41**.

〈TWC〉 INSTALL 4" YELLOW TWO–WAY CENTER LINE MARKINGS WITH RPM's (PAINT LINE), SEE STD. PLAN **T29–45**.

〈TWL〉 INSTALL 4" YELLOW TWO–WAY LEFT TURN LANE MARKINGS WITH RPM's (PAINT LINE), SEE STANDARD PLAN **T29–46**.

〈MSM〉 INSTALL 4" YELLOW PAINTED MEDIAN STRIPE MARKINGS (VARYING WIDTH WITH RPM's (PAINT LINE), SEE STANDARD PLAN **T29–47**.

〈YDC〉 INSTALL 4" YELLOW PAINTED DOUBLE CENTER LINE MARKINGS WITH RPM's (PAINT LINE), SEE STANDARD PLAN **T29–48**.

〈Y〉 INSTALL 4" YELLOW PAINTED EDGE LINE WITH RPM's (PAINT LINE), SEE STANDARD PLAN **T29–48**.

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CITY OF Vancouver WASHINGTON

TYPICAL PEDESTRIAN REFUGE ISLAND NOTES

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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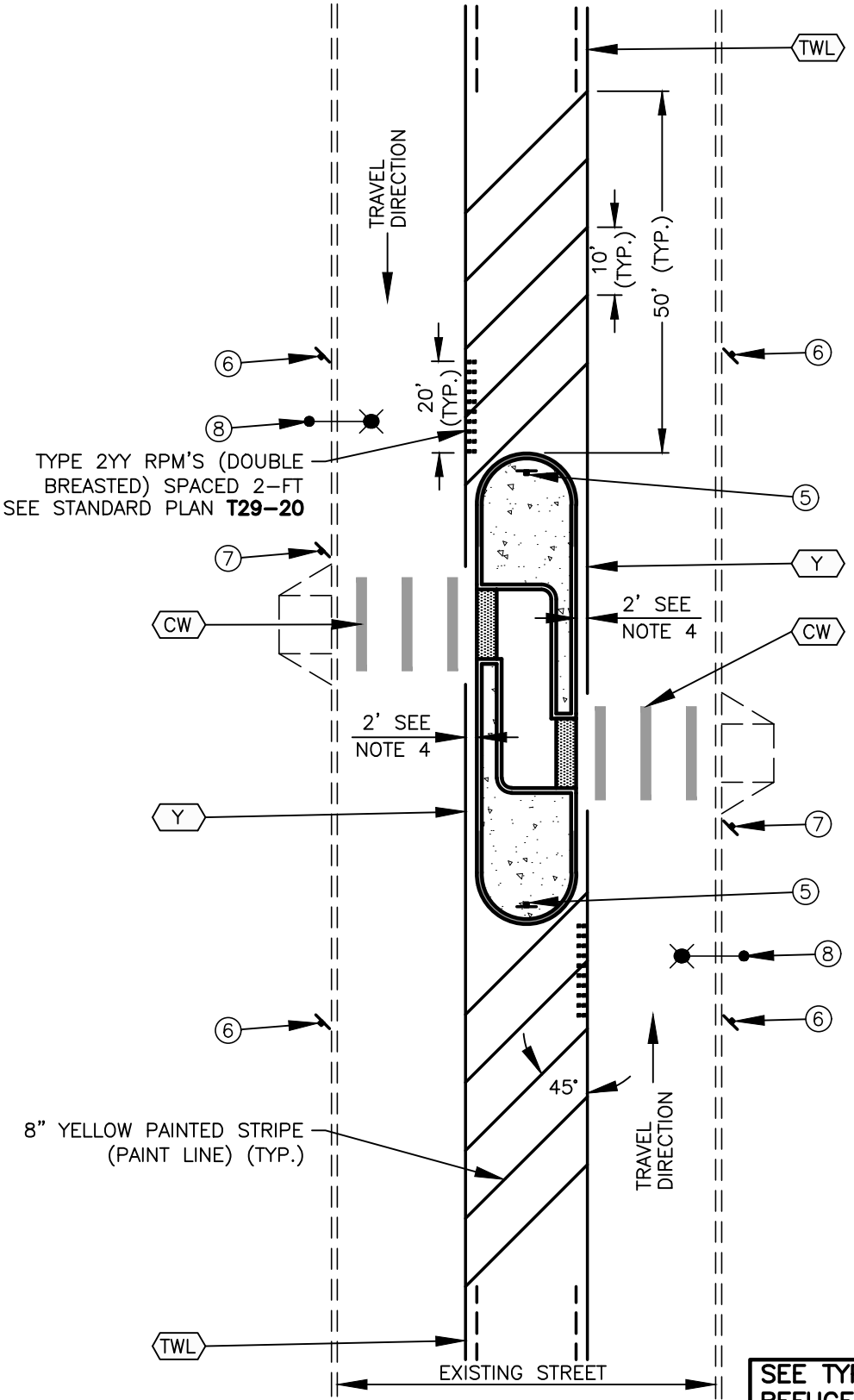
STD. PLAN NO. **T02-17D**



SEE TYPICAL PEDESTRIAN REFUGE ISLAND OPTIONS ON COV STD. PLAN T02-17A.
SEE TYPICAL PEDESTRIAN REFUGE ISLAND NOTES ON COV STD. PLAN T02-17D.

CITY OF Vancouver WASHINGTON

TYPICAL PEDESTRIAN REFUGE ISLAND SIGNING AND STRIPING

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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STD. PLAN NO. **T02-17E**

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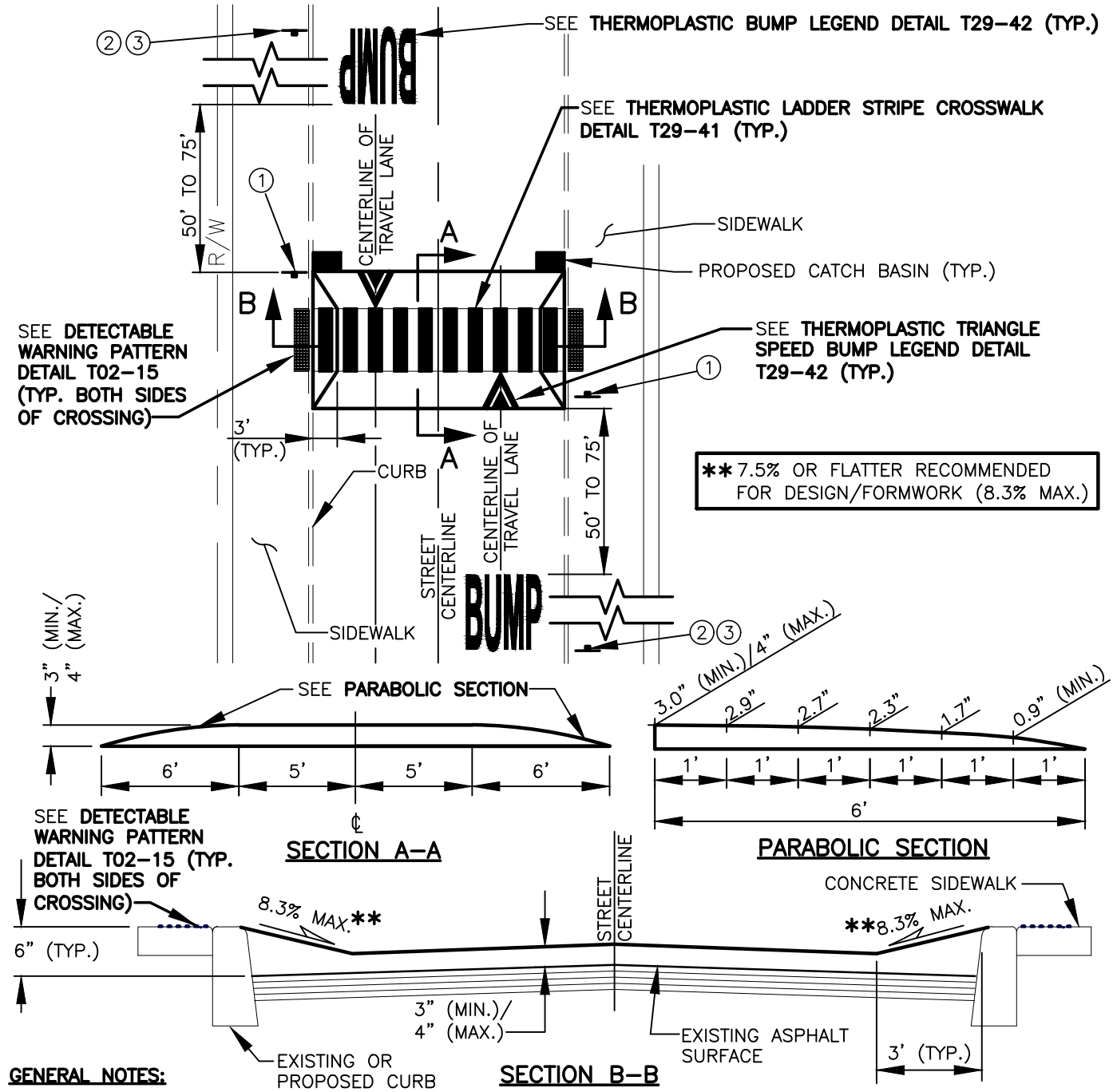

GENERAL NOTES:

1. RAISED CROSSWALK REQUIRES STORM WATER TO BE COLLECTED AND CONVEYED TO AN APPROPRIATE LOCATION.
2. RAISED CROSSWALK SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.

SIGNS:

(1) INSTALL MUTCD STANDARD SIGNS W11-2 AND W16-7 UNLESS IN SCHOOL WALK ROUTE THEN USE S1-1 AND W16-7. SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03.**

(2) INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03.**

(3) IN PLACING SIGNS AT MULTIPLE RAISED CROSSWALKS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCK OR MILEAGE, NOT AT EACH RAISED CROSSWALK.

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CITY OF Vancouver WASHINGTON

RAISED CROSSWALK PREFERRED OPTION

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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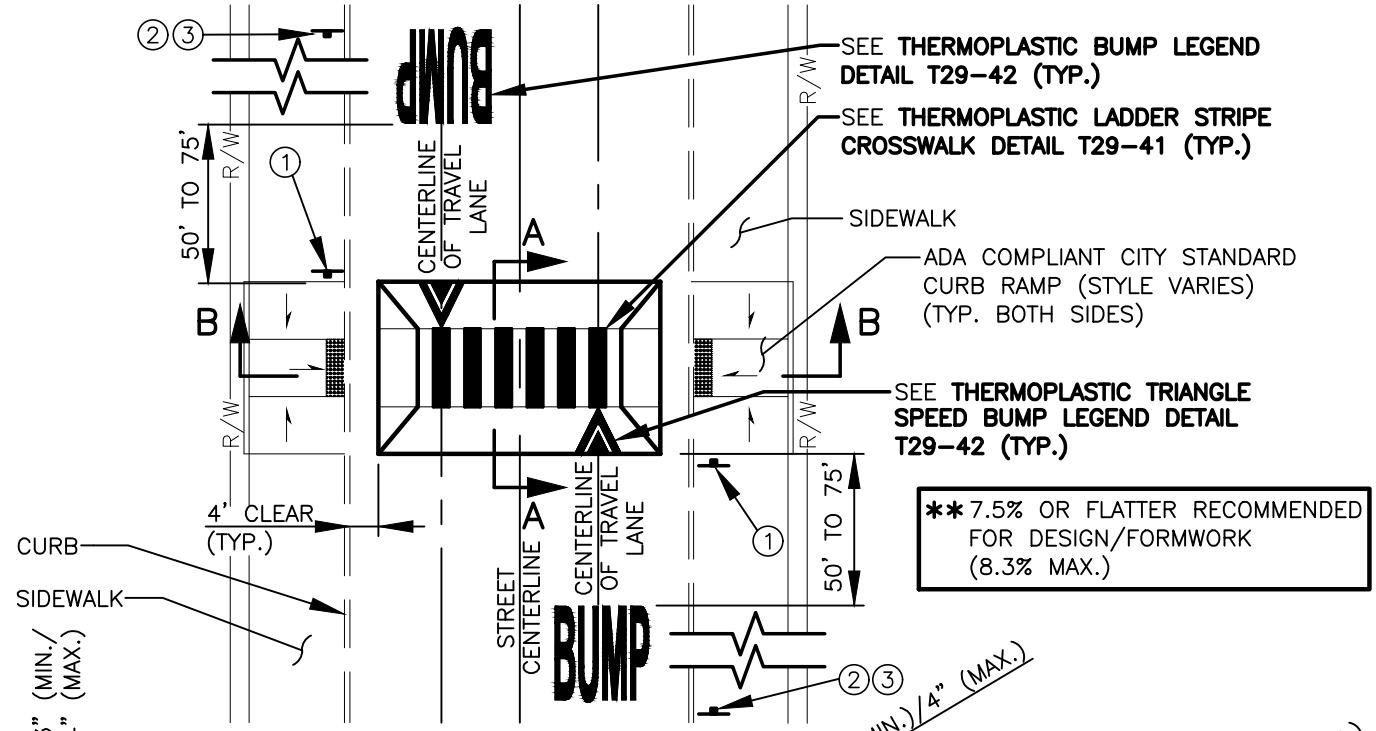
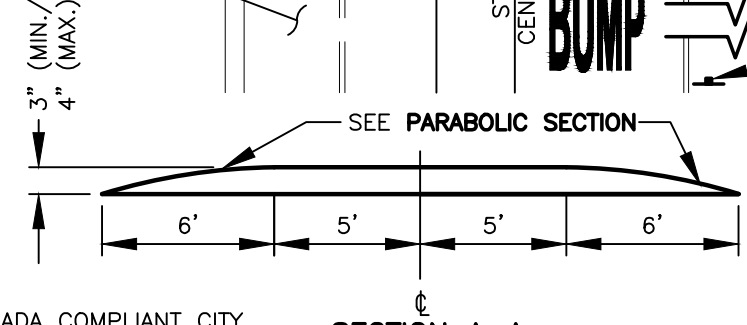
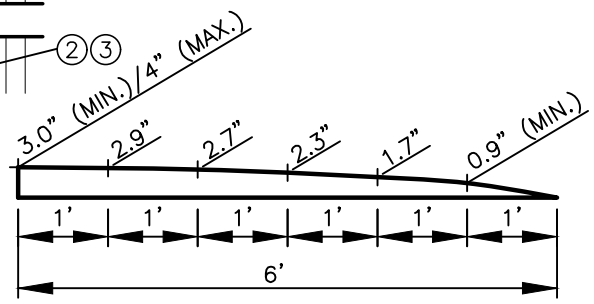
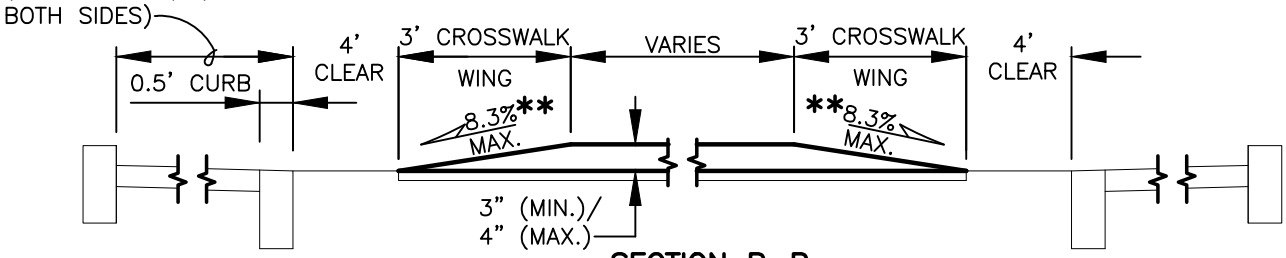




SECTION A–A



PARABOLIC SECTION



SECTION B–B

GENERAL NOTES:

1. RAISED CROSSWALK REQUIRES STORM WATER TO BE COLLECTED AND CONVEYED TO AN APPROPRIATE LOCATION.
2. USE THIS OPTION ONLY WITH APPROVAL FROM CITY DIRECTOR.
3. RAISED CROSSWALK SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H–22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.

SIGNS:

① INSTALL MUTCD STANDARD SIGNS W11–2 AND W16–7 UNLESS IN SCHOOL WALK ROUTE THEN USE S1–1 AND W16–7. SEE **SIGN MOUNTING DETAILS T29–02 AND T29–03.**

② INSTALL MUTCD STANDARD W17–1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29–02 AND T29–03.**

③ IN PLACING SIGNS AT MULTIPLE RAISED CROSSWALKS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16–4" DESIGNATING HOW MANY BLOCK OR MILEAGE, NOT AT EACH RAISED CROSSWALK.

CITY OF Vancouver WASHINGTON

RAISED CROSSWALK OPTION B

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
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STD. PLAN NO. **T02-21**

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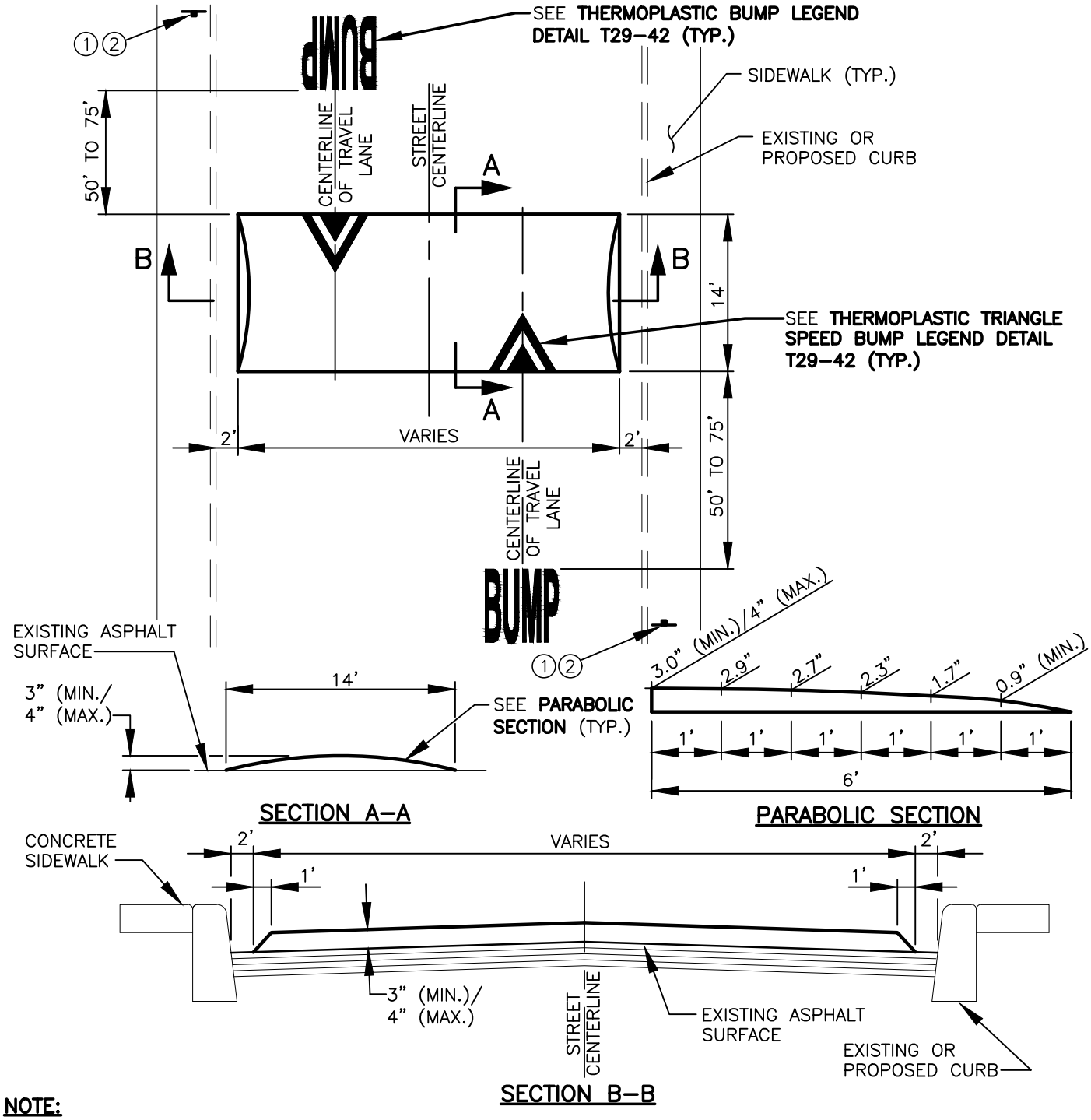

NOTE:

1. SPEED BUMP SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.

SIGNS:

① INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03.**

② IN PLACING SIGNS AT MULTIPLE SPEED BUMPS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCKS OR MILEAGE, NOT AT EACH RAISED CROSSWALK.

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CITY OF Vancouver WASHINGTON

SPEED BUMP

CITY OF VANCOUVER
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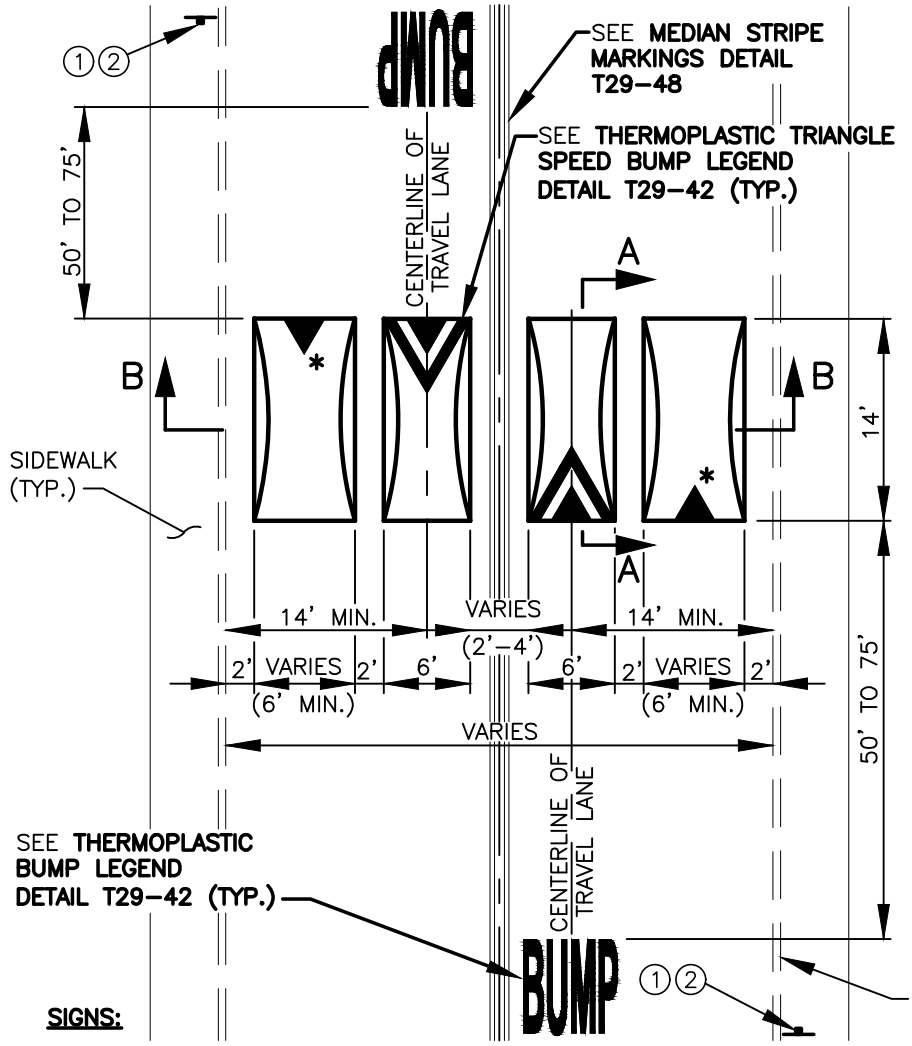
STD. PLAN NO. **T02-22**



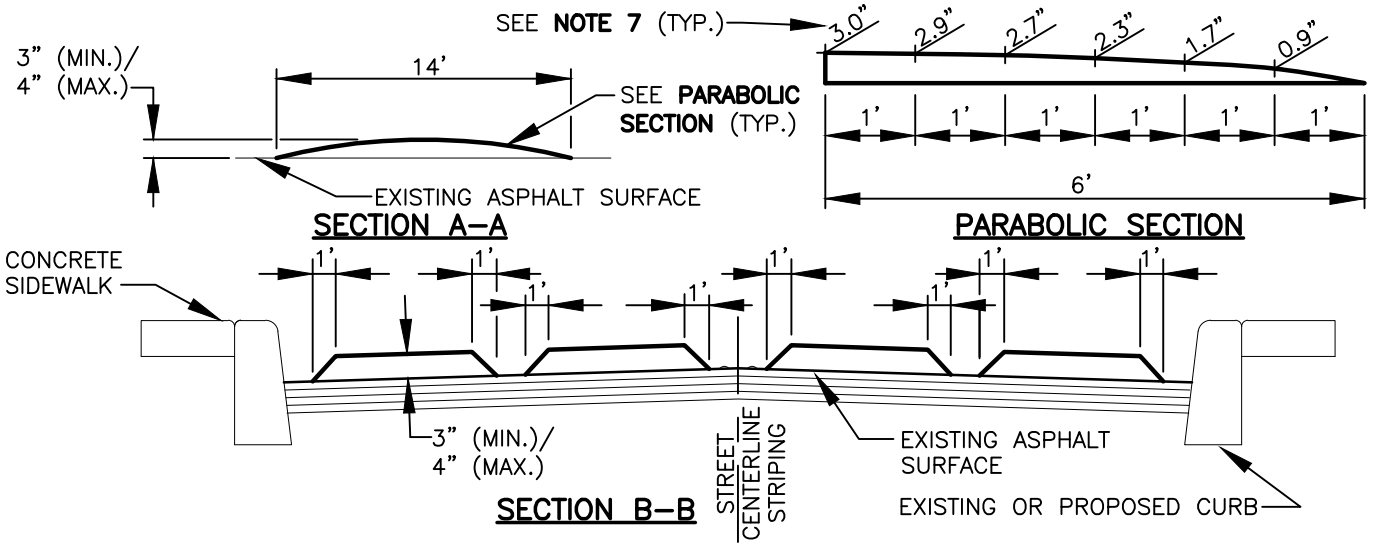
NOTES:

1. POSITION SPEED CUSHIONS ON STRAIGHT SECTIONS OF ROADWAY.
2. USE OF TYPE I ON STREETS LESS THAN 38' WIDE REQUIRES PARKING RESTRICTIONS 50' ON EACH SIDE.
3. TYPE I NOT APPROPRIATE FOR STREETS LESS THAN 36' WIDE.
4. CENTERLINE STRIPE MAY NOT BE IN THE CENTER OF ROADWAY, BUT IF NOT STRIPED, POSITION CUSHIONS SYMMETRICAL TO CENTER OF ROADWAY.
5. SPEED CUSHION SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.
6. * USE ADDITIONAL PAVEMENT MARKING ON STREETS WITH BIKE LANES (CENTER IN BIKE LANE). SEE **BIKE LANE TRIANGLE SPEED BUMP LEGEND DETAIL T29-42 (TYP.)**
7. 0.9" (MIN.) AT FIRST 1', AT TOP 3" (MIN.)/4" (MAX.).

SIGNS:

① INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03.**

② IN PLACING SIGNS AT MULTIPLE SPEED CUSHIONS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCKS OR MILEAGE, NOT AT EACH RAISED CROSSWALK.



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CITY OF Vancouver WASHINGTON

SPEED CUSHION TYPE 1

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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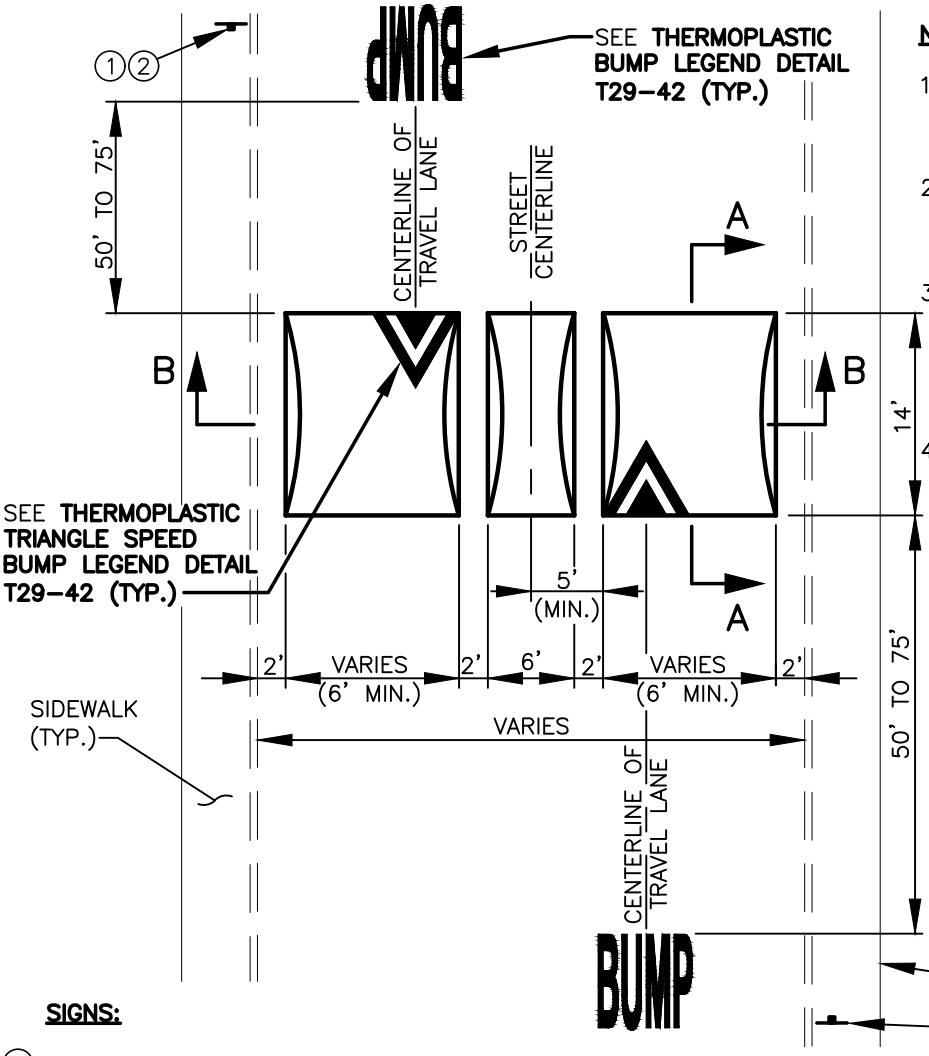
CDC	MAHE	8/04
REVISION	APPROVED BY	APPROVAL DATE
7	MAHE	3/24

STD. PLAN NO. **T02-23**



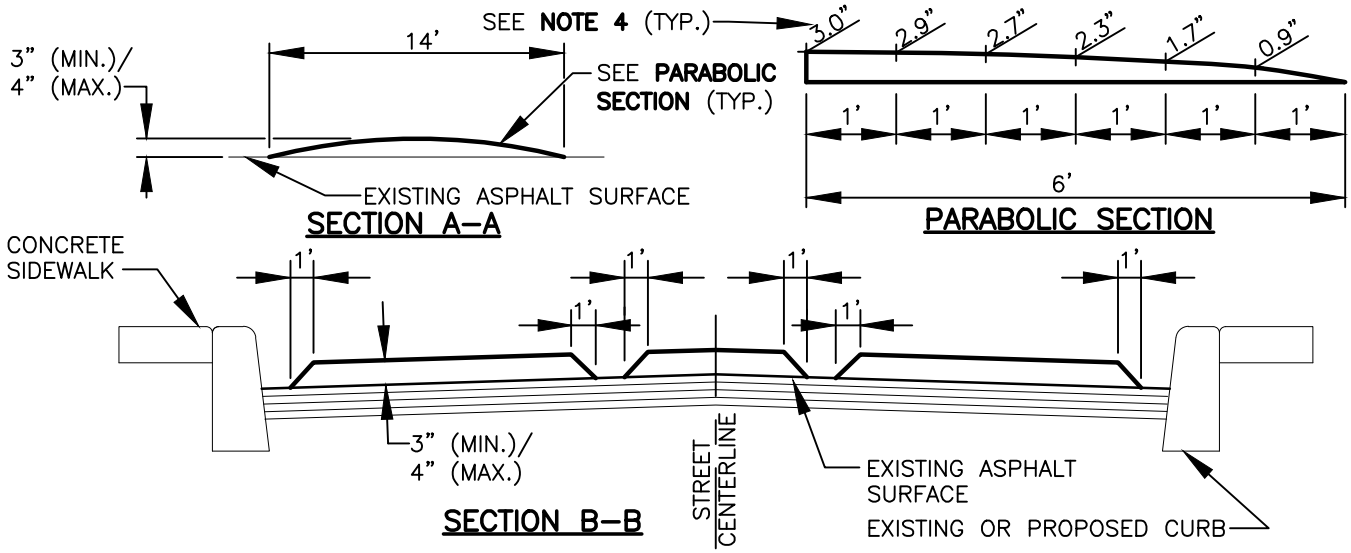
NOTES:

1. POSITION SPEED CUSHIONS ON STRAIGHT SECTIONS OF ROADWAY.
2. TYPE II INTENDED FOR STREETS LESS THAN 38' WIDE WITHOUT TRANSIT SERVICES.
3. SPEED CUSHION SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H–22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.
4. 0.9" (MIN.) AT FIRST 1', AT TOP 3" (MIN.)/4" (MAX.).

SIGNS:

① INSTALL MUTCD STANDARD W17–1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29–02 AND T29–03.**

② IN PLACING SIGNS AT MULTIPLE SPEED CUSHIONS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16–4" DESIGNATING HOW MANY BLOCKS OR MILEAGE, NOT AT EACH RAISED CROSSWALK.



CITY OF Vancouver WASHINGTON

SPEED CUSHION TYPE II

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

DRAWN BY	APPROVED BY	APPROVAL DATE
CDC	MAHE	8/04
REVISION	APPROVED BY	APPROVAL DATE
7	MAHE	3/24

STD. PLAN NO. **T02-24**

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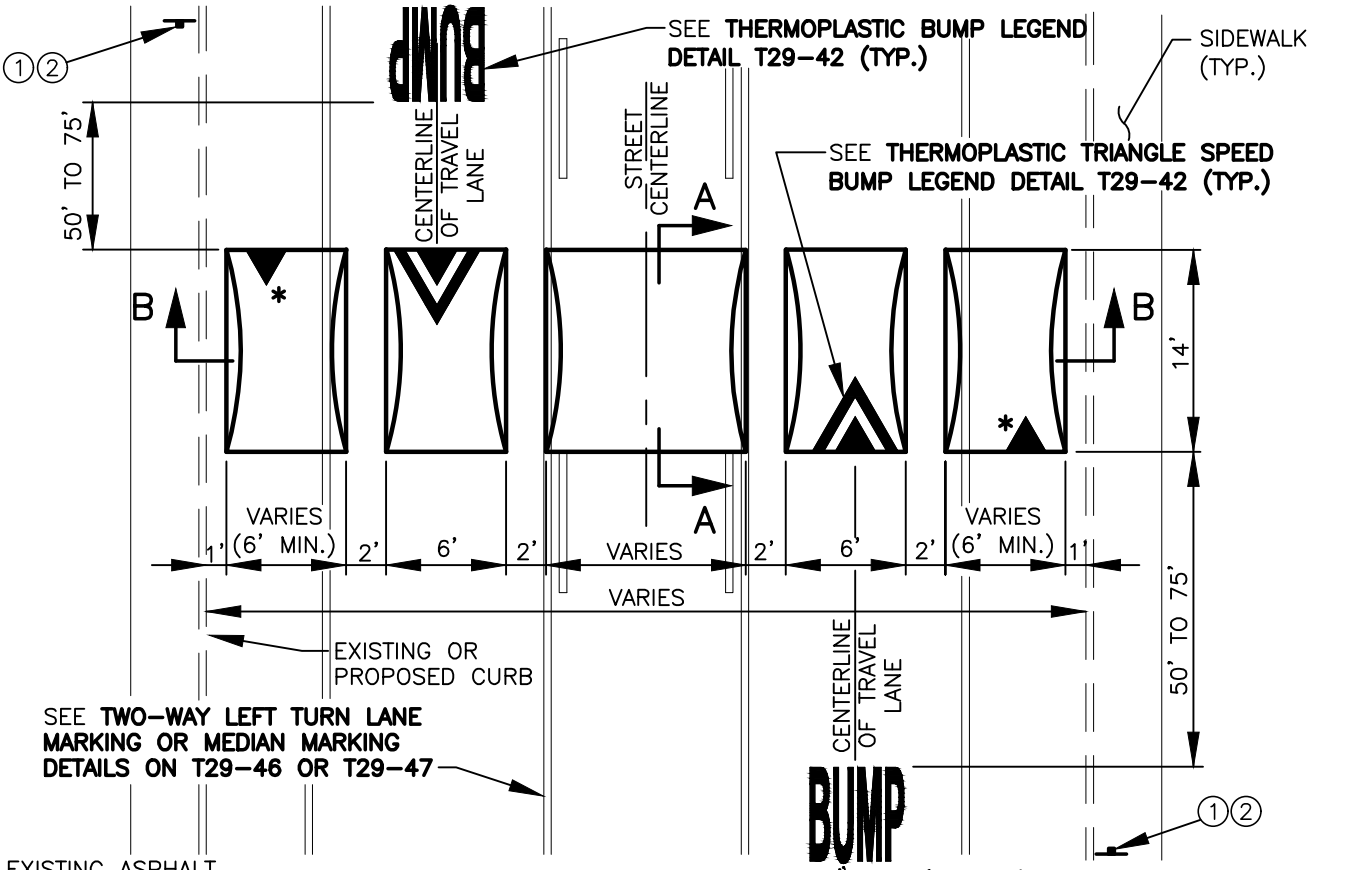

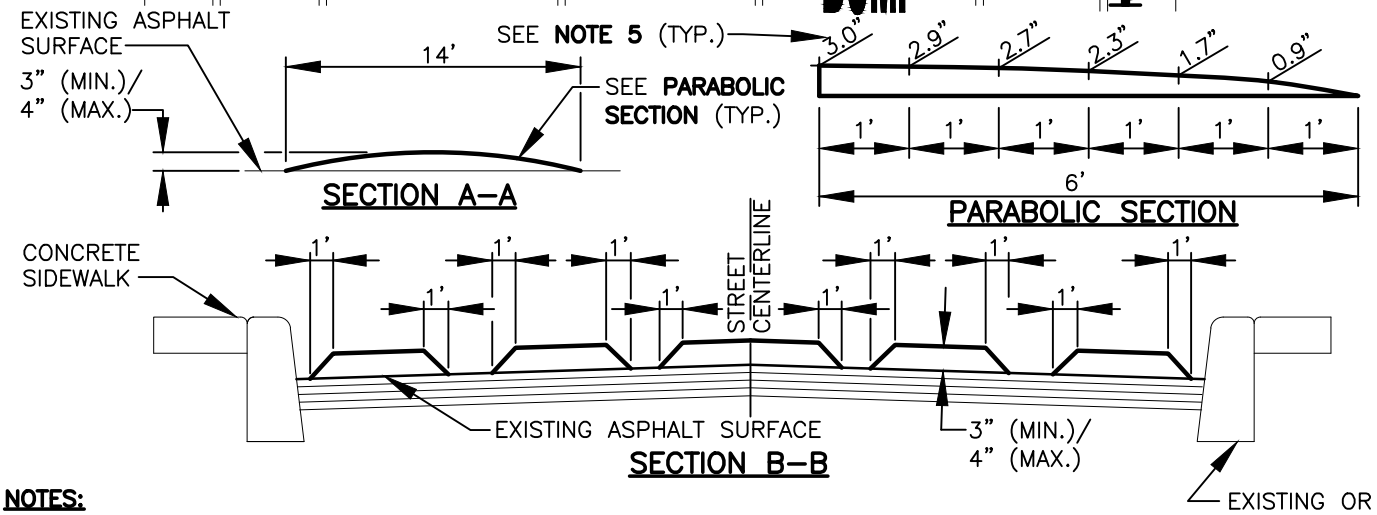

NOTES:

1. POSITION SPEED CUSHIONS ON STRAIGHT SECTIONS OF ROADWAY.
2. HOLD EDGE OF SPEED CUSHION A MINIMUM OF 3' FROM MEDIAN CURB.
3. SPEED CUSHION SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.
4. *USE ADDITIONAL PAVEMENT MARKING ON STREETS WITH BIKE LANES (CENTER IN BIKE LANE. SEE **BIKE LANE TRIANGLE SPEED BUMP LEGEND DETAIL T29-42 (TYP.).**
5. 0.9" (MIN.) AT FIRST 1', AT TOP 3" (MIN.)/4" (MAX.).

SIGNS:

(1) INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 150' (POSTED SPEED=25MPH) OR 175' (POSTED SPEED=30MPH) AHEAD OF RAISED CROSSWALK AT BEGINNING OF A SERIES OF HUMPS OR AS NECESSARY. SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03.**

(2) IN PLACING SIGNS AT MULTIPLE SPEED CUSHIONS ALONG A ROADWAY, HAVE THE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCKS OR MILEAGE, NOT AT EACH RAISED CROSSWALK.

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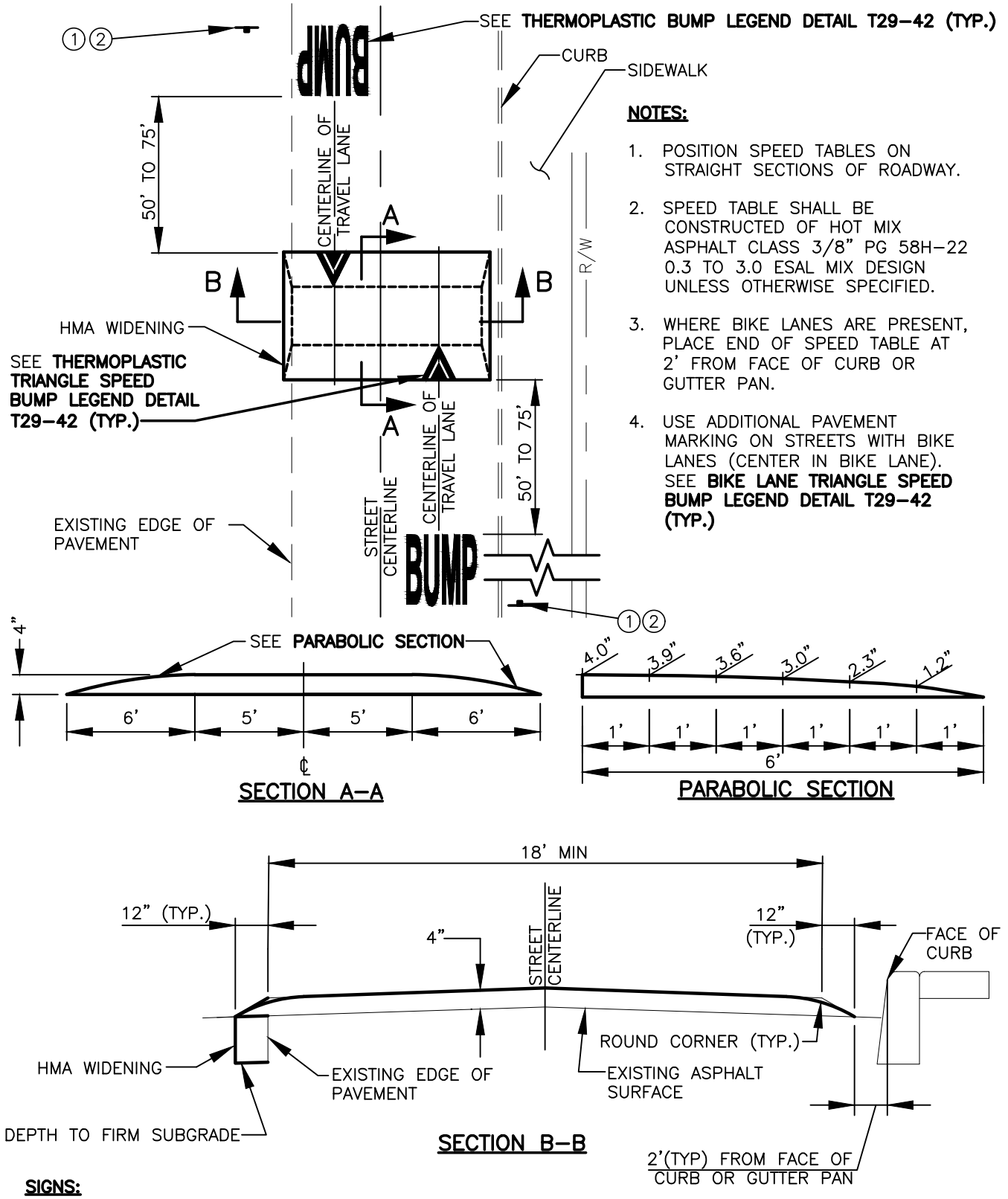
CITY OF Vancouver WASHINGTON

SPEED CUSHION TYPE III

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

DRAWN BY	APPROVED BY	APPROVAL DATE
CDC	MHH	8/04
REVISION	APPROVED BY	APPROVAL DATE
7	MHH	3/24

STD. PLAN NO. **T02-25**



NOTES:

1. POSITION SPEED TABLES ON STRAIGHT SECTIONS OF ROADWAY.
2. SPEED TABLE SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.
3. WHERE BIKE LANES ARE PRESENT, PLACE END OF SPEED TABLE AT 2' FROM FACE OF CURB OR GUTTER PAN.
4. USE ADDITIONAL PAVEMENT MARKING ON STREETS WITH BIKE LANES (CENTER IN BIKE LANE). SEE **BIKE LANE TRIANGLE SPEED BUMP LEGEND DETAIL T29-42 (TYP.)**

SIGNS:

(1) INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 150' AHEAD OF SPEED TABLE AT THE BEGINING OF A SERIES OF SPEED TABLES OR AS NECESSARY SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03**

(2) IN PLACING SIGNS AT MULTIPLE SPEED TABLES ALONG A ROADWAY, PLACE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCKS OR MILEAGE, NOT AT EACH SPEED TABLE.

CITY OF Vancouver WASHINGTON

SPEED TABLE (25 MPH POSTED)

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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CDC	MAHE	7/19
REVISION	APPROVED BY	APPROVAL DATE
3	MAHE	3/24

STD. PLAN NO. **T02-26A**

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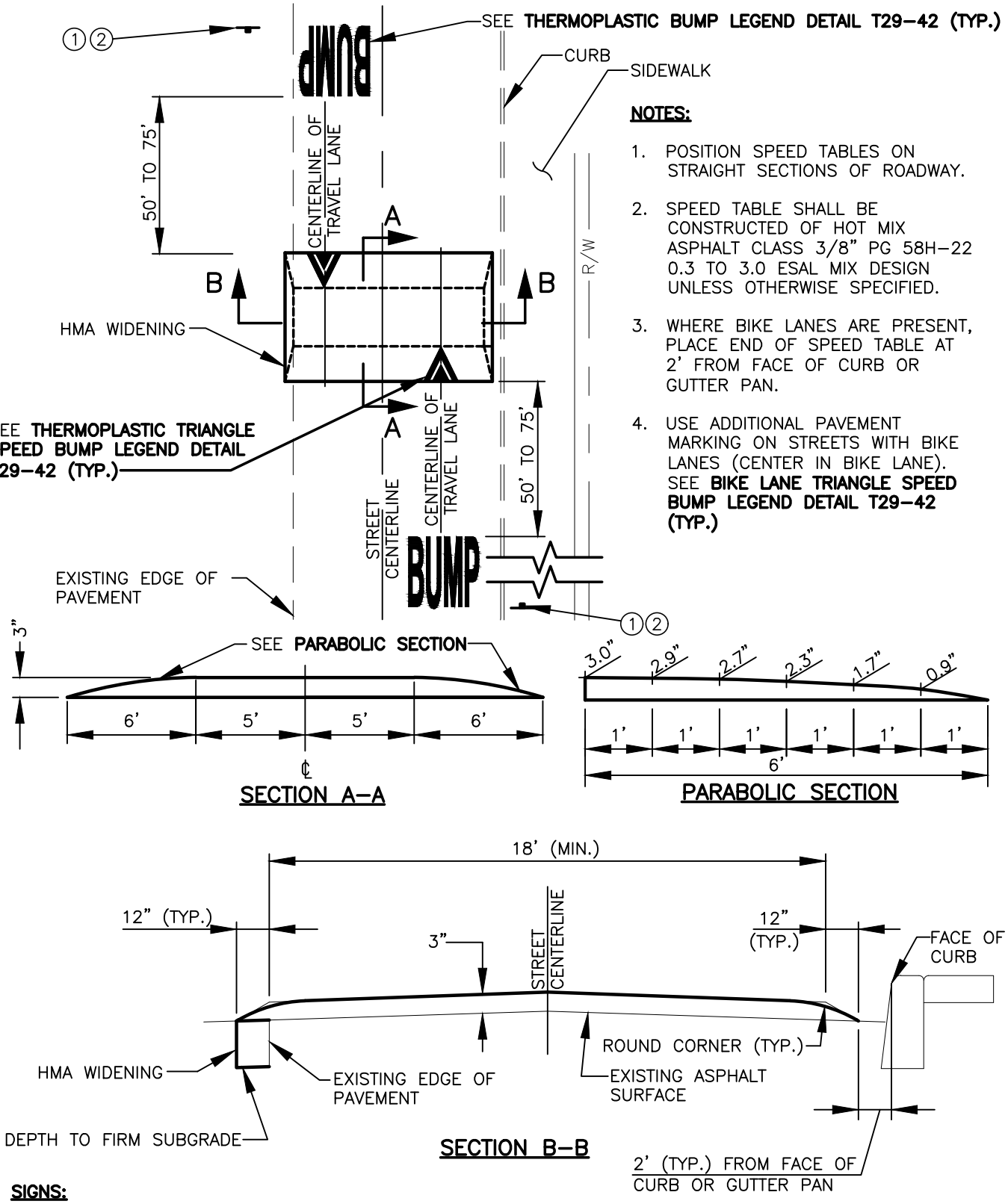

NOTES:

1. POSITION SPEED TABLES ON STRAIGHT SECTIONS OF ROADWAY.
2. SPEED TABLE SHALL BE CONSTRUCTED OF HOT MIX ASPHALT CLASS 3/8" PG 58H-22 0.3 TO 3.0 ESAL MIX DESIGN UNLESS OTHERWISE SPECIFIED.
3. WHERE BIKE LANES ARE PRESENT, PLACE END OF SPEED TABLE AT 2' FROM FACE OF CURB OR GUTTER PAN.
4. USE ADDITIONAL PAVEMENT MARKING ON STREETS WITH BIKE LANES (CENTER IN BIKE LANE). SEE **BIKE LANE TRIANGLE SPEED BUMP LEGEND DETAIL T29-42 (TYP.)**

SIGNS:

(1) INSTALL MUTCD STANDARD W17-1 ("SPEED BUMP") SIGN 175' AHEAD OF SPEED TABLE AT THE BEGINING OF A SERIES OF SPEED TABLES OR AS NECESSARY SEE **SIGN MOUNTING DETAILS T29-02 AND T29-03**

(2) IN PLACING SIGNS AT MULTIPLE SPEED TABLES ALONG A ROADWAY, PLACE SIGNS AT THE BEGINNING OF THE INSTALLATION WITH SUPPLEMENTAL SIGN "W16-4" DESIGNATING HOW MANY BLOCKS, NOT AT EACH SPEED TABLE.

CITY OF Vancouver WASHINGTON

SPEED TABLE (30 MPH POSTED)

CITY OF VANCOUVER
DEPARTMENT OF PUBLIC WORKS
TRANSPORTATION DIVISION

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CDC	MAHE	7/19
REVISION	APPROVED BY	APPROVAL DATE
3	MAHE	3/24

STD. PLAN NO. **T02-26B**

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